



The Corporation of the City of Courtenay

Council Agenda

Meeting #: R10/2024
Date: June 12, 2024
Time: 4:00 p.m.
Location: CVRD Civic Room, 770 Harmston Ave, Courtenay

We respectfully acknowledge that the land we gather on is Unceded territory of the K'ómoks First Nation, the traditional keepers of this land.

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Verbal update by: Sarah Smith, Director of Regional Development, and Tara Schmidt Senior Development Manager Vancouver Island at BC Housing.	
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A delegation by Comox Strathcona Waste Management Service (CSWM) to provide an update on its Solid Waste Management Plan Renewal:	
<ul style="list-style-type: none">• Sarah Willie, Manager of Solid Waste Planning and Policy Development• Marc Rutten, General Manager of Engineering Services• Vivian Schau, Senior Manager of CSWM Services	
The CSWM is renewing its Solid Waste Management Plan and aims to update the community on the management of garbage, recycling, yard, and food waste programs for the next decade. Additionally, the delegation will discuss opportunities for the City to participate in the next phase of planning and public consultation.	
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9.1	<p>Union of British Columbia Municipalities (UBCM) Resolution - Advancing Local Government Actions Toward the Declaration on the Rights of Indigenous Peoples Act (DRIPA) and Reconciliation (Councillor Jolicoeur)</p> <p>WHEREAS the Province of BC is implementing the 10 Principles within the Declaration of Rights of Indigenous Peoples Act (DRIPA) and local governments and Indigenous organizations are collectively working towards reconciliation actions; and</p> <p>WHEREAS many local governments and Indigenous organizations do not have the capacity for the extensive work required for effective partnership building in the spirit of reconciliation;</p> <p>THEREFORE BE IT RESOLVED that UBCM request that the Province of BC provide an ongoing funding stream for local governments and Indigenous partners to</p>	

advance DRIPA and local reconciliation actions.

10. COUNCIL RESOLUTIONS

10.1 Urgent Need for Downtown Washroom Facilities – Advocacy to Island Health (Councillor Morin)

WHEREAS the numbers of people in our community who are unsheltered have increased dramatically, and there are inadequate washroom facilities for those who are unsheltered; and

WHEREAS we are seeing evidence of an increase of public defecation and urination, potentially posing a public health risk to humans, animals, and the environment; and

WHEREAS due to the significant compromised health conditions of those who are unsheltered, and for safety and security reasons, public washrooms require enhanced maintenance and supervision; and

WHEREAS according to the Canadian National Housing Strategy Act of 2019, every person has a fundamental human right to housing; and

WHEREAS basic housing includes provision of heat, washroom facilities, and clean water; and

WHEREAS the responsibility for public health lies with provincial and federal governments and health authorities such as Island Health;

THEREFORE BE IT RESOLVED Council direct staff to request a meeting with Island Health’s Medical Health Officer and appropriate Island Health staff to discuss the health impacts of the lack of adequate daytime and no after-hours washroom facilities for those without shelter; and

BE IT FURTHER RESOLVED that the Mayor, on behalf of Council, send a letter to Island Health requesting the urgent provision of capital and operational funding to support downtown 24-hour washroom facilities; and

BE IT RESOLVED that Island Health be requested to appear as a delegation at Council to respond to the City’s requests regarding provision of and support for downtown 24-hour washroom facilities.

11. NEW BUSINESS

12. BYLAWS

12.1 For Adoption:

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13. COUNCIL REPORTS

- 13.1 Councillor Cole-Hamilton
- 13.2 Councillor Frisch
- 13.3 Councillor Hillian 397
- 13.4 Councillor Jolicoeur
- 13.5 Councillor McCollum 399
- 13.6 Councillor Morin
- 13.7 Mayor Wells

14. IN CAMERA RESOLUTION

THAT Council close the meeting to the public pursuant to the following subsections of the *Community Charter*:

90 (1) A part of a council meeting may be closed to the public if the subject matter being considered relates to or is one or more of the following:

- (a) personal information about an identifiable individual who holds or is being considered for a position as an officer, employee or agent of the municipality or another position appointed by the municipality;
- (c) labour relations or other employee relations;
- (e) the acquisition, disposition or expropriation of land or improvements, if the council considers that disclosure could reasonably be expected to harm the interests of the municipality;
- (i) the receipt of advice that is subject to solicitor-client privilege, including communications necessary for that purpose;
- (j) information that is prohibited, or information that if it were presented in a document would be prohibited, from disclosure under section 21 of the Freedom of Information and Protection of Privacy Act; and
- (k) negotiations and related discussions respecting the proposed provision of a municipal service that are at their preliminary stages and that, in the view of the council, could reasonably be expected to harm the interests of the municipality if they were held in public.

15. ADJOURNMENT



Special Council Minutes

Meeting #: S1/2024
Date: May 15, 2024
Time: 4:00 pm
Location: CVRD Civic Room, 770 Harmston Ave, Courtenay

Council Present: B. Wells
W. Cole-Hamilton (electronic)
D. Hillian
E. Jolicoeur (joined the meeting at 4:25 p.m.)
M. McCollum
W. Morin

Regrets: D. Frisch

Staff Present: G. Garbutt, City Manager (CAO)
A. Langenmaier, Director of Financial Services
K. O'Connell, Director of Corporate Services
C. Kilborn, Staff Sargent RCMP
T. Beauregard, Staff Sargent RCMP

1. CALL TO ORDER

Mayor Wells called the meeting to order at 4:01 p.m. and respectfully acknowledged that the meeting was conducted on the Unceded territory of the K'ómoks First Nation, the traditional keepers of this land.

2. UNFINISHED BUSINESS

2.1 Prioritization of Community Policing Services for 2024/25

Moved by Hillian
Seconded by Jolicoeur

THAT Council identify the following top five areas of concern regarding police services in the City of Courtenay for 2024/25:

1. **Violent Crime:** Drug trafficking, violence in relationships, sexual assaults;
2. **Property Crime;**
3. **Road Safety:** Roadways and off-road vehicles;
4. **Police Community Relations:**
 - Liaising with residents and community;
 - Bylaws: homeless encampments;
 - Collaboration with Comox Valley Regional District emergency planning;
 - Mental health awareness and support;
 - Police presence in Downtown;
5. **Support reconciliation with and enhance service delivery to Indigenous communities and K'ómoks First Nation (KFN):**
 - Police and community relations regarding KFN bylaws;
 - Increasing communications support with KFN; and

THAT Council direct staff to communicate their top five areas of concern to the RCMP.

CARRIED

**Moved by Morin
Seconded by McCollum**

THAT Council request the RCMP include initiative status and outcomes as part of their quarterly report to Council; and

THAT Council request the RCMP provide a report at the end of the year summarizing their annual activities service activities and outcomes.

CARRIED

3. ADJOURNMENT

Without objection, Mayor Wells adjourned the meeting at 5:12pm.



Council Minutes

Meeting #: R9/2024
Date: May 22, 2024
Time: 4:00 pm
Location: CVRD Civic Room, 770 Harmston Ave, Courtenay

Council Present: B. Wells
W. Cole-Hamilton
D. Frisch
D. Hillian
E. Jolicoeur (electronic)
M. McCollum
W. Morin

Staff Present: G. Garbutt, City Manager (CAO)
C. Davidson, Director of Engineering Services (electronic)
A. Langenmaier, Director of Financial Services
S. Saunders, Director of Recreation, Culture & Community Services
K. Shaw, Director of Public Works Services
M. Wade, Director of Development Services
E. Chow, Manager Legislative Services
N. Gothard, Environmental Planner
C. Thompson, Manager of Public Works Services
L. Bourgeois, Deputy Corporate Officer

1. CALL TO ORDER

Mayor Wells called the meeting to order at 4:00 pm and respectfully acknowledged that the meeting was conducted on the Unceded territory of the K'ómoks First Nation, the traditional keepers of this land.

2. INTRODUCTION OF LATE ITEMS

Councillor Morin informed that the K'ómoks First Nation has requested support regarding an Addition to Reserve Application to the Government of Canada, and requires a response no later than June 10, 2024. As the next Council meeting is scheduled for June 12, 2024, this item has been brought forward for Council's consideration this evening.

Moved By Morin

Seconded By Cole-Hamilton

THAT Council add correspondence from K'omoks First Nation under a new item numbered 5.3 - *Letter from K'omoks First Nation – Requesting support regarding an Addition to Reserve Application to the Government of Canada.*

CARRIED

3. ADOPTION OF MINUTES

3.1 Regular Council Minutes - May 8, 2024

Council, by unanimous consent, passed the following motion:

THAT Council adopt the May 8, 2024 Regular Council minutes.

CARRIED

4. STAFF REPORTS

4.1 Financial Services

4.1.1 Regional Social Development Grant Service Feedback

Moved By Frisch

Seconded By Hillian

THAT Council direct staff to inform the Comox Valley Regional District (CVRD) that it does not support participating in the proposed Regional Social Development Grant Service at this time; and

THAT the City request the CVRD to coordinate a fulsome review of all grant and grant like programs of Courtenay, Comox, Cumberland, and the CVRD to inform future grant service development.

CARRIED

4.2 Operational Services

4.2.1 Sustainable Infrastructure Investment Plan

Moved By Frisch

Seconded By Hillian

THAT Council approve the Sustainable Infrastructure Investment Plan.

CARRIED

Moved By Morin
Seconded By Frisch

THAT Council direct staff to implement the Sustainable Infrastructure Investment Plan by including the Plan's recommendations in the 2025-2029 financial planning process.

CARRIED

5. EXTERNAL REPORTS AND CORRESPONDENCE

5.1 Letter from Ravi Kahlon - Minister of Housing

Moved By Frisch
Seconded By Morin

THAT Council receive the letter from Ravi Kahlon, Minister of Housing regarding the City of Courtenay's request for Homeless Encampment Action Response Team (HEART) and Homeless Encampment Action Response Temporary Housing (HEARTH) programs.

CARRIED

5.2 Letter from Office of the Prime Minister

Moved By McCollum
Seconded By Frisch

THAT Council receive the letter from Office of the Prime Minister regarding the City of Courtenay's request for federal aid in infrastructure funding and municipal financing.

CARRIED

5.3 Letter from K'omoks First Nation – Request for Support Regarding an Addition to Reserve Application to the Government of Canada

Moved By Morin
Seconded By Hillian

THAT Council support the K'omoks First Nation's Addition to Reserve Application to the Government of Canada for three parcels (208 acres/84 hectares) of fee-simple land held by the Nation in the Union Bay Area (Southlands Project) for the following reasons:

- a. The support for the proposed Addition to Reserve is consistent with the reconciliation commitments made by the City of Courtenay in "Official Community Plan Bylaw 3070, 2022" and 2023-2026 Strategic Priorities Plan; and

- b. The proposed Addition reflects the intent of the policies contained with in the Regional Growth Strategy with respect to economic development and reconciliation for K'ómoks First Nation.

CARRIED

Moved By Frisch

Seconded By Cole-Hamilton

THAT Mayor Wells, on behalf of Council, send a letter to the K'ómoks First Nation in support of the Addition to Reserve Application to the Government of Canada.

CARRIED

6. INTERNAL REPORTS AND CORRESPONDENCE

6.1 Courtenay and District Memorial Outdoor Pool and Wading Pool

Moved By Hillian

Seconded By Cole-Hamilton

THAT Council receive the “Courtenay and District Memorial Outdoor Pool and Wading Pool” briefing note.

CARRIED

6.2 Management Reports - 2024

Moved By Hillian

Seconded By Frisch

THAT Council receive the “Management Reports – 2024” briefing note.

CARRIED

Without objection, Mayor Wells called a recess at 6:02 pm. The Council meeting resumed at 6:21 pm.

7. COUNCIL RESOLUTIONS

7.1 Rise and Report – 1915 Cumberland Road

On May 8, 2024, Council held a Special In Camera Council meeting, rising and reporting on the following motion:

THAT Council receive the report dated May 8, 2024 and direct staff to execute a Section 219 covenant for 1915 Cumberland to meet the conditions of the Zoning Amendment Bylaw No. 3044 that include: sewer main, Larsen Road turnaroud, amenity contributions and through traffic restrictions; and

THAT Council rise and report this resolution at the May 22, 2024 Council meeting.

8. NOTICE OF MOTION

8.1 Urgent Need for Downtown Washroom Facilities – Advocacy to Island Health (Councillor Morin)

WHEREAS the numbers of people in our community who are unsheltered have increased dramatically, and there are inadequate washroom facilities for those who are unsheltered; and

WHEREAS we are seeing evidence of an increase of public defecation and urination, potentially posing a public health risk to humans, animals, and the environment; and

WHEREAS due to the significant compromised health conditions of those who are unsheltered, and for safety and security reasons, public washrooms require enhanced maintenance and supervision; and

WHEREAS according to the Canadian National Housing Strategy Act of 2019, every person has a fundamental human right to housing; and

WHEREAS basic housing includes provision of heat, washroom facilities, and clean water; and

WHEREAS the responsibility for public health lies with provincial and federal governments and health authorities such as Island Health;

THEREFORE BE IT RESOLVED Council direct staff to request a meeting with Island Health’s Medical Health Officer and appropriate Island Health staff to discuss the health impacts of the lack of adequate daytime and no after-hours washroom facilities for those without shelter; and

BE IT FURTHER RESOLVED that the Mayor, on behalf of Council, send a letter to Island Health requesting the urgent provision of capital and operational funding to support downtown 24-hour washroom facilities; and

BE IT RESOLVED that Island Health be requested to appear as a delegation at Council to respond to the City’s requests regarding provision of and support for downtown 24-hour washroom facilities.

9. NEW BUSINESS

10. BYLAWS

10.1 For First and Second Readings:

10.1.1 City of Courtenay Official Community Plan Amendment Bylaw No. 3141, 2024

Moved By Frisch
Seconded By Cole-Hamilton

THAT Council give first and second readings to “*City of Courtenay Official Community Plan Amendment Bylaw No. 3141, 2024*”.

CARRIED

Moved By Hillian
Seconded By Morin

THAT Council direct staff to set a public hearing date for June 19, 2024 at 4:00 pm at the CVRD Civic Room 770 Harmston Ave Courtenay, BC.

CARRIED

Moved By Cole-Hamilton
Seconded By Frisch

THAT Council direct staff to provide notice of the scheduled public hearing as per section 466 of the *Local Government Act*, section 94 of the *Community Charter* and “*City of Courtenay Development Procedures Bylaw No. 3106, 2023*”.

CARRIED

Moved By Hillian
Seconded By Morin

THAT Council under section 475 *Local Government Act*, will provide opportunities it considers appropriate for consultation with persons it considers will be affected, including the Comox Valley Regional District, Comox/Cumberland, First Nations, and the province/provincial agencies; and

THAT Council, under section 476 *Local Government Act*, will consult the school board.

CARRIED

10.2 For First, Second and Third Readings:

10.2.1 Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing)

Moved By Hillian
Seconded By Frisch

THAT Council give first reading to Zoning Amendment Bylaw No. 3135, 2024 (small-scale, multi-unit housing) dated May 8, 2024.

CARRIED

Moved By Frisch

Seconded By Morin

That Council modify Zoning Bylaw Amendment Bylaw No. 3135, 2024 (small-scale, multi-unit housing) as follows:

- a. In Section 6.19.1. (i) delete “or” after (1) secondary Suite add “and”;
- b. In Section 6.19.1 (ii) delete “or strata titling”;
- c. In Section 7.1.13 delete “the Downtown Core Area” and replace with “Walkable Areas”;
- d. In Section 7.1.13 (1) delete “for additional units, secondary to the principal residential use” and replace with “on a property”;
- e. In Section 7.1.13 add after Schedule 7F “Walkable Areas” and after Schedule 7E add “shown in Schedule C of this Bylaw”;
- f. In Schedule 7A-Required Number of Off-Street Parking Spaces add the R-SSMUH use under A. Residential and add under the Required Parking Space 1 per dwelling unit to correspond with the SSMUH use;
- g. In Schedule A to Zoning Amendment Bylaw No.3135, 2024 (small-scale, multi-unit housing), renumbering the duplicate occurrence of 8.1.6 to become 8.1.7, and renumbering the remainder of section 8.1 accordingly;
- h. In Schedule A to Zoning Amendment Bylaw No.3135, 2024 (small-scale, multi-unit housing) Section 8.1.9 Off Street Parking (2) delete “No more than 50% of the front yard area shall be used for off street parking” and replace with “Where more than one access point to a property is available, such as a front street and laneway, required parking stalls must be distributed in such a manner as to prevent a continuous row of parking stalls”;
- i. In (e) AMENDING DIVISION 8 Part 1 Residential Zones 2. Change “B17 to B18” and add after Small-Scale Multi-Unit (R-SSMUH) Zone, “Agriculture One (A-1), Public Use and Assembly Two (PA-2)”;
- j. In DIVISION 3 Part 1 add to the definition of duplex after “other” “and for certainty does not include a building that only contains a principal dwelling unit that contains a secondary suite”;
- k. In DIVISION 3 Part 1 for the definition dwelling townhouse:

- i. after “has” add the words “*at least two*”;
 - ii. delete the word “*a*”; and
 - iii. add the letters “*es*” to access;
- I. In DIVISION 3 Part 1 for the definition of secondary suite:
- i. add the word “*and*” after “*located*”; and
 - ii. delete the words “*contained within a building which is a single real estate entity*”.

CARRIED

Moved By McCollum

Seconded By Frisch

THAT Council give second reading as modified to Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing).

CARRIED

Moved By Cole-Hamilton

Seconded By Frisch

THAT Council give third reading as modified to Zoning Amendment Bylaw No. 3135, 2024 (small-scale-multi-unit housing).

CARRIED

Moved By Frisch

Seconded By McCollum

THAT given Ministerial Order No. M114, dated May 8, 2024, and regrading Approval Exemption (Controlled Access Highway) Regulation, Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing) does not require referral to the Minister of Transportation and Infrastructure for approval.

CARRIED

Moved By Cole-Hamilton

Seconded By Frisch

THAT Council confirms that it has given consideration to the Provincial Policy Manual and Site Standards document prepared by the Province of British Columbia, specifically, the standards recommended in Part 4 of the document.

CARRIED

Moved By McCollum

Seconded By Cole-Hamilton

THAT pursuant to Section 484.4 (2) of the *Local Government Act*, Staff prepare and submit the required letter of notification to the Province as soon as practicable after adoption of Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing), identifying the “Restricted Zone” properties that have been rezoned to the Residential Small-Scale Multi-Housing zone and the additional properties within “Restricted Zones” that are exempt from Sections 481.3 (4) and (5) of the *Local Government Act*.

CARRIED

Moved By Frisch

Seconded By McCollum

THAT staff, following final adoption and before the end of 2024, bring a report back to Council on the effects of on the development of infill housing within Courtenay.

CARRIED

Moved By Cole-Hamilton

Seconded By Frisch

THAT, given the size and scope of Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing), staff may need to bring forward additional amendments to correct the zoning bylaw, which will be done at no cost to the property owner.

CARRIED

10.2.2 Fees and Charges Amendment Bylaw No. 3138, 2024 – Development Application Fees

Moved By Hillian

Seconded By Frisch

THAT Council give first, second and third reading to "Fees and Charges Amendment Bylaw No. 3138, 2024 – Development Application Fees".

CARRIED

11. COUNCIL REPORTS

11.1 Councillor Cole-Hamilton

No report provided.

11.2 Councillor Frisch

No report provided.

11.3 Councillor Hillian

No report provided.

11.4 Councillor Jolicoeur

No report provided.

11.5 Councillor McCollum

No report provided.

11.6 Councillor Morin

Councillor Morin submitted a report of activities; see agenda.

11.7 Mayor Wells

Mayor Wells reviewed his attendance at the following events:

- The grand opening of the Courtenay Chamber of Commerce's Business Hub, located in downtown Courtenay, on May 9, 2024;
- The Royal Canadian Air Force's 100th anniversary at the Comox Air Show on May 18, 2024; and
- A Moose Hide Campaign event held on May 16, 2024.

Mayor Wells, in solidarity with the K'omoks First Nation, denounced an upcoming performance by the Danger Cats and informed that the City of Courtenay stands with First Nations communities.

12. ADJOURNMENT

Mayor Wells terminated the meeting at 6:46 pm.

May 31, 2024

The City of Courtenay
Mayor and Council
830 Cliffe Avenue
Courtenay, BC V9N 2J7

RE: Purpose-built shelter and permanent supportive housing at 925 Braidwood and Ryan Road

In March 2024, BC Housing purchased land at 925 Braidwood & Ryan Road following consultation with City staff and City Council. The proposed developments at this site will include a purpose-built shelter with on-site connections to health, mental health and housing and a permanent supportive housing building, providing an opportunity for people to leave the streets and shelter system for safe and stable housing.

This development is an important part of BC Housing's concerted effort to build new housing in the community and address the urgent need for culturally safe, indoor shelter to people experiencing homelessness. Since acquiring the land in March, we have been undertaking due diligence at the site, which has included liaising with K'ómoks First Nation on archeological considerations. We are working on the rezoning application and will continue to provide neighbours, stakeholders and the broader community the opportunity to engage with us on the designs. Feedback received during that engagement will be used to shape and improve on the project designs.

Community Updates and Feedback

- We will continue to provide updates on our webpage: letstalkhousingbc.ca/courtenay-925-braidwood-road
- This page includes a Q&A section where community members can ask questions about the project, and answers will be posted to the page.
- Community members can also email BC Housing at communityrelations@bchousing.org.
- We are working to launch a Design Discussion Guide on our website in June inviting neighbours and community members to learn more and provide feedback on design.

BC Housing is committed to walking alongside this community and to keeping the lines of communication open to ensure the success of this new permanent supportive housing and purpose-built shelter.

Kind regards,



Sarah Smith

Director, Regional Development, Vancouver Island

Office of the Chief Administrative Officer

770 Harmston Avenue, Courtenay, BC V9N 0G8
Tel: 250-334-6000 Fax: 250-334-4358
Toll free: 1-800-331-6007
www.comoxvalleyrd.ca



File: 5360-30/SWMP

April 26, 2024

Sent via email only

Geoff Garbutt
Chief Administrative Officer
City of Courtenay
830 Cliffe Ave
Courtenay, BC V9N 2J7

Re: Solid Waste Management Planning Process – Step 3

Our staff are requesting an opportunity to present an update on the renewal of the Solid Waste Management Plan to the City of Courtenay.

Comox Strathcona Waste Management Service (Service) is undertaking a Solid Waste Management Plan (Plan) renewal. This Plan will describe how the Service will manage garbage, recycling, yard, and food waste programs for the next decade. The Ministry of Environment and Climate Change Strategy recommends that plans be renewed every ten years to ensure that the plan reflects best practices and the evolving realities of solid waste. The Plan was updated 12 years ago, in 2012.

There are four steps of plan development, and each step builds upon the previous. The renewal process is now in *Step 3: Evaluate Strategies*. Step 3 aims to develop potential solid waste management strategies for consideration, assess the financial and administrative impacts and to consult the public on the specific strategies.

Service senior staff request an opportunity to provide a short presentation to the City of Courtenay on the progress to date and forthcoming opportunities to engage on the process as we move into this next phase of planning and public consultation.

The Comox Strathcona Waste Management service area is located in the Coast Salish, Kwakwaka'wakw, and Nuu-chah-nulth territories.

Comox Strathcona Waste Management manages over 100,000 tonnes of waste and recycled material annually and oversees a number of diversion and education programs for the Strathcona and Comox Valley Regional Districts.

Please contact Bridget Meagher, CSWM@comoxvalleyrd.ca to coordinate a date and time in May or June 2024 for such a presentation. To support climate change initiatives, we prefer a virtual participation.

Sincerely,

J. Warren

James Warren

Chief Administrative Officer

cc: Adriana Proton, Manager of Legislative Services, City of Courtenay
Marc Rutten, General Manager of Engineering Services
Vivian Schau, Senior Manager of CSWM Services
Sarah Willie, Manager of Solid Waste Planning and Policy Development

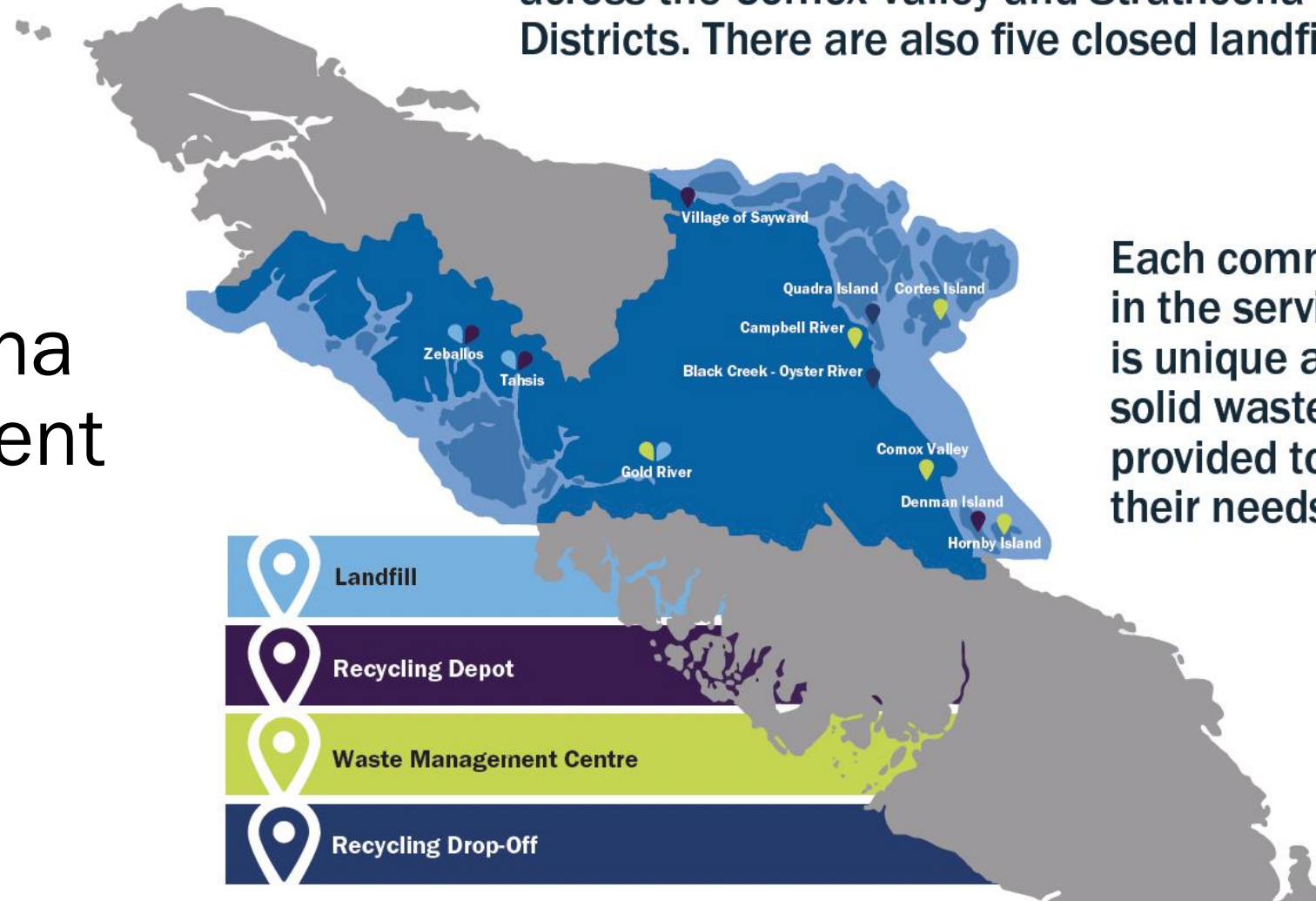
Solid Waste Management Plan Renewal



City of Courtenay
Council Presentation
June 12, 2024

CSWM operates waste management centres, recycling depots, transfer stations and landfills across the Comox Valley and Strathcona Regional Districts. There are also five closed landfills.

What is the Comox Strathcona Waste Management Service?



Each community in the service area is unique as are the solid waste services provided to meet their needs.

What is a Solid Waste Management Plan?



- The long-term vision for garbage and recycling.
- Presents the programs, services, infrastructure and policies that guide the design and implementation of solid waste for the next decade.
- The current Regional Solid Waste Management Plan was approved by the Ministry of Environment & Climate Change Strategy in 2013.

Why is a Solid Waste Management Plan Needed?

- The Ministry of Environment and Climate Change Strategies recommends plans be renewed every 10 years.
- As communities, regulations, and public needs change, CSWM must adapt to meet the evolving environment through the delivery of effective, cost efficient and convenient solid waste management services.
- Waste disposal is a long-term problem, without quick solutions.

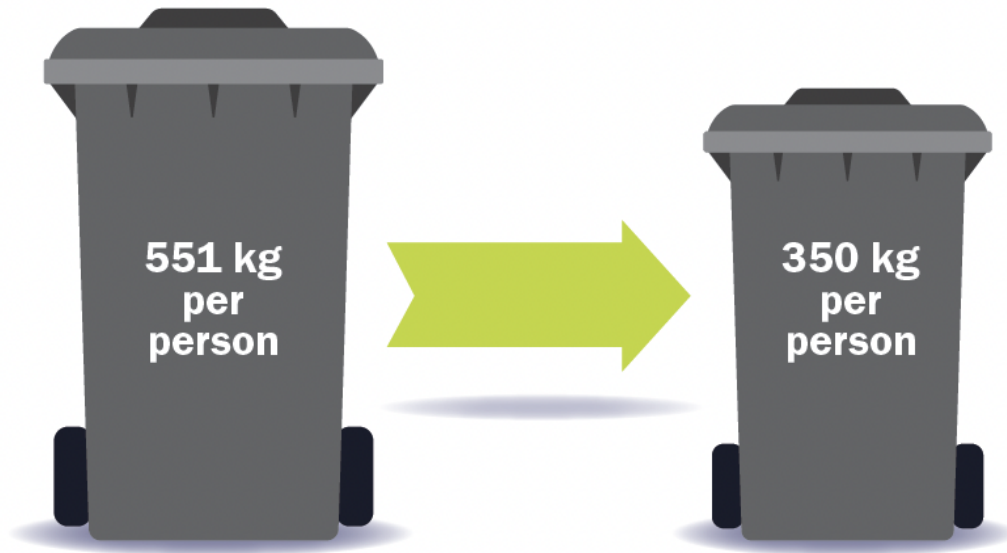


What is the goal of a Solid Waste Management Plan?

The goal is to produce less garbage.

On average, each person in the service area generates 551 kg of waste annually. The provincial target is 350 kg per person.

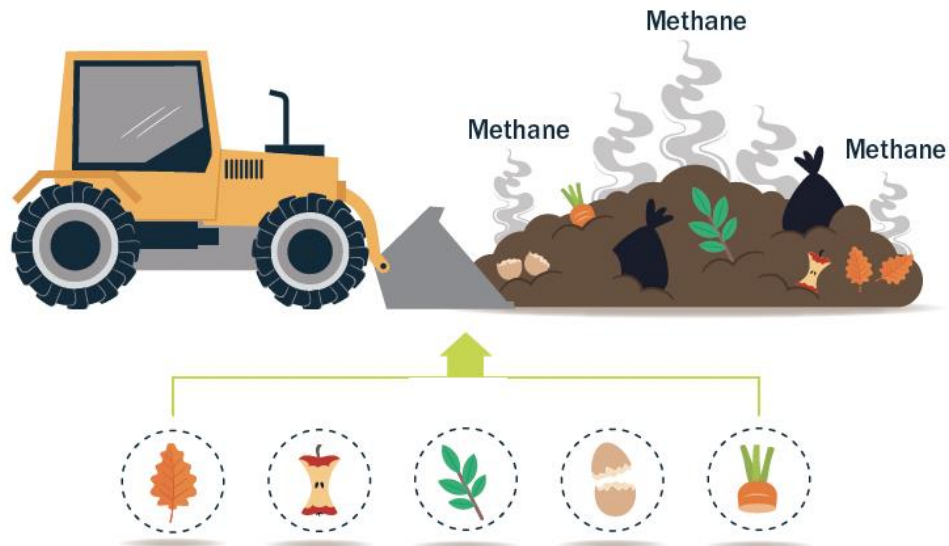
The CSWM service manages over 75,000 tonnes of waste and recycled material annually and oversees diversion and education programs for the CVRD and the SRD.



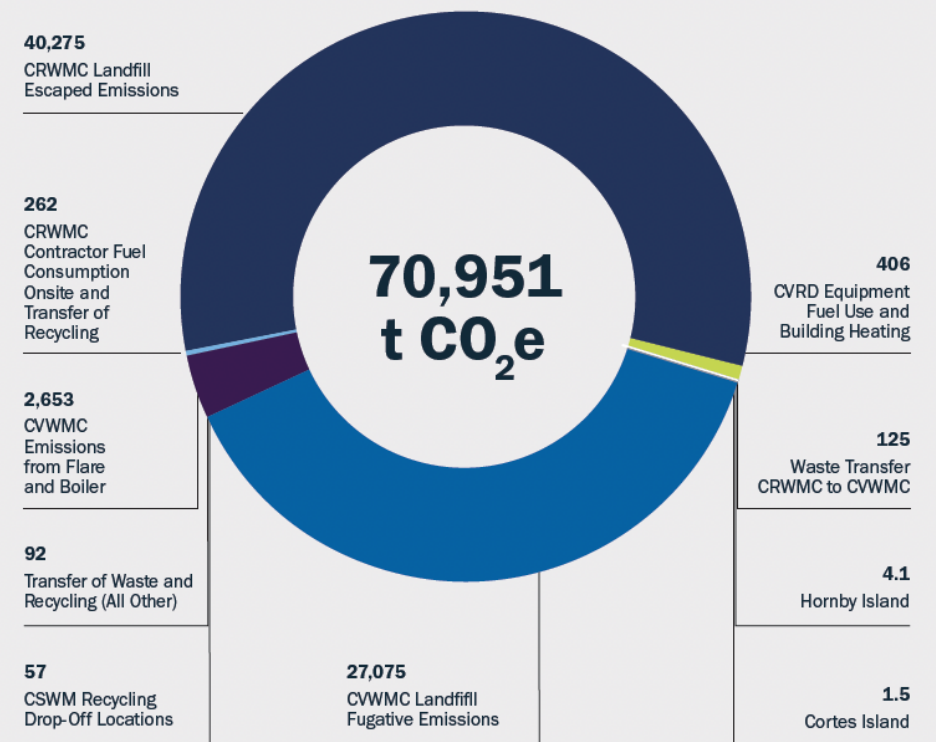
Greenhouse Gas Emissions

Municipal landfills account for about 23% of Canada's methane emissions. 95% of CSWM GHG's are generated from Comox Valley and Campbell River Waste landfills.

Less organic waste in landfills = less methane

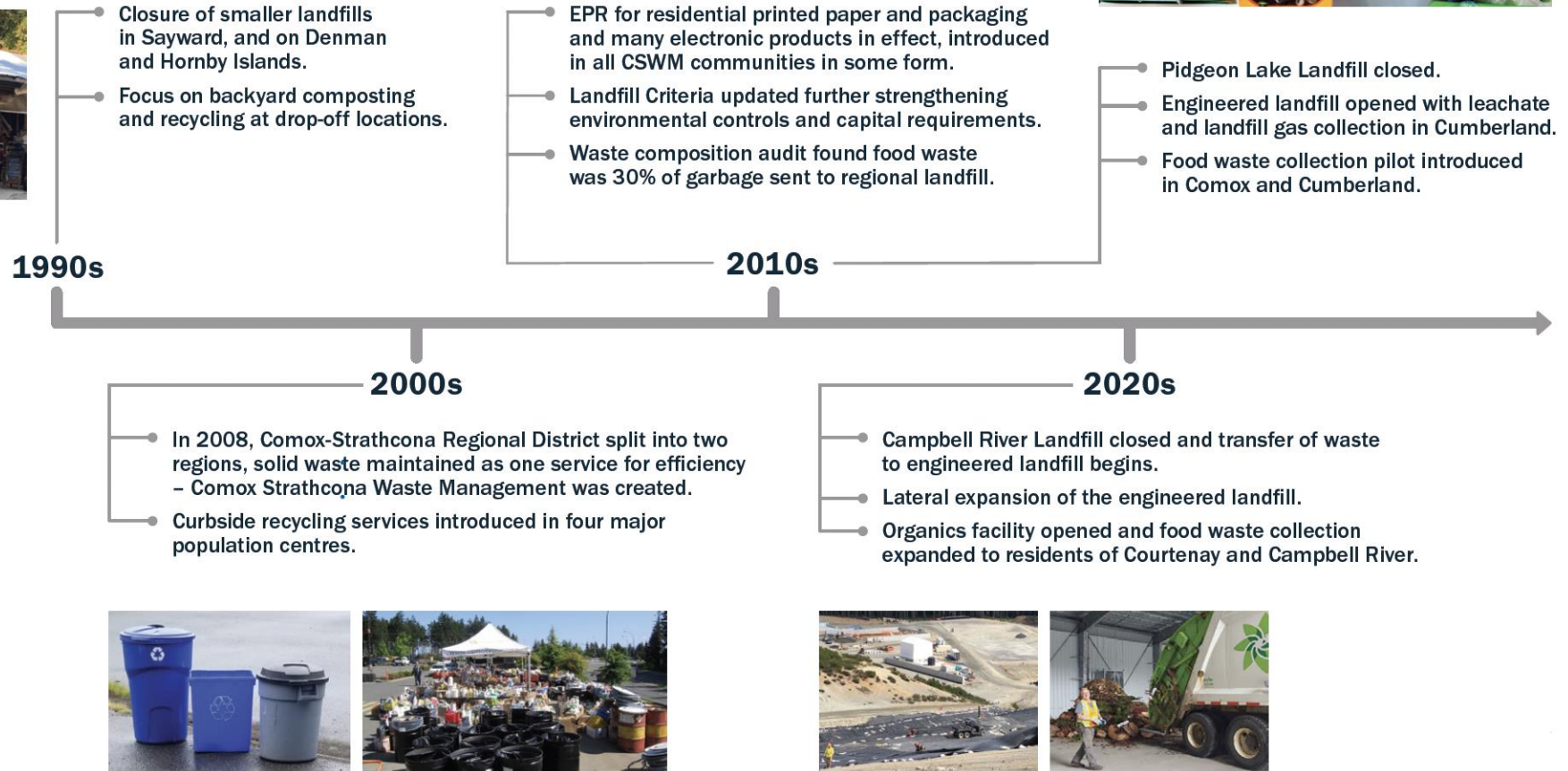


Estimate of 2021 Greenhouse Gases from CSWM Service (Tonnes of CO₂ Equivalent)



Evolution of Waste Management in CSWM

How far we have come:



Sphere of Influence

Municipal or First Nation	Regional District	Provincial Government	Federal Government
<ul style="list-style-type: none"> ✓ Residential Collection ✓ ICI or Multi-Family Collection ✓ Public Area Waste Collection ✓ Building and Business permits 	<ul style="list-style-type: none"> ✓ Solid Waste Management Plans ✓ Residual Waste Management ✓ Waste Diversion Targets ✓ Collection in Electoral Areas ✓ Recycling Depots ✓ Advocacy 	<ul style="list-style-type: none"> ✓ Environmental Management Act ✓ Recycling Reg. ✓ LFG Management Reg. ✓ Organic Matter Recycling Reg. ✓ Funding of Infrastructure 	<ul style="list-style-type: none"> ✓ Canadian Environmental Protection Act ✓ Single-use Plastics Prohibition Reg. ✓ Plastics Labelling, Registry, Recycled Content ✓ Landfill Emissions ✓ Grant Funding

The SWMP Renewal Process



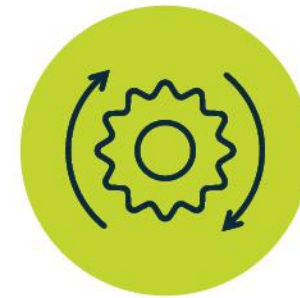
1.
Initiate the process
July 2022 to December 2022



2.
Set the Plan Direction
January 2023 to September 2023



3.
Evaluate Options
2024 to 2025



4.
Prepare and Adopt the Plan
2025 to 2026

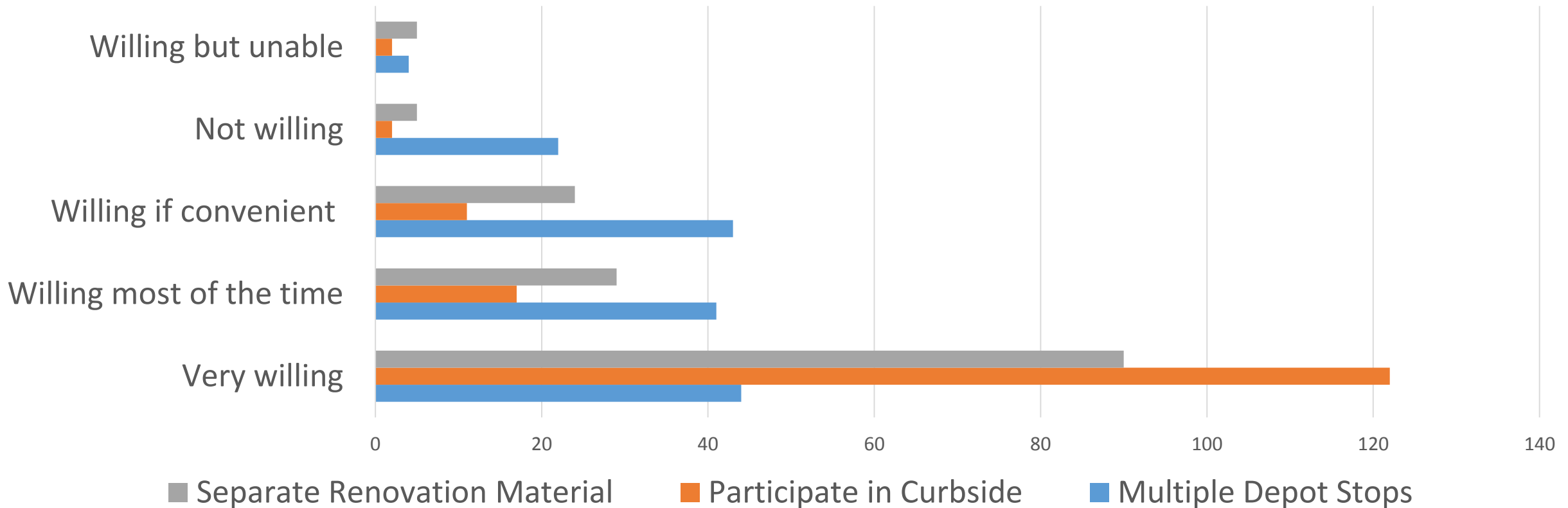
Step 1: Initiation - 2022

- Recruited the Regional Solid Waste Advisory Committee
- Survey of experiences with garbage, recycling and organics in their community (November 2022)
- Respondents expressed a desire for improved facilities to support an increased diversion of recycling, yard and food waste, with a primary focus on the need for increasing convenience and accepting a wider range of materials.



Resident Survey – 157 Courtenay Responses

How willing and able are you to take the following actions to increase your waste diversion?



Step 2: Plan Direction - 2023

Goal

1

To support regional circular economy approaches, and to work towards a goal of zero waste.



Goal

2

To reduce and remediate environmental impacts from solid waste management activities.



Goal

3

To reduce greenhouse gas emissions and to pursue beneficial use of landfill gas.



Goal

4

To responsibly manage waste, and to minimize the amount of residual waste in landfills.



Goal

5

To have informed residents, businesses and community partners empowered to participate effectively and consistently in proper waste management practices. To use CSWM enforcement powers when necessary.



Goal

6

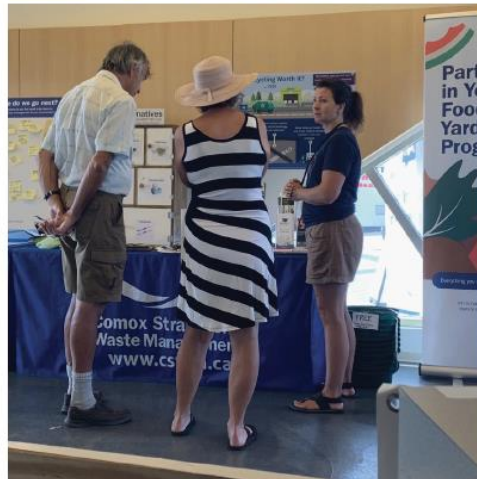
To ensure that the CSWM solid waste service are financially sustainable and delivered equitably across the region, recognizing and leveraging the unique challenges and cultures of our rural and remote communities.



To learn more about our goals and guiding principles you can visit our project webpage:

www.engagecomoxvalley.ca/swmp

What We Have Heard So Far...



Step 2: Canada Day – Feedback

- Improve options for recycling soft plastics, glass and Styrofoam
- Improved recycling options and waste management support for hospitals, businesses, schools, and homeless population.
- Explore options for recycling or reuse of dog waste.
- Rethink packing, i.e.: stop wrapping fruits and vegetables in plastic, stop individually packaging everything. Regulate the type of packaging/ wrapping – make it recyclable.
- More frequent recycling pickup, or larger containers, multiple sites that make it easier for people to recycle.

Step 2: Canada Day – Feedback

- Toolkit for businesses that show better recycling and waste reduction practices.
- Provide alternative options for seniors who are unable to drive to the depot.
- Recommend a drop off repair outlet (i.e. seamstress who can repair clothing and then donate it) so items don't end up in the landfill.
- Allow a second yard waste cart.

Step 3: Strategies and Options

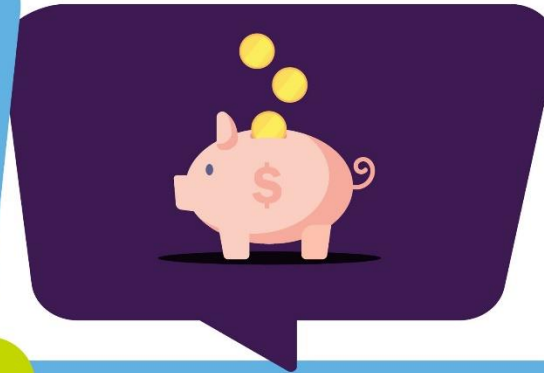


Step 3: Strategies and Options - 2024

- Generate strategies to achieve desired objectives
 - Landfill Tour in Cumberland – September 14th
 - Organics Facility Open House in Campbell River – September 21st
- Evaluate financial and resource implications
- Advisory and Board to consider long list of strategies.
- Bring ideas and options to the public for consideration through in-person and online consultation in 2025



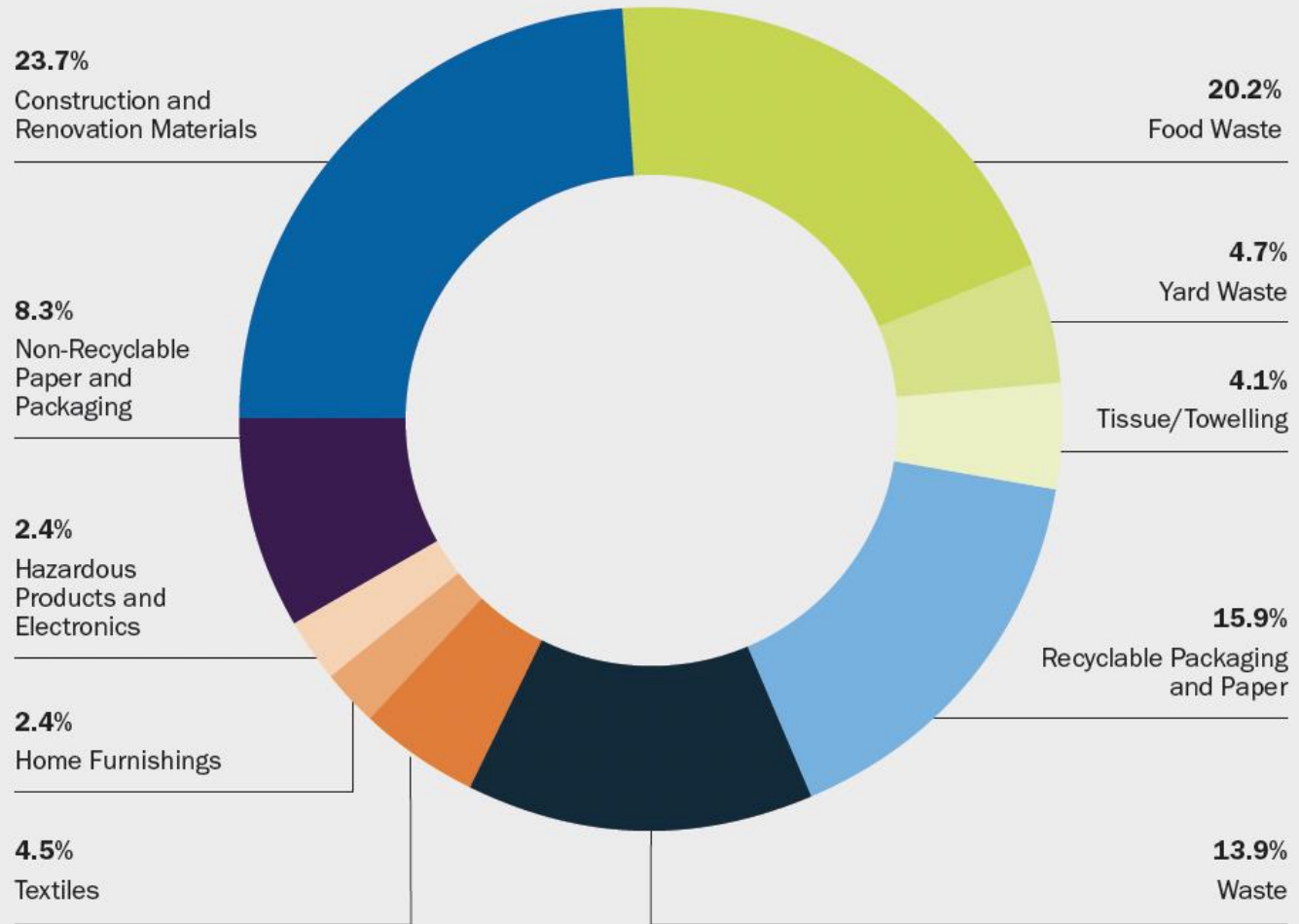
Questions?



Weigh In On Waste



2017 Waste Composition Study Results





The Corporation of the City of Courtenay

Staff Report

To: Council

File No.: 0390-20 UBCM

From: City Manager (CAO)

Date: June 12, 2024

Subject: 2024 UBCM Minister and Staff Meeting Requests

PURPOSE:

To receive direction from Council with respect to Minister and Ministry of Staff level meeting requests at the 2024 UBCM convention to address City of Courtenay Strategic Priorities, community issues and infrastructure projects.

BACKGROUND:

The 2024 UBCM Convention will take place September 16 – 20, 2024 in Vancouver, BC. In conjunction with the conference appointments are available with provincial government representatives and Ministers which provide an opportunity to discuss impactful community issues, share information on the City Strategic Priority projects and seek provincial support for key initiatives.

DISCUSSION:

Through the City's strategic planning processes and financial planning a series of priorities and projects have been developed on both an annual and longer term horizon. The UBCM convention meetings provide an opportunity to meet with Provincial Ministers and their staff as well as other provincially focused agencies and corporations that can assist the City in addressing community issues and priorities. Given input and discussions by Council and a scan of strategic priorities and projects the following meeting requests with Cabinet Ministers and MACC staff have been drafted for Council's consideration.

1. Minister of Housing: regarding Heart and Hearth program – meeting request to discuss Minister response regarding Heart and Hearth request: discuss housing conditions, share opportunities identified by the City and request funding support.
2. Minister of Mental Health and Addictions – meeting request to discuss:
 - a. Follow up to City letter dated March 18, 2024 requesting immediate prioritization of funding of treatment and medical detox beds in the Comox Valley, in alignment with the Substance Use Strategy and Walk with Me Report.
 - b. Funding and support to increase access to hygiene services for those experiencing homelessness including: operational and capital funding for overnight public washroom access, funding towards supported shower programs 7 days / week, and increased access to water.
3. Minister of Transportation and Infrastructure – meeting to discuss drainage management challenges in a shared watershed and seek support for implementation of the City Integrated Rainwater Management Plan.
4. Ministry of Housing - Share information on South Courtenay Sewer project and seek information on potential grant funding support
5. Ministry of Municipal Affairs – meeting request to:
 - a. Advocate for and request Provincial funding towards recreation infrastructure projects to address the growing infrastructure funding gap in municipal recreation facilities.
 - b. Discuss East Side Fire hall project access to grant programs

- c. Discuss implications for the cost of fire service infrastructure related to certification time limits requiring asset replacement
- 6. Island Health – meeting request to seek funding and support to increase access to hygiene services for those experiencing homelessness including: operational and capital funding for overnight public washroom access, funding towards supported shower programs 7 days / week, and increased access to water.
- 7. Ministry of Emergency Management and Climate Readiness – meeting to share information on Anderton Dyke replacement project and seek information on potential grant funding support

POLICY ANALYSIS:

Meeting requests reflect a number of policy documents and direction for the City including the City’s 2022-2026 Strategic Priorities, Annual and Five Year Financial Plan for Capital Projects, Official Community Plan, Water, Sewer and Parks and Recreation Master Plans as well as Asset Management Policies.

FINANCIAL IMPLICATIONS:

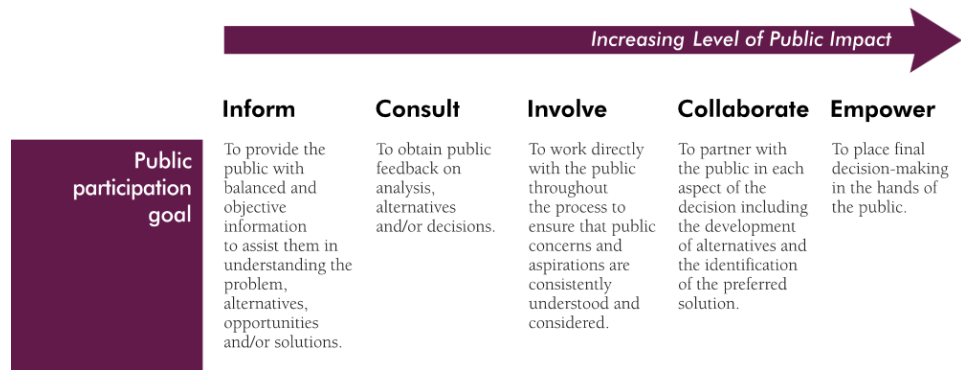
Attendance at UBCM by Council and UBCM meeting requests are accommodated through the current financial plan. Meeting topics directed toward grants have the potential for positive financial impact on the City’s capital plans.

ADMINISTRATIVE IMPLICATIONS:

UBCM Minister and Staff Meetings (request and materials) are supported and prepared by staff and departments across the organization lead by Legislative Services and the City Manager’s Office.

PUBLIC ENGAGEMENT:

Staff would inform the public based on the IAP2 Spectrum of Public Participation:



© International Association for Public Participation www.iap2.org

OPTIONS:

1. THAT Council direct staff to make application for Minister and Ministry Staff meetings (Council to provide specific direction)
2. THAT Council provide alternative direction to staff.

Prepared by: Geoff Garbutt, M.Pl., MCIP, RPP, City Manager (CAO)



The Corporation of the City of Courtenay

Staff Report

To: Council

File No.: 3090-20-2306/DVP00048

From: Director of Development Services

Date: June 12, 2024

Subject: Development Variance Permit Application No. 2306 – 1580 Fitzgerald Avenue

PURPOSE:

For Council to consider approval of Development Variance Permit No. 2306 to allow for the redevelopment of the existing property to permit the construction of a new church at 1580 Fitzgerald Avenue (LOT A, (DD P54316) SECTION 41, COMOX DISTRICT, PLAN 7449) within the property’s current Public Assembly One (PA-1) Zone.

BACKGROUND:

The property owners of 1580 Fitzgerald Avenue are seeking variances to the Zoning Bylaw’s Public Assembly One (PA-1) zone, sections 8.26.2, 8.26.5 and 8.26.10, to construct a new church on the property identified in Figure 1 Subject Property and Figure 2 Site location

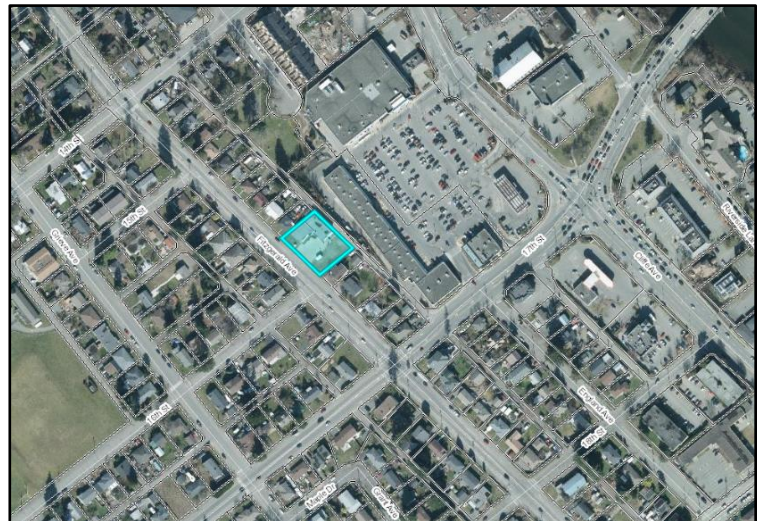
The Development Variance Permit application is requesting variances to setbacks and landscape buffers in the PA-1 zone to support the design of a new two storey church with sanctuary, hall, kitchen, meeting rooms and offices (Figure 4). This includes variance requests to the zone’s minimum lot size, rear and side yard setbacks and its side and rear yard landscaping area requirements.

The property is designated *Institutional* in the Official Community Plan (OCP). Single residential dwelling lots designated as *Urban Residential* adjoin the site’s property lines to the north and south. Fitzgerald Avenue runs along its east facing frontage and a City owned lane along the west property line.

Figure 1: Subject Property



Figure 2: Site location - 1580 Fitzgerald



The property drops approximately 2 metres in elevation from its property line along Fitzgerald Avenue, to the rear property line. The property also has a similar elevation change dropping north to south, making the southeast corner of the lot the low point of the property.

The property's existing church (Figure 3) is sited toward the north half of the lot with a paved parking lot located directly off Fitzgerald, west and south from the church. The existing building is legally non-conforming to some of the zone's setbacks and landscape requirements and has stood in its current form since the mid-1980s.

Beyond the existing building and parking lot, the property primarily consists of previously disturbed but un-manicured lawn and gravel. The property does not currently contain any mature trees.

The site previously contained Schedule 2 uses and is registered in the Provincial Contaminated Site Registry. In 2016, the site was issued a Certificate of Compliance (from what is now the Ministry of Environment and Climate Change Strategy) indicating that the site had been satisfactorily remediated and that development could be allowed as long as future proposals still met the requirements and conditions specified in the Certificate of Compliance.

Due to the site's institutional use, a Development Permit will not be required. If Council approves the requested variances, the owner has submitted a Building Permit Application that can be issued subsequently.

Figure 3: Existing Site from Fitzgerald Avenue



Figure 4: Rendering of Proposed Development from Fitzgerald Avenue



DISCUSSION:

The PA-1 zone determines density through lot coverage whereas the maximum allowed is 40% of the total lot area for all *buildings* and *accessory buildings*. The current proposal has a lot coverage of 27.1%. Therefore, the variances do not affect the property’s density or use and no rezoning is required to facilitate the proposed development.

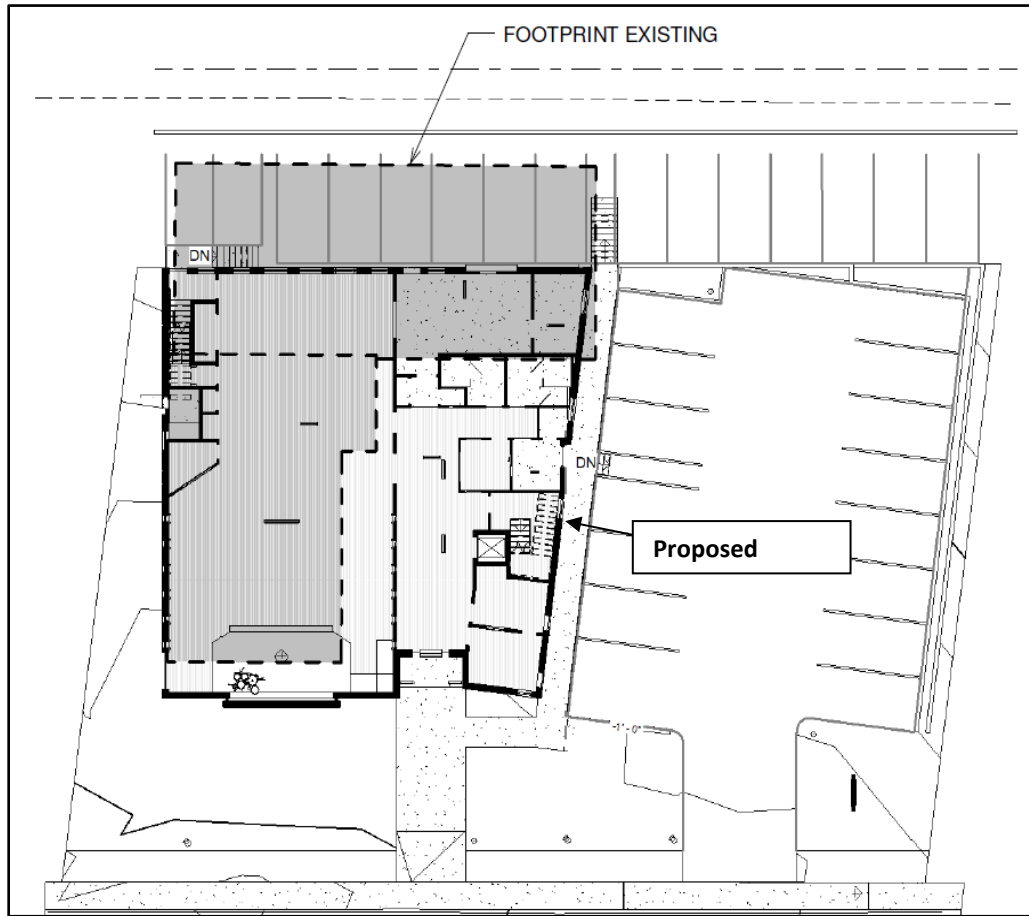
The OCP designates this block as *Urban Residential* and consists primarily of single-residential dwellings. The institutional use was retained in the OCP for the church property. The church will provide community services that are sought within *Urban Residential* neighbourhoods and a mix of compatible uses is recognized in the OCP as important for creating complete, vibrant neighbourhoods.

The three lots (1570, 1580 and 1590 Fitzgerald Avenue) were consolidated in 1985 that now form the subject property. Following consolidation, a 3,000 ft² addition was approved by the City that permitted the construction of the church as it stands today. This building expansion was approved well before Courtenay’s current zoning bylaw was adopted and is legal non-conforming.

The requested variances are largely to accommodate the required off-street parking while still allowing a building large enough to meet the needs of the Salvation Army and its community members. As shown in the Figure 5 below where the dashed line is the existing building footprint and the solid line is the proposed building footprint, the requested variances allow the building to be sited on the property so that:

- Additional parking is created in the rear along the lane, below the frontage and out of public sight.
- The current on-site parking lot is shifted south and east, recapturing landscaped area inside the the Fitzgerald Avenue property line.
- The building shifts slightly toward the Fitzgerald Avenue but where the building is tapered to be narrower in the front and wider in the rear.
- The building is largely redeveloped over the curret footprint where it can utilize existing excavation and work within the development restrictions relating to the site’s contamination.

Figure 5: Existing Footprint over Proposed Footprint



The application looks to vary Zoning Bylaw No. 2500 Section PA-1:

1. Section 8.26.2 minimum lot size from an area not less than 2,500m² to an area not less than 1,600m²,
2. Section 8.26.5(2) rear yard setback from 10m to 6.8m,
3. Section 8.26.5(3) side yard setback from 9.0m to 1.2m on the west side yard,
4. Section 8.26.10(1) landscaping area from 7.5m to 0.7m on the west side yard, and from 7.5m to 1.0m on the east side yard and,
5. Section 8.26.10(2) from 6.0m to 0.0m in the rear yard adjoining the lane.

Many of the variance requests do not deviate significantly from the long standing, non-conforming siting of the existing church. The new church is largely being redeveloped on the footprint of the existing church. Table No. 1 below identifies the PA-1 zone regulations, the proposed variances and the existing Church legal non-conforming siting.

Table 1 - Proposed Variances

Public Assembly One (PA-1)	Required	Proposed	Existing
Lot Coverage	40%	27.1%	--
Minimum Lot Frontage	45m	45.7m	--
Building Height	12m	10m	--
Minimum Lot Size	Not less than 2,500m ²	Not less than 1,600m ²	1,658m ²
Front Yard Setback	7.5m	7.5m	--
Rear Yard Setback	10m	6.8m	1.66m
Side Yard Setback (North West Side)	9.0m	1.2m	1.1m
Side Yard Setback (South East Side)	9.0m	21.4m	--
Front Yard Landscape Area	6.0m	6.0m	--
Side Yard Landscape Area (North West Side)	7.5m	0.7m	1.14m
Side Yard Landscape Area (South East Side)	7.5m	1.0m	--
Rear Yard Landscape Area	6.0m	0.0m	1.66m
Parking Required	23	29	--

Site Contamination

Regulation of contaminated sites is the jurisdiction of the province (Ministry of Environment and Climate Change Strategy). A “risk based” Certificate of Compliance was issued in 2016. The Certificate of Compliance represented a provincial decision to allow development to proceed on-site, with conditions, based on contamination that was first identified in December 2010 while removing underground heating oil storage tanks. The tanks serviced a previously demolished single detached residential dwelling.

The ministry responsible has confirmed that the Certificate of Compliance is still valid and that it is the responsibility of the municipality to review the Certificate of Compliance and ensure that the development proposal meets the land use and development requirements required by the Certificate. The applicant has provided a report by a qualified professional that opines that the proposed development meets the conditions of the Certificate of Compliance (**Attachment No. 5**).

No substantial changes to the site plan will be allowed without an amendment to the Development Variance Permit and re-review of the Certificate of Compliance. Adherence to the site plan and review by an environmental consultant will be a condition of the Development Variance Permit and subsequent the Building Permit, if they are to be issued.

Infrastructure

The property is already connected to City sewer, water and storm water services. As part of their Building Permit application, the applicant will be required to pay for modeling for sanitary and water flow to determine if any off-site infrastructure improvements are necessary to permit the development.

The City is also requiring a lane condition assessment for the rear lane to review the need for potential frontage improvements in this public right-of-way.

Landscaping

Based on the cost estimate provided by Arianne Huene Landscape Design dated November 14, 2023, a Landscape Security for \$51,044.00 will need to be paid prior to the issuance of a Building Permit. This is a condition of Development Variance Permit No. 2306.

The Landscape Plan shows 5 trees to be planted, whereas the Tree Density Target (TDT) for this site is 8 trees. Because no Tree Cutting Permit is required for this proposal, the *Tree Protection and Management Bylaw No. 2850* does not come into effect. Instead, staff rely on other policies such as our OCP and Urban Forest Strategy to encourage additional tree planting where possible. Staff inquired about the potential of additional trees to be planted but the applicant relayed that they had operational needs for open spaces for the various programs they offer. They prefer open sight lines throughout the property so the ‘eyes on the street’ principle of CPTED can be applied. Staff support this rationale.

Sign Permits

The proposed redevelopment includes signage. A separate Sign Permit will need to be obtained in compliance with Sign Bylaw No. 2760.

POLICY ANALYSIS:

Official Community Plan Review

Located in west central Courtenay, the subject property is designated Institutional within the Official Community Plan (OCP).

If the variances are approved, the proposed redevelopment would support the realization of numerous OCP policies for Institutional land use including:

1. Support integration of civic and institutional uses within centres and neighbourhoods.
2. Support limited office, commercial, and residential uses that are associated with and accessory to institutional uses.
3. Enable the flexible and appropriate adaptive use of institutional lands to meet evolving community needs.

Cycling Network

Located along Fitzgerald Avenue, the subject property directly fronts a portion of Courtenay’s Cycling Network. Fitzgerald Avenue currently operates as a ‘Bike Lane’ and is envisioned to remain a permanent component of the Recommended Long Term Cycling Network, including short-to-medium term upgrades that would see Fitzgerald Avenue as a buffered or painted bike lane.

Bus Stop Improvement

According to the Comox Valley Regional District, the existing BC Transit bus stop along the subject property’s frontage would benefit from a bus shelter and other potential amenities. City of Courtenay Staff

may request transit/bus stop improvements as part of the *works and services* required by the Building Permit.

FINANCIAL IMPLICATIONS:

In accordance with *Local Government Act* section 561, Development Cost Charges (DCCs) are not payable for places of public worship.

ADMINISTRATIVE IMPLICATIONS:

Processing Development Variance Permit applications is a statutory component of the corporate work plan and a core duty of the Development Services Department. Work to date has primarily been carried out by Development Services staff, although other departments have provided referral comments.

STRATEGIC PRIORITIES REFERENCE:

This initiative addresses the following strategic priorities:

- Good Governance - Review and streamline development process and set targets for application processing times

PUBLIC ENGAGEMENT:

Per *Development Procedures Bylaw No. 2106, 2023*, no Community Information Meeting (CIM) is required for this Development Variance Permit Application.

The Salvation Army, the property owner and operator of the church, voluntarily held a public presentation on Wednesday, January 31, so that members of the public could see the proposed redevelopment and ask questions.

In accordance with *Local Government Act Section 499*, Staff mailed a public notification to 18 to property owners and occupants within a 30 metre area on May 31, 2024 (**Attachment No. 4**). Zero (0) public comments have been received at time of writing this report. Two public inquiries about the potential redevelopment were fielded by Staff prior to the official public notification being mailed. Comments received after this report will be presented to Council at the Regular Council meeting.

Neighbour Support

In consideration of the impact the requested variances could have on adjacent property owners, it is noteworthy that both of property owners directly adjacent to the subject property have provided letters of support for the development as proposed (**Attachment No. 3**).

OPTIONS:

1. THAT Council vary the following sections of Zoning Bylaw No. 2500, 2007:
 - a) Section 8.26.2 – *Minimum Lot Size* – A lot shall have an area of not less than, from 2,500m² to 1,600m²;
 - b) Section 8.26.5(2) – *Rear Yard Building Setback* – from 10.0m to 6.8m;
 - c) Section 8.26.5(3) – *Side Yard Building Setback* – for the north west side yard from a minimum of 9.0m to 1.2m;
 - d) Section 8.26.10(1) – *Landscape and Screening Requirements* – where a lot adjoins a residential zone, a minimum landscaped area shall be provided along the inside of all affected property lines from 7.5 m to 0.7 m for the north west side property line and from 7.5 m to 1.0m for the south east side property line; and

e) Section 8.26.10(2) – Landscape and Screening Requirements – where a lot adjoins a street, a landscaped area shall be provided along the entire frontage of the property facing the street, from 6.0m to 0.0m.”

THAT Council approve Development Variance Permit No. 2306 (1580 Fitzgerald Avenue) which would allow for the redevelopment of the property to construct a new church and parking subject to the following conditions:

- a) the payment of a landscape security in the amount of \$51,044.00 prior to the issuance of a Building Permit; and
 - b) no substantial changes to the site plan be permitted without an amendment to the Development Variance Permit and re-review of the Certificate of Compliance.
2. THAT Council defer and request additional information from staff for “Development Variance Permit No. 2306” (1580 Fitzgerald Avenue) through a resolution.
 3. THAT Council deny the application for “Development Variance Permit No. 2306” (1580 Fitzgerald Avenue).

ATTACHMENTS:

Attachment No. 1 – Draft Development Variance Permit No. 2306

Attachment No. 2 – Proposed Site Plan

Attachment No. 3 – Neighbour Letters of Support

Attachment No. 4 – Public Notification Mailout

Attachment No. 5 – Site Contamination – Site Design Confirmation Letter

Prepared by: Jacob Cramer, Planner 1 – Development Planning

Reviewed by: Marianne Wade, RPP, MCIP, Director of Development Services

Nancy Gothard, RPP, Manager of Community and Sustainability Planning

Concurrence: Geoff Garbutt, M.Pl., MCIP, RPP, City Manager (CAO)

THE CORPORATION OF THE CITY OF COURTENAY

Permit No. 3090-20-2306/DVP00048

DEVELOPMENT VARIANCE PERMIT

June 12, 2024

To issue a Development Variance Permit

To: Name: THE GOVERNING COUNCIL OF THE SALVATION ARMY IN CANADA
Address: 2 OVERLEA BLVD, TORONTO, ON, M4H 1P4

Property to which permit refers:

Legal: LOT A, (DD P54316) SECTION 41, COMOX DISTRICT, PLAN 7449

Civic: 1580 Fitzgerald Avenue

Conditions of Permit:

Permit issued to the property legally described as LOT A, (DD P54316) SECTION 41, COMOX DISTRICT, PLAN 7449 to allow the construction of a church with the following variances to the City of Courtenay Zoning Bylaw No. 2500, 2007:

- *Section 8.26.2 – Minimum Lot Size – A lot shall have an area of not less than, from 2,500m² to 1,600m².*
- *Section 8.26.5(2) – Rear Yard Building Setback – from 10.0m to 6.8m.*
- *Section 8.26.5(3) – Side Yard Building Setback – for the north west side yard from a minimum of 9.0m to 1.2m.*
- *Section 8.26.10(1) – Landscape and Screening Requirements – where a lot adjoins a residential zone, a minimum landscaped area shall be provided along the inside of all affected property lines from 7.5m to 0.7m for the north west side property line and from 7.5m to 1.0m for the south east side property line.*
- *Section 8.26.10(2) – Landscape and Screening Requirements – where a lot adjoins a street, a landscaped area shall be provided along the entire frontage of the property facing the street, from 6.0m to 0.0m.*

Development Variance Permit No. 2306 is also subject to the following conditions:

1. Development must be in conformance with the site plan, elevations and associated project data, dated February 13, 2024, by Philippa Atwood Architecture in ***Schedule No.1.***
2. Development must be in substantial conformance with the landscape plan by Arianne Huene Landscape Design dated November 14, 2023 in ***Schedule No. 2.***

3. Submission of landscape security prior to building permit issuance in the amount of \$51,044.00 (\$40,835.00 X 125%). The amount is calculated based on the November 14, 2023 cost estimate by Huene Landscape Design, as attached in *Schedule No. 3*.
4. That no substantial changes to the site plan be permitted without an amendment to the Development Variance Permit and re-review of the Certificate of Compliance.

Time Schedule of Development and Lapse of Permit

That if the permit holder has not substantially commenced the construction authorized by this permit within (24) months after the date it was issued, the permit lapses.

Date

Director of Development Services



* Landscaping shown is for visualization purposes only. See Landscape Plan for detailed information.

1580 FITZGERALD

SALVATION ARMY - COMOX VALLEY COMMUNITY CHURCH

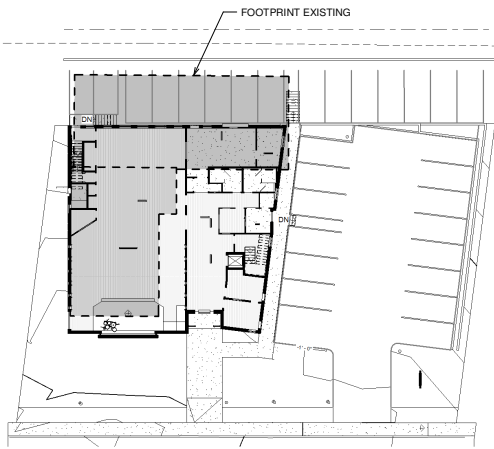
Scale
Date Issue Date 2023-11-20 11:03:43 AM

DP000
Project number Sheet

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Schedule No. 1

Phi Architecture Inc.
 Philipp Atwood Architect, AIBC, MRAC, LEED AP
 5 Little Bear Way Royston BC V0R 2V0
 philippa@atwoodarchitect.com
 250.705.0433 / 250.218.0724

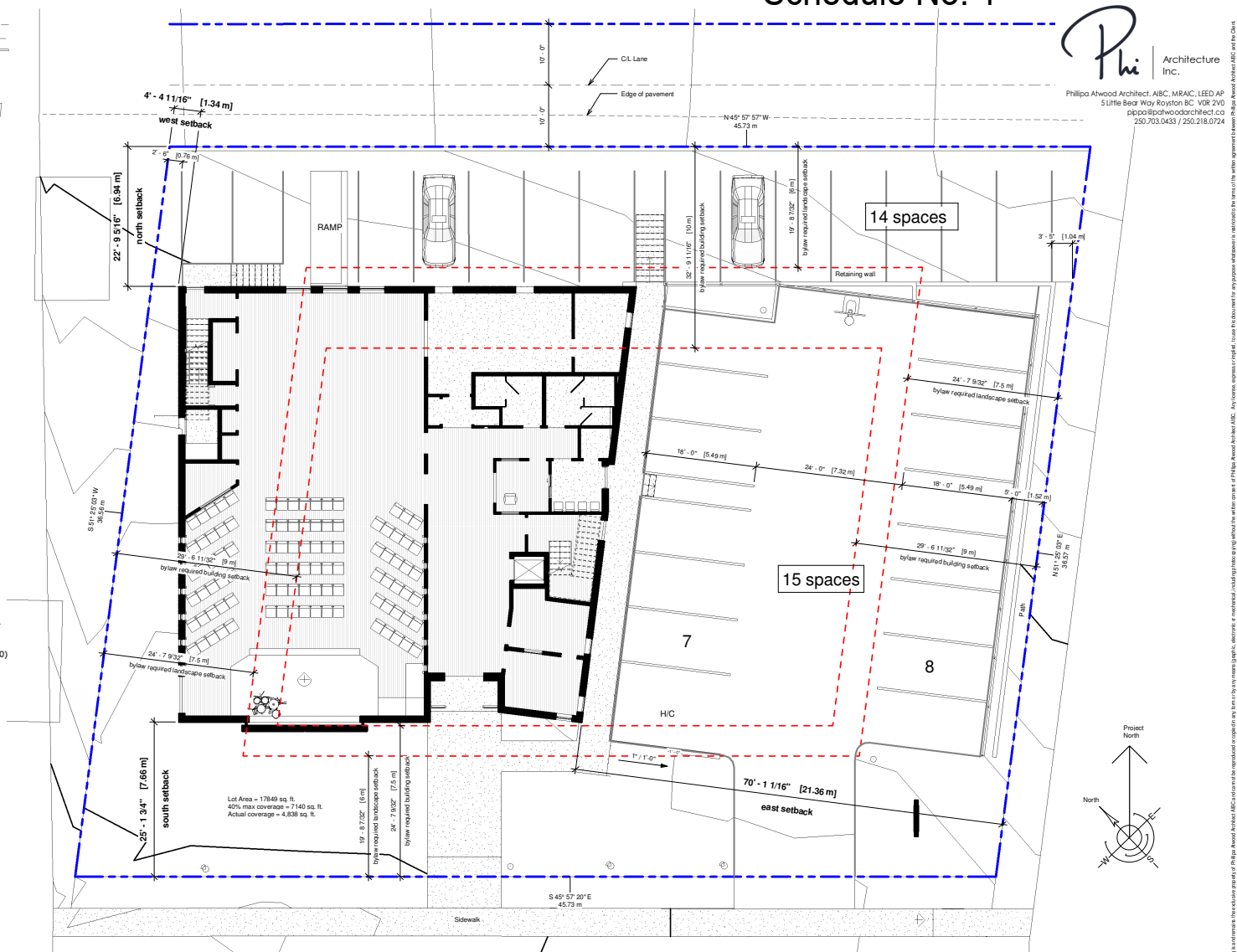


1 EXISTING SITE PLAN COMPARISON
 1" = 20'-0"

SALVATION ARMY
COMOX VALLEY COMMUNITY CHURCH
 1580 Fitzgerald Avenue, Courtenay

Zoning Compliance

Description	New two storey church building with Sanctuary, Hall, kitchen, meeting rooms, offices and ancillary spaces with surface parking.																
AHJ	City of Courtenay																
Legal Address	Lot A, Section 41, Comox District, Plan 7449																
Civic Address	1580 Fitzgerald Ave., Courtenay (Includes 1570, 1580 and 1590)																
Zoning	Public Assembly (PA-1)																
Permitted Use	Church and Religious Centre																
DP Area	None (Adjacent to Downtown & Multi-Residential)																
Floor Area	Main Floor	432 m ² (4649 sq ft)															
	Upper Floor	318 m ² (3420 sq ft)															
	Total	750 m ² (8069 sq ft)															
Area of Lot	1,658 m ²																
Building Footprint	450 m ²																
Lot Coverage	Permitted	40 %															
	Proposed	27.1 %															
Building Height	Permitted	12 m															
	Proposed	10 m															
Building Setbacks	<table border="1"> <tr> <th></th> <th>Required</th> <th>Proposed</th> </tr> <tr> <td>Front</td> <td>7.5 m</td> <td>7.5 m</td> </tr> <tr> <td>*Rear</td> <td>10.0 m</td> <td>6.94 m (Existing 1.66m)</td> </tr> <tr> <td>*West Side</td> <td>9.0 m</td> <td>1.34 - 4 m (Existing 1.14m)</td> </tr> <tr> <td>East Side</td> <td>9.0 m</td> <td>21.4 m</td> </tr> </table>			Required	Proposed	Front	7.5 m	7.5 m	*Rear	10.0 m	6.94 m (Existing 1.66m)	*West Side	9.0 m	1.34 - 4 m (Existing 1.14m)	East Side	9.0 m	21.4 m
	Required	Proposed															
Front	7.5 m	7.5 m															
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*West Side	9.0 m	1.34 - 4 m (Existing 1.14m)															
East Side	9.0 m	21.4 m															
Landscaping	<table border="1"> <tr> <th></th> <th>Required</th> <th>Proposed</th> </tr> <tr> <td>Front</td> <td>6.0 m</td> <td>6.0 m</td> </tr> <tr> <td>*Rear</td> <td>6.0 m</td> <td>0.0 m (Existing 1.66m)</td> </tr> <tr> <td>*West Side</td> <td>7.5 m</td> <td>0.76 - 4 m (Existing 1.14m)</td> </tr> <tr> <td>*East Side</td> <td>7.5 m</td> <td>1.04 m</td> </tr> </table>			Required	Proposed	Front	6.0 m	6.0 m	*Rear	6.0 m	0.0 m (Existing 1.66m)	*West Side	7.5 m	0.76 - 4 m (Existing 1.14m)	*East Side	7.5 m	1.04 m
	Required	Proposed															
Front	6.0 m	6.0 m															
*Rear	6.0 m	0.0 m (Existing 1.66m)															
*West Side	7.5 m	0.76 - 4 m (Existing 1.14m)															
*East Side	7.5 m	1.04 m															
Parking	Church	1 space per 4 seats = 90 / 4 = 23 spaces															
	Proposed	29 parking spaces															
	Small spaces allowed = 10% if more than 20 spaces = 2																
	Accessible parking required = 1 space																
Loading	Required	None															
	Proposed	None															
	* Variances required																



2 SITE PLAN DP
 1/8" = 1'-0"

FITZGERALD

SALVATION ARMY - CV COMMUNITY CHURCH

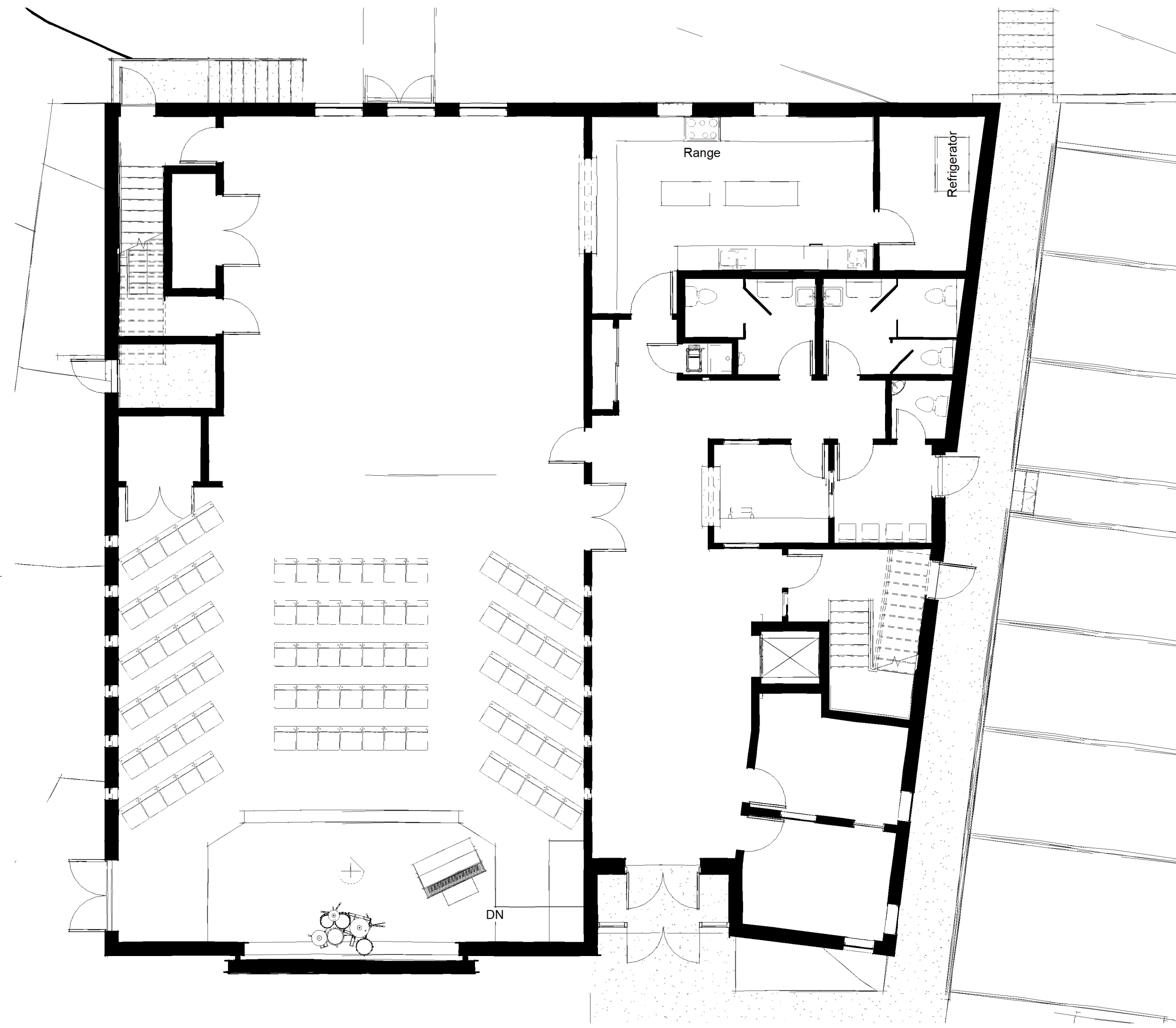
REVISED 13FEB2024
 LANDSCAPE SETBACKS

DP001

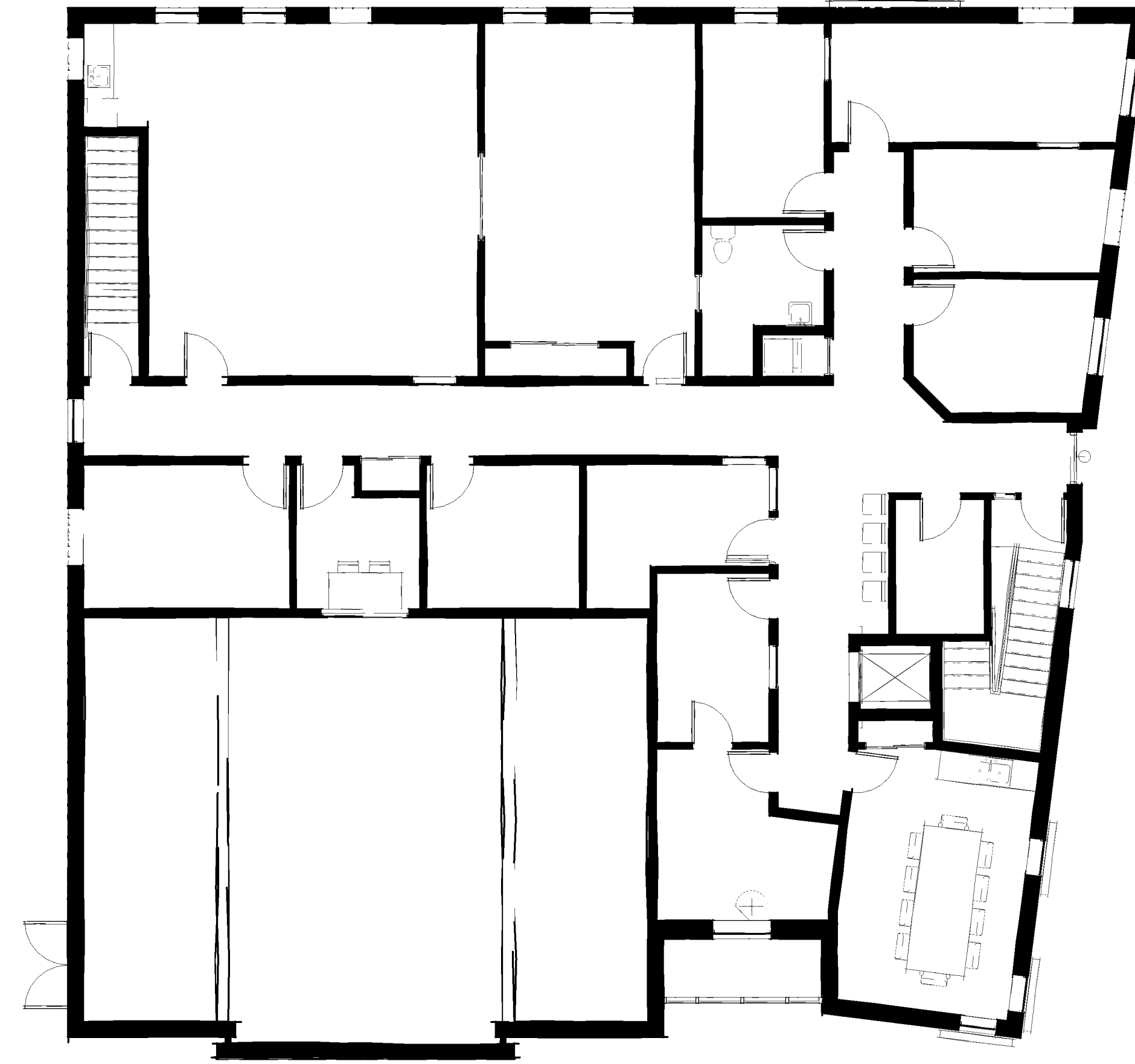
Project number | Sheet

Scale	As indicated	Plot date:
Date	Issue Date	2024-02-13 12:57:28 PM

1 MAIN DP
1/8" = 1'-0"



2 SECOND DP
1/8" = 1'-0"

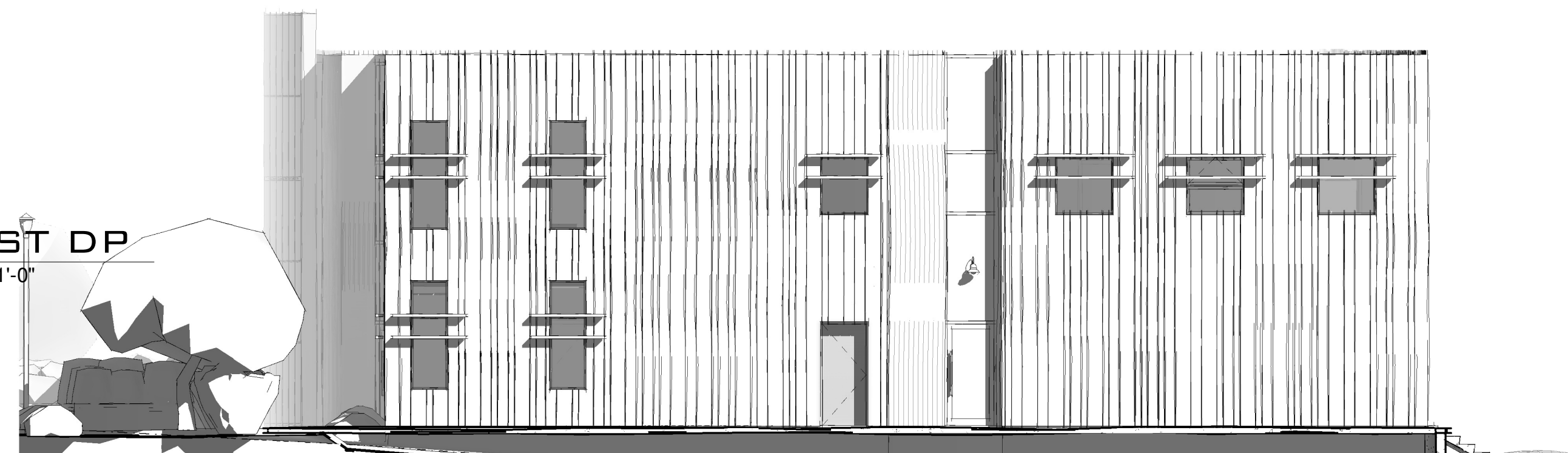


FLOOR AREAS:
Main 4631 sq. ft.
Second 3448 sq. ft.
TOTAL = 8079 sq. ft.

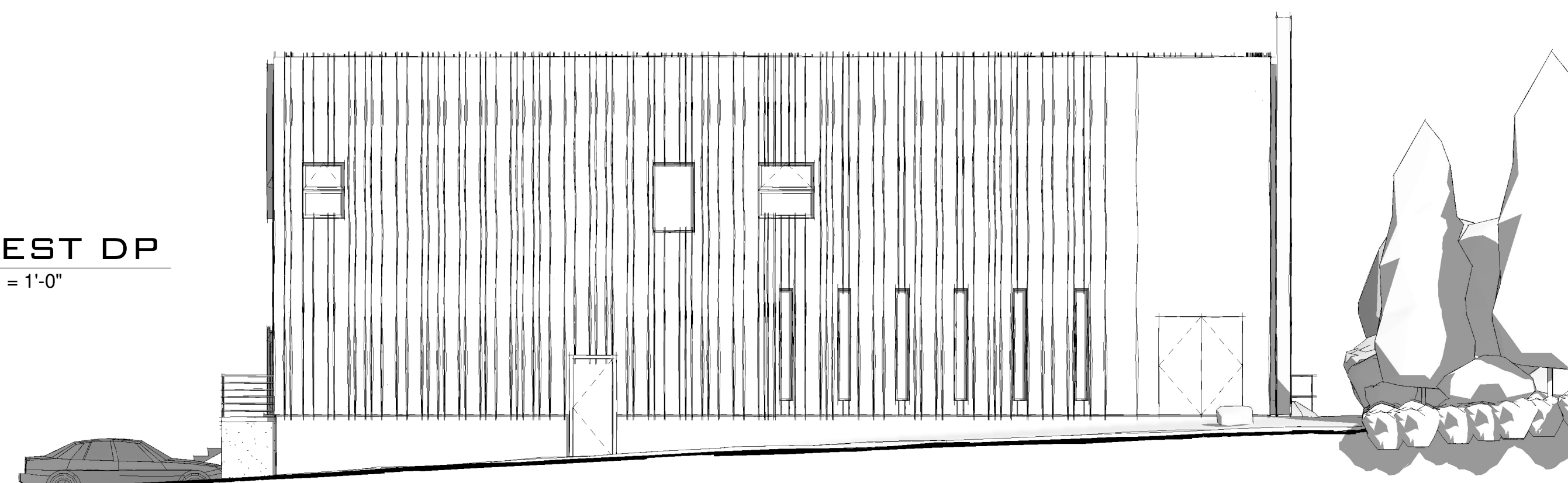
3 SOUTH DP
1/8" = 1'-0"



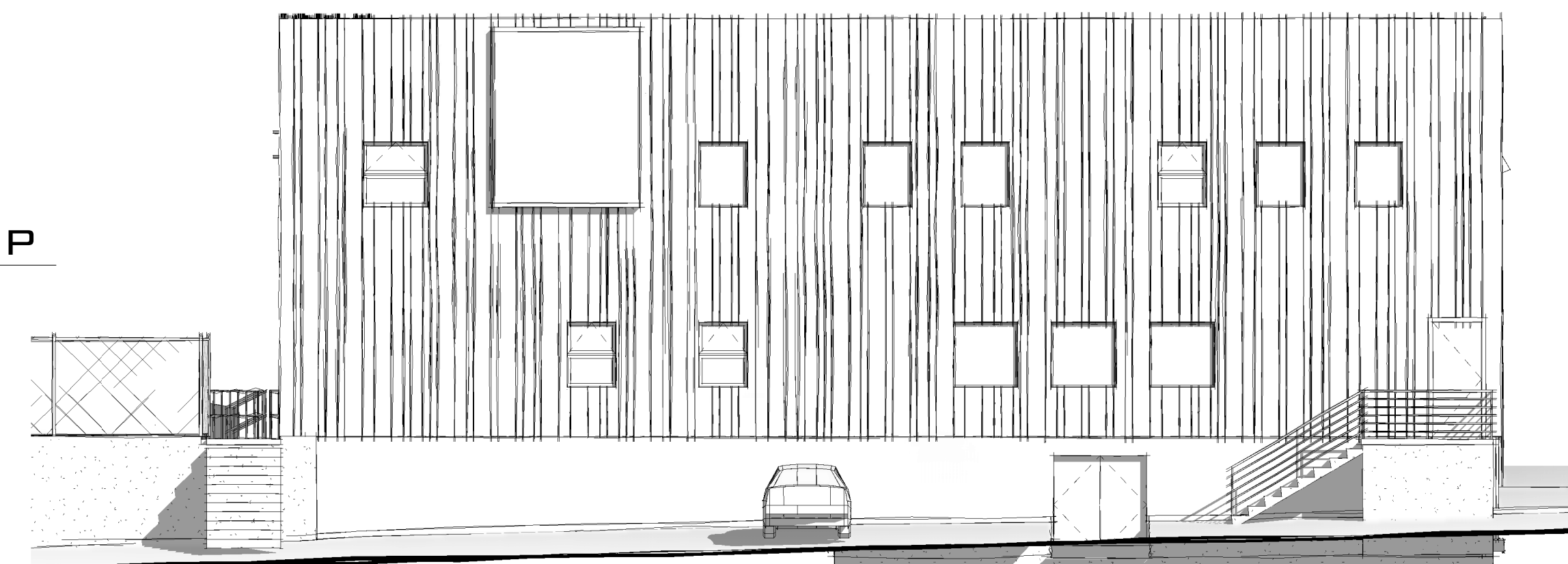
4 EAST DP
1/8" = 1'-0"



6 WEST DP
1/8" = 1'-0"



5 NORTH DP
1/8" = 1'-0"

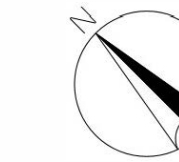




PROPERTY AREA
= 1657.95 SQ M (17,846 SQ FT)

LANDSCAPED AREA OF PROPERTY
= 368.36 SQ M (3,965 SQ FT)

Schedule No. 2



WALL AS PER BUILDING PLANS

BENDER BOARD EDGING ALONG
NORTH PROPERTY LINE
- 25CM X 10CM X 6M BOARDS
(1" X 4" X 20' BOARDS)

RIVER ROCK OVER GEOTEXTILE FABRIC
- RIVER ROCK DIAMETER
= 5CM / 2" TO 30CM / 12"

DECORATIVE ANGULAR
CUMBERI AND SANDSTONE BOULDERS
- PLACED POINTED VERTICALLY
- 45CM / 18" DIAMETER TO 12M / 48" DIAMETER

SPECIMEN TREES
2 COLUMNAR EASTERN WHITE PINE
AND TEMPELHOF JAPANESE PINE
- ANCHORS BUILDING WITHIN LANDSCAPE
- UNDERPLANTED WITH KINKIKNIK GROUND
COVER AND GULF STREAM NANDINA

15M / 5' BORDER OF RIVER ROCK
IN MUNICIPAL SETBACK
- SIDEWALK SNOW REMOVAL

*** OPTIONAL LANDSCAPE FEATURE ***
SQUARE CORTEN PLANTERS, 2.4M X 2.4M X 0.6M
(8' W X 8' L X 2' H)
- CREATES FOCAL FEATURES AND FRAMES BUILDING
- DROUGHT TOLERANT HARDY YUCCA & HAMELN BUNNY TAILS GRASS
- PEA GRAVEL MULCH

NOTES

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1.800.474.2886 - www.onecall.ca

PROPERTY NOTES

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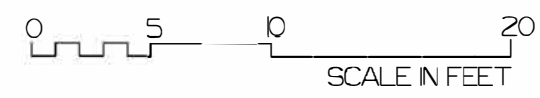
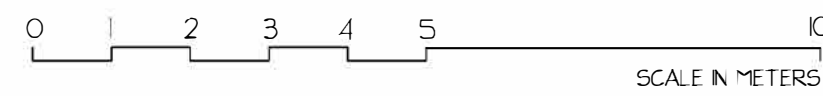
No.	Date	Description



ARIANNE HUENE LANDSCAPE DESIGN
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arianne@ariannehuenelandscapedesign.ca

SALVATION ARMY
COMOX VALLEY
COMMUNITY CHURCH
1570 Fitzgerald Ave
Courtenay, BC V9N 2S2

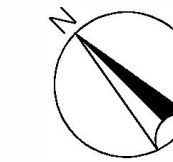
SCALE: 1" = 8'	PROJECT NO: LANDSCAPE PLAN
DRAWN BY: A. Huene	SHEET NO: 1 / 4
CHECKED BY:	
DATE: Nov 6, 2023	
DATE OF PRINT: Nov 14, 2023	



PROPERTY AREA
= 1657.95 SQ M (17,846 SQ FT)

LANDSCAPED AREA OF PROPERTY
= 368.36 SQ M (3,965 SQ FT)

Schedule No. 2



WALL AS PER BUILDING PLANS

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DECORATIVE ANGULAR
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- PLACED POINTED VERTICALLY
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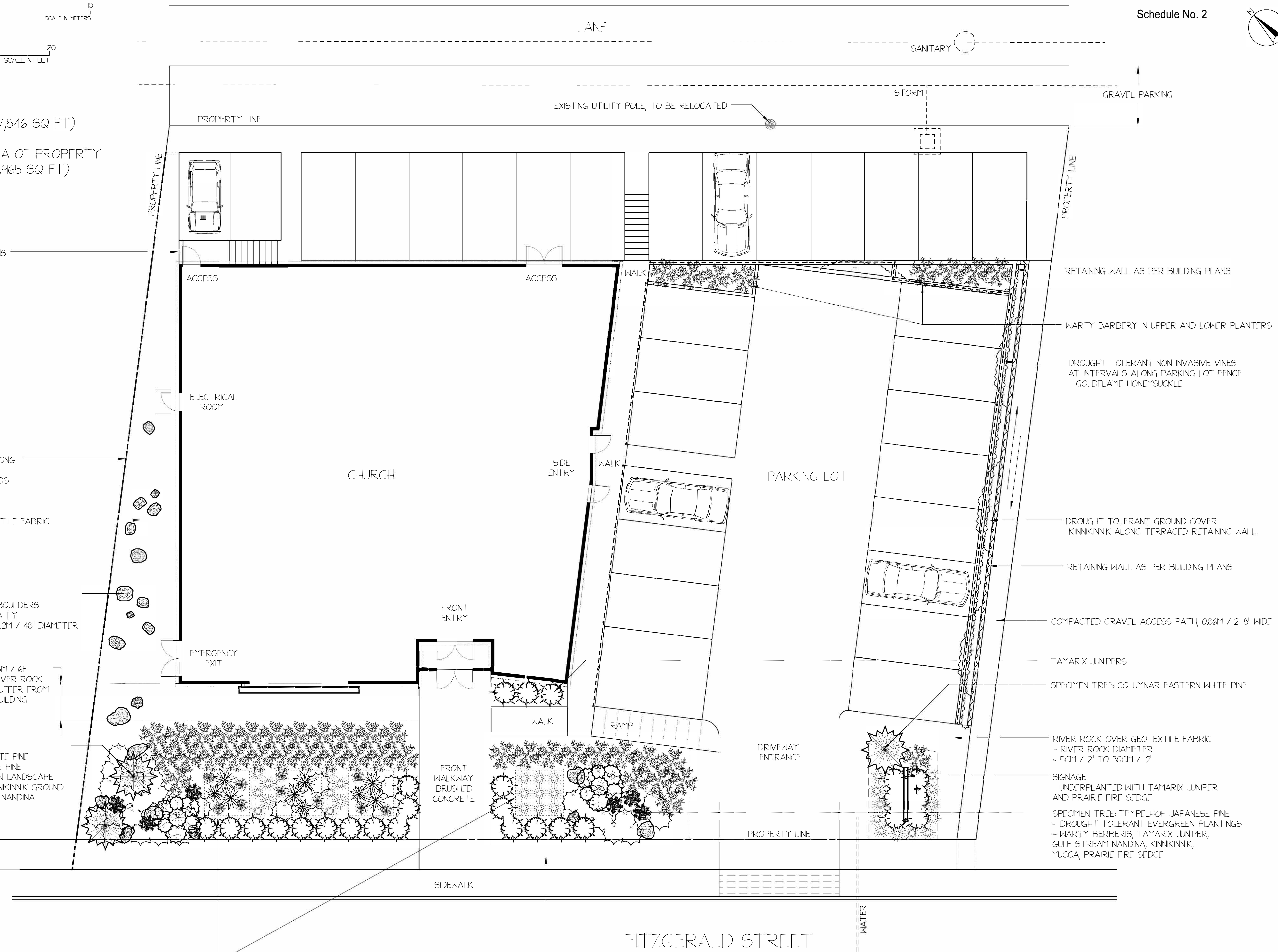
1.8M / 6FT
RIVER ROCK
BUFFER FROM
BUILDING

SPECIMEN TREES:
2 COLUMNAR EASTERN WHITE PINE
AND TEMPELHOF JAPANESE PINE
- ANCHORS BUILDING WITHIN LANDSCAPE
- UNDERPLANTED WITH KINKIKNIK GROUND
COVER AND GULF STREAM NANDINA

1.5M / 5FT
RIVER ROCK
MUNICIPAL
SETBACK

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1.5M / 5' BORDER OF RIVER ROCK
IN MUNICIPAL SETBACK
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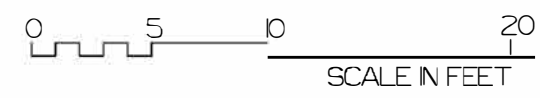
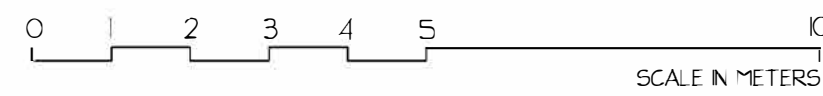
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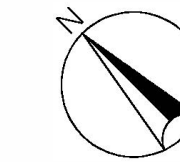
SALVATION ARMY
COMOX VALLEY
COMMUNITY CHURCH
1570 Fitzgerald Ave
Courtenay, BC V9N 2S2

SCALE: 1" = 8'	PROJECT NO: LANDSCAPE PLAN
DRAWN BY: A. Huene	SHEET NO: 1 / 4
CHECKED BY:	
DATE: Nov 6, 2023	
DATE OF PRINT: Nov 14, 2023	



ALL MEASUREMENTS INDICATED IN METERS (METRIC)

Schedule No. 2



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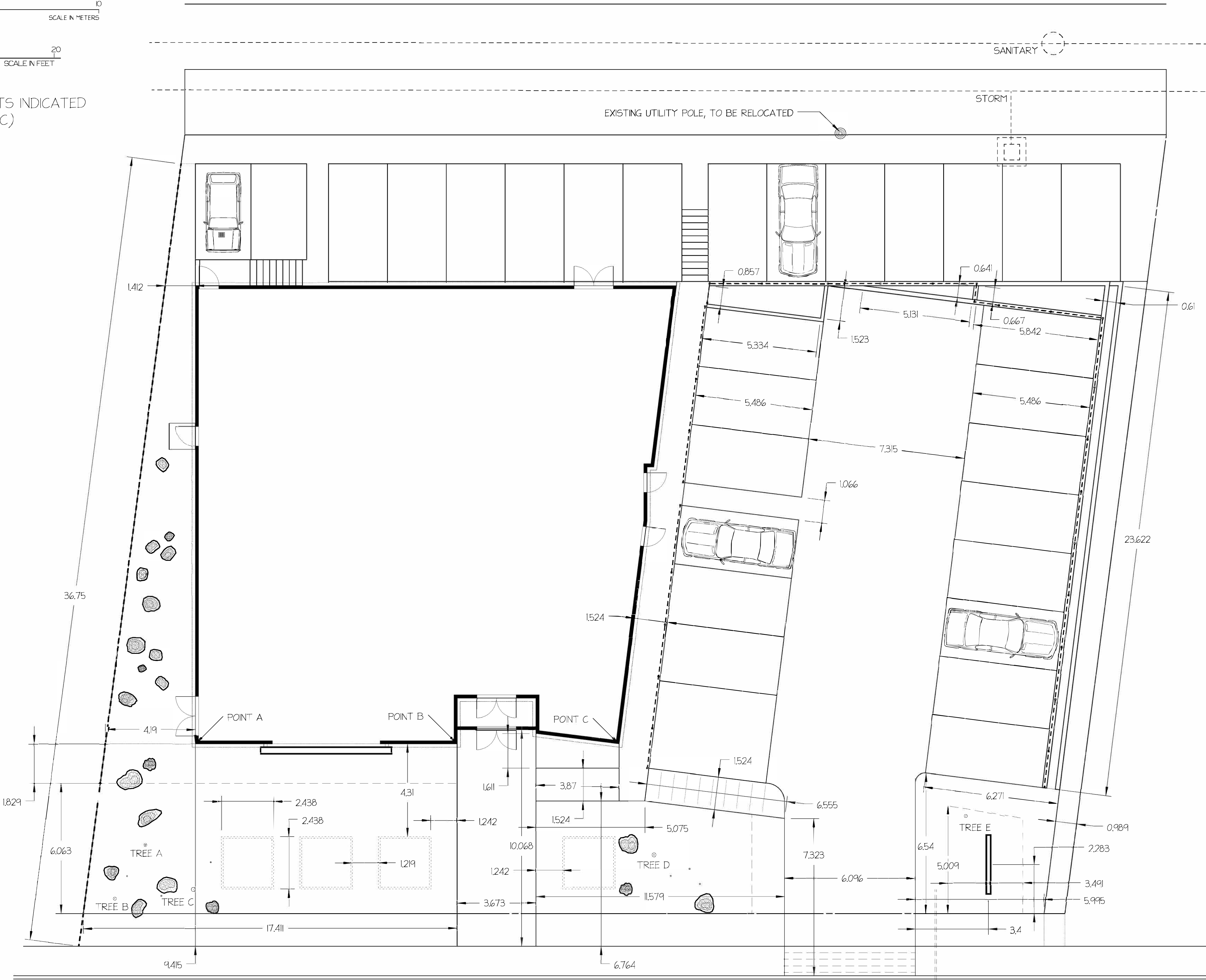
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CANADIAN LANDSCAPE STANDARDS

SECTIONS OF NOTE:

SOL PREPARATION NOTES:

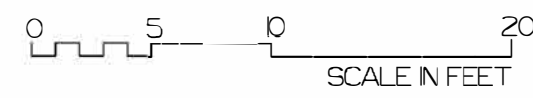
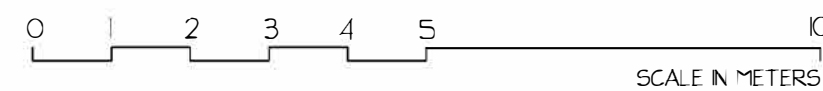
5.1.3. Growing medium quality should adhere to the requirements within this section.
5.1.5. Growing medium should be tested and certified in accordance with this section.
5.3.3.2. Compacted subgrades shall be scarified to a minimum depth of 15cm (6in.) immediately before placing the growing medium.
5.3.3.4. Growing media shall be placed and spread with suitable equipment and in a manner that does not adversely affect its structure.
5.3.3.5. Placed growing media shall be allowed to settle or shall be compacted by light rolling such that it is firm against deep footprints prior to planting. Compaction shall not be more than is necessary to meet this requirement.

TABLE T- 5.3.5.5. Minimum depths of growing media
- For small shrubs, and for all trees, 45 cm (18 in.) of growing media is required.
(30 cm / 12" of additional soil on top of 15cm / 6" existing subsoil)

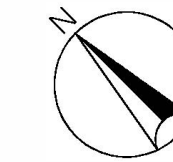
TREE TRIANGULATION

TREE A = POINT A = 5.231 M, POINT B = 15.249 M
TREE B = POINT A = 8.114 M, POINT B = 17.483 M
TREE C = POINT A = 6.769 M, POINT B = 14.018 M
TREE D = POINT B = 10.524 M, POINT C = 5.411 M
TREE E = POINT B = 23.945 M, POINT C = 16.510 M

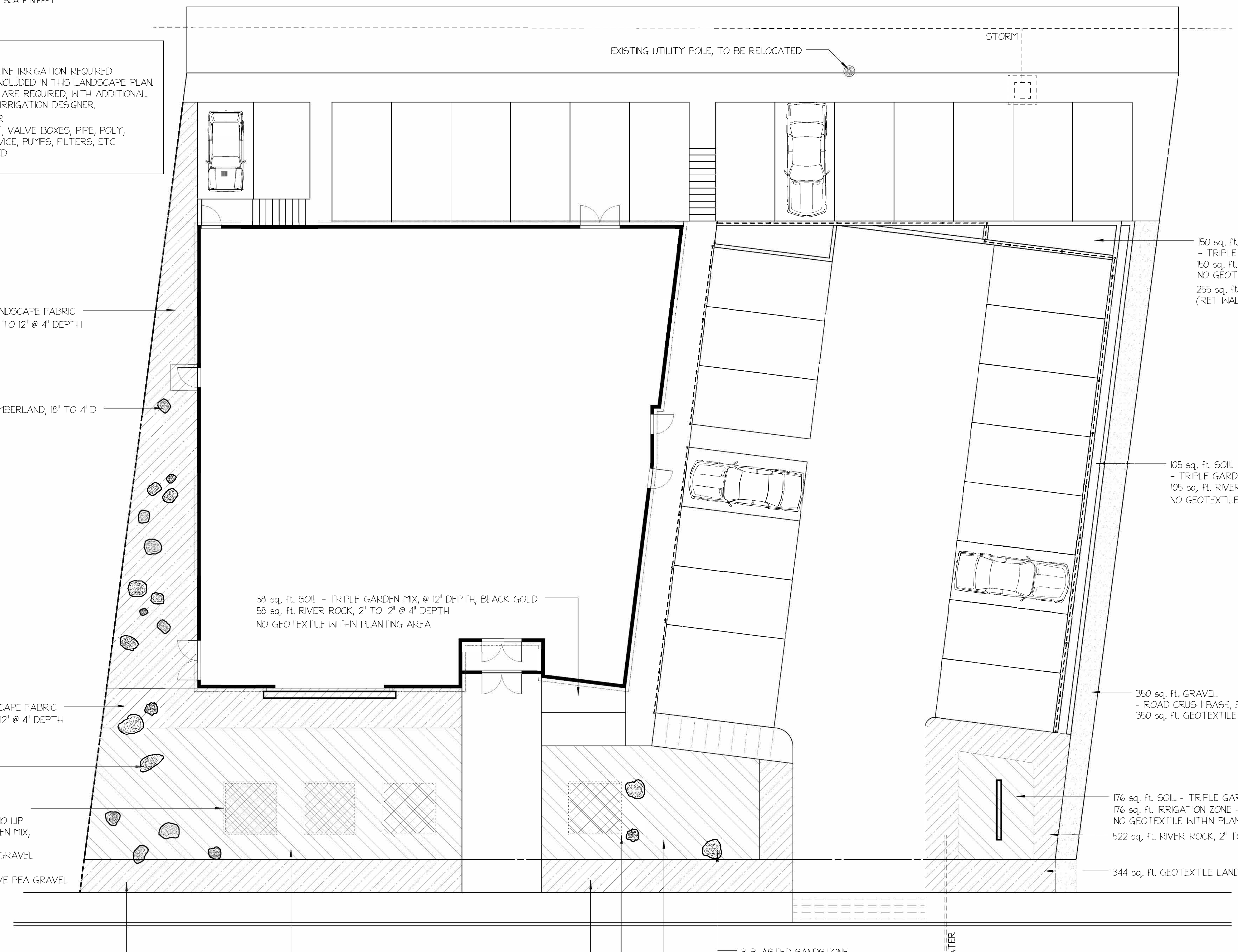
SCALE: 1" = 8'	PROJECT NO: HARDSCAPE DIMENSIONS
DRAWN BY: A. Huene	SHEET NO: 2 / 4
CHECKED BY:	DATE: Nov 6, 2023
DATE OF PRINT: Nov 14, 2023	



Schedule No. 2



IRRIGATION NOTES
 - 4 ZONES OF DRIP EMITTER LINE IRRIGATION REQUIRED
 - IRRIGATION DESIGN IS NOT INCLUDED IN THIS LANDSCAPE PLAN
 - THE FOLLOWING MATERIALS ARE REQUIRED, WITH ADDITIONAL MATERIALS AS SPECIFIED BY IRRIGATION DESIGNER.
 1 IRRIGATION CONTROLLER
 1 ALL IRRIGATION CONDUIT, VALVE BOXES, PIPE, POLY, FITTINGS, BACK FLOW DEVICE, PUMPS, FILTERS, ETC AND ELECTRICAL REQUIRED



NOTES

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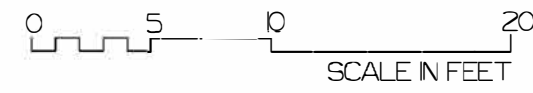
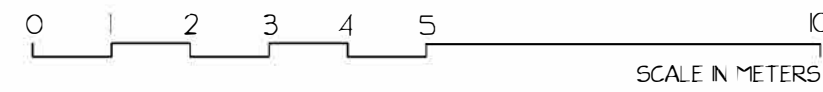
No.	Date	Description



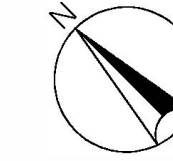
ARIANNE HUENE LANDSCAPE DESIGN
 www.ariannehuelandscape.com
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 arianne@ariannehuelandscape.com

**SALVATION ARMY
 COMOX VALLEY
 COMMUNITY CHURCH**
 1570 Fitzgerald Ave
 Courtenay, BC V9N 2S2

SCALE: 1" = 8'	PROJECT NO: HARDSCAPE TAKEOFFS
DRAWN BY: A. Huene	SHEET NO: 3 / 4
CHECKED BY:	DATE: Nov 6, 2023
DATE OF PRINT: Nov 14, 2023	



Schedule No. 2



SEE ATTACHED MASTER PLANT LIST FOR DETAILS

Qty	Botanical Name	Common Name
Trees		
2	<i>Pinus parviflora</i> 'Tempelhof'	Tempelhof Japanese White Pine
3	<i>Pinus strobus</i> 'Fastigiata'	Columar Eastern White Pine
Conifers		
26	<i>Juniperus sabina</i> 'Tamariscifolia'	Tamarix Juniper
Shrubs		
44	<i>Berberis verruculosa</i>	Warty Barberrry
6	<i>Nandina domestica</i> 'Gulf Stream'	Gulf Stream Dwarf Heavenly Bamboo
9	<i>Yucca filamentosa</i>	Soapweed
Ornamental Grasses		
31	<i>Carex testacea</i> 'Prairie Fire'	Prairie Fire New Zealand Sedge
10	<i>Pennisetum alopecuroides</i> 'Hameln'	Dwarf Fountain Grass
Groundcovers		
61	<i>Arctostaphylos uva-ursi</i>	Kinnickinick
Vines		
6	<i>Lonicera heckrottii</i> 'Goldflame'	Goldflame Honeysuckle

DRAWING NOTES:

Landscape Contractor must have access to Master Plant List.

DROUGHT TOLERANCE:

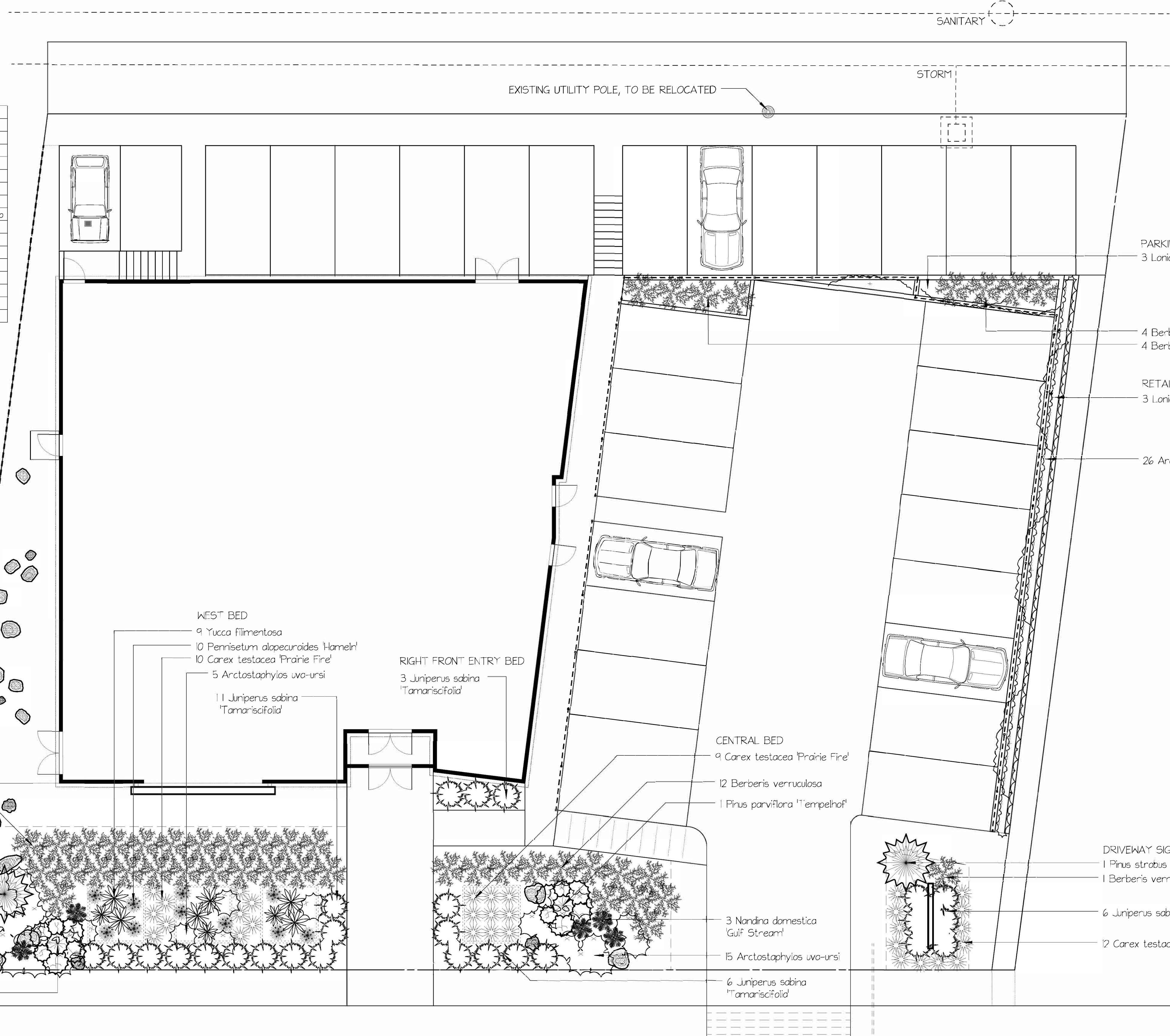
All plants, even those considered drought tolerant, require 2 years of watering to become established.

DEER:

Plants indicated outside fencing are considered deer resistant but are not guaranteed deer proof.

PLANT TOXICITY:

Many plants are poisonous to pets and children. Care should be taken in the landscape with all plants.



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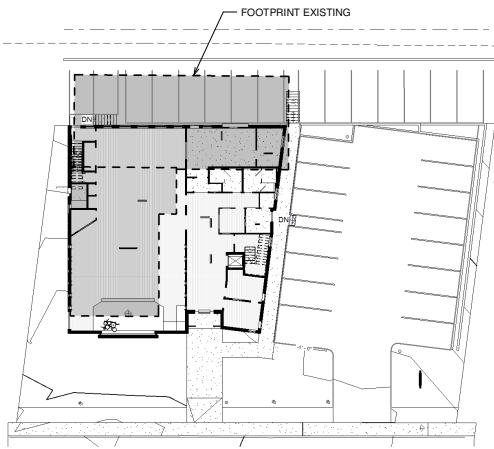
CANADIAN LANDSCAPE STANDARDS

SECTIONS OF NOTE:

6.1. Plants shall be selected, transported, and handled in accordance with this section.
6.1.12.1. SUBSTITUTIONS
All plants shall be supplied as specified on the plant list. There shall be no substitutions of plants without prior written approval from the landscape designer.
6.2.9.1. PLANTING ACCESSORIES
Tree ties used to secure guy wires and stakes shall be installed in accordance with this section.

6.3.4.1.D. Woody plants shall be planted with the root flare 2.5 - 5cm above finished grade.
6.3.4.2.D. Place plants to depth equal to original growing depth.
6.3.4.3.B. Trees - The trunk flare shall be 2.5-5cm above the finished grade and visible after the tree has been planted.
6.3.4.3.D. Growing medium shall be watered in when growing medium has been placed to two-thirds of the depth of the rootball and allowed to settle around the roots. After the water has been absorbed, the backfilling shall be completed and tamped lightly. Any settling shall be corrected by raising the tree.
6.3.4.3.H. All string, rope, burlap and other restricting elements shall be cut and removed out to the perimeter of the rootball; all wire basket handles shall be cut off flush with the top ring or folded back down into the planting hole.
6.3.7. WATERING
1. Plants shall be watered immediately after planting to the depth of their root systems.
2. Watering shall be carried out when required and in volumes relative to specific plant needs and growing medium type to maintain available moisture through the root zone.

		Arianne Huene Landscape Design 250.871.2454		Salvation Army Comox Valley Community Church 1570 Fitzgerald Ave, Courtenay, V9N2S2	
		Quantity	Description	NOTES	UNIT COST
23	BLASTED SANDSTONE - CUMBERLAND, 18" TO 4' D	APPROX 8 YARDS	\$60 / YD	\$480	
4	CORTEN STEEL PLANTER, 8' L X 8' W X 2' H, NO BASE NO LIP		\$1,000	\$4,000	
2279 sq. ft	GEOTEXTILE LANDSCAPE FABRIC	4' X 100' ROLLS = 6 ROLLS	\$100	\$600	
256 sq. ft	GRAVEL - 3/8" PEA GRAVEL @ 2" DEPTH	APPROX 2 YARDS	\$65 / YD	\$130	
350 sq. ft	GRAVEL - ROAD CRUSH BASE, 3/4" MINUS @ 3" DEPTH, COMPACTED	APPROX 4 YARDS	\$50 / YD	\$200	
4080 sq. ft	RIVER ROCK, 2" TO 12" @ 4" DEPTH	APPROX 55 YARDS	\$60 / YD	\$3,300	
2149 sq. ft	SOIL - TRIPLE GARDEN MIX, @ 12" DEPTH, BLACK GOLD	APPROX 80 YARDS	\$70 / YD	\$5,600	
256 sq. ft	SOIL - TRIPLE GARDEN MIX, @ 18" DEPTH, BLACK GOLD	APPROX 15 YARDS	\$70 / YD	\$1,050	
	DELIVERY FEES FOR LOOSE MATERIAL	APPROX 7 @ \$200 PER DELIVERY	\$1,400	\$1,400	
	LABOUR COSTS	3 WEEKS X 3 PERSONS X 40HR WEEK @ \$25 / HR	\$10,000	\$10,000	
	EXCAVATION, GRADING AND COMPACTING		\$6,000	\$6,000	
	PLANTS	SEE MASTER PLANT LIST FOR DETAILS	\$3,638.88	\$3,700.00	
			SUBTOTAL	\$36,460.00	
			GST @ 5%	\$1,823.00	
			PST @ 7%	\$2,552.20	
			TOTAL	\$40,835.20	
PLEASE NOTE: QUANTITIES AND UNIT COSTS ARE ESTIMATES ONLY. QUANTITIES ARE PROVIDED FOR LANDSCAPE PLAN COST ESTIMATE DEVELOPMENT PERMIT SUBMISSION. CONTRACTORS AND CLIENTS ARE RESPONSIBLE FOR ENSURING ACCURATE QUANTITIES AND COSTS. ADDITIONAL MATERIALS MAY BE REQUIRED.					

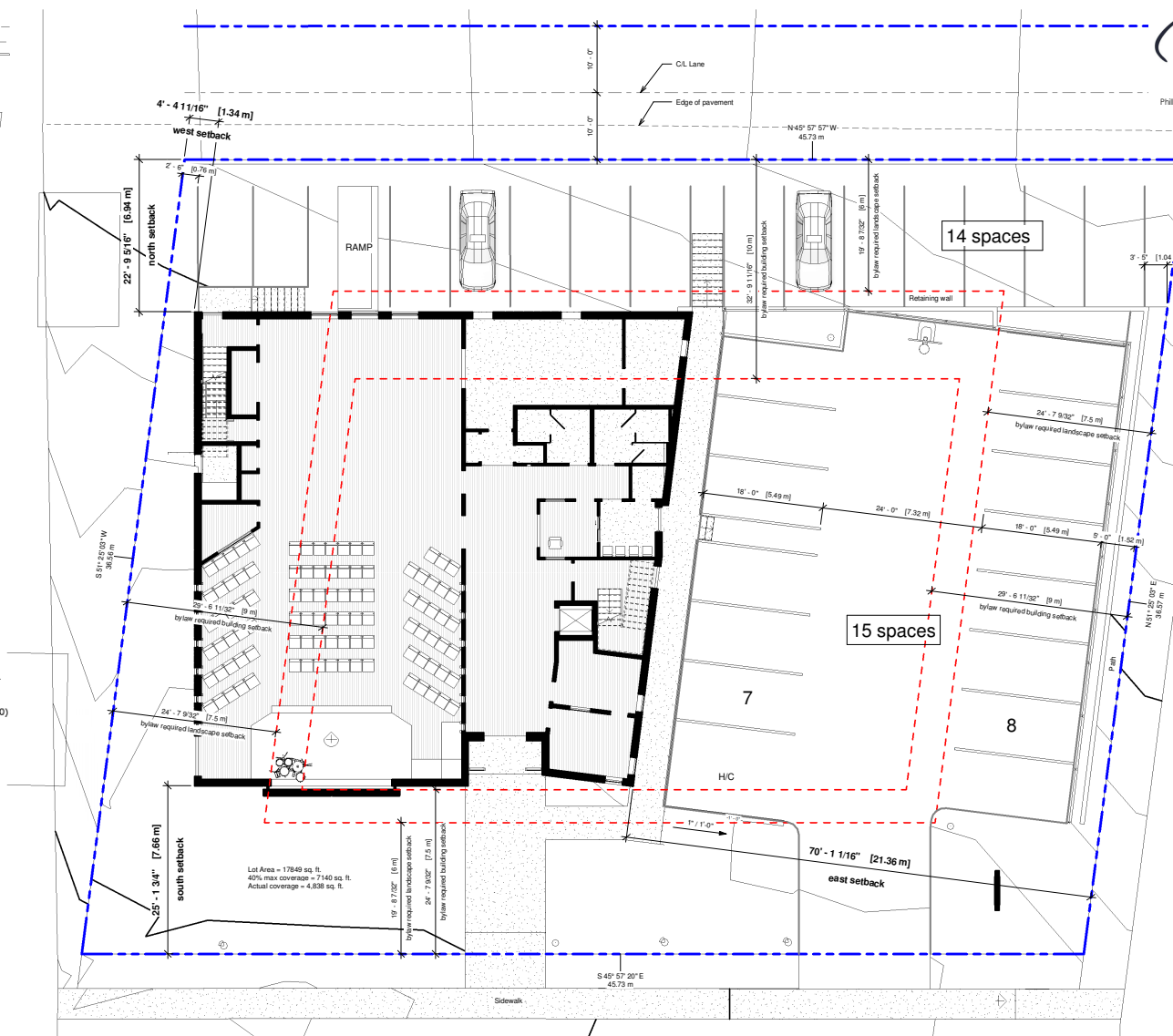


EXISTING SITE PLAN COMPARISON
1
1" = 20'-0"

SALVATION ARMY
COMOX VALLEY COMMUNITY CHURCH
1580 Fitzgerald Avenue, Courtenay

Zoning Compliance

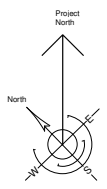
Description	New two storey church building with Sanctuary, Hall, kitchen, meeting rooms, offices and ancillary spaces with surface parking.																
AHJ	City of Courtenay																
Legal Address	Lot A, Section 41, Comox District, Plan 7449																
Civic Address	1580 Fitzgerald Ave., Courtenay (Includes 1570, 1580 and 1590)																
Zoning	Public Assembly (PA-1)																
Permitted Use	Church and Religious Centre																
DP Area	None (Adjacent to Downtown & Multi-Residential)																
Floor Area	Main Floor	432 m ² (4649 sq ft)															
	Upper Floor	318 m ² (3420 sq ft)															
	Total	750 m ² (8069 sq ft)															
Area of Lot	1,658 m ²																
Building Footprint	450 m ²																
Lot Coverage	Permitted	40 %															
	Proposed	27.1 %															
Building Height	Permitted	12 m															
	Proposed	10 m															
Building Setbacks	<table border="1"> <thead> <tr> <th></th> <th>Required</th> <th>Proposed</th> </tr> </thead> <tbody> <tr> <td>Front</td> <td>7.5 m</td> <td>7.5 m</td> </tr> <tr> <td>*Rear</td> <td>10.0 m</td> <td>6.94 m (Existing 1.66m)</td> </tr> <tr> <td>*West Side</td> <td>9.0 m</td> <td>1.34 - 4 m (Existing 1.14m)</td> </tr> <tr> <td>East Side</td> <td>9.0 m</td> <td>21.4 m</td> </tr> </tbody> </table>			Required	Proposed	Front	7.5 m	7.5 m	*Rear	10.0 m	6.94 m (Existing 1.66m)	*West Side	9.0 m	1.34 - 4 m (Existing 1.14m)	East Side	9.0 m	21.4 m
	Required	Proposed															
Front	7.5 m	7.5 m															
*Rear	10.0 m	6.94 m (Existing 1.66m)															
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East Side	9.0 m	21.4 m															
Landscaping	<table border="1"> <thead> <tr> <th></th> <th>Required</th> <th>Proposed</th> </tr> </thead> <tbody> <tr> <td>Front</td> <td>6.0 m</td> <td>6.0 m</td> </tr> <tr> <td>*Rear</td> <td>6.0 m</td> <td>0.0 m (Existing 1.66m)</td> </tr> <tr> <td>*West Side</td> <td>7.5 m</td> <td>0.76 - 4 m (Existing 1.14m)</td> </tr> <tr> <td>*East Side</td> <td>7.5 m</td> <td>1.04 m</td> </tr> </tbody> </table>			Required	Proposed	Front	6.0 m	6.0 m	*Rear	6.0 m	0.0 m (Existing 1.66m)	*West Side	7.5 m	0.76 - 4 m (Existing 1.14m)	*East Side	7.5 m	1.04 m
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*East Side	7.5 m	1.04 m															
Parking	Church	1 space per 4 seats = 90 / 4 = 23 spaces															
	Proposed	29 parking spaces															
	Small spaces allowed = 10% if more than 20 spaces = 2																
	Accessible parking required = 1 space																
Loading	Required	None															
	Proposed	None															
	* Variances required																



2 SITE PLAN DP
1/8" = 1'-0"

FITZGERALD

Phi Architecture Inc.
Phillipa Atwood Architect, AIBC, MRAIC, LEED AP
S Little Bear Way Royston BC V0R 2V0
pippo@pawoodarchitect.com
250.705.0433 / 250.218.0724



SALVATION ARMY - CV COMMUNITY CHURCH

REVISED 13/FEB/2024
LANDSCAPE SETBACKS

DP001

Scale	As indicated	Plot date:
Date	Issue Date	2024-02-13 12:57:28 PM

Project number	Sheet
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Giving Hope Today

The Salvation Army

Comox Valley Ministries

1580 Fitzgerald Ave.

Courtenay, B.C. V9N 2S2

Telephone: (250) 338-8221

Fax: (250) 338-8209

Website: www.cvsalarmy.ca

WILLIAM & CATHERINE BOOTH
FOUNDERS

LYNDON BUCKINGHAM
GENERAL

COMMISSIONERS LEE & DEBBIE
GRAVES
TERRITORIAL COMMANDER

To City of Courtenay
Attention Planning Department

**Re Application for a Development Variance Permit for
THE SALVATION ARMY COMOX VALLEY COMMUNITY CHURCH
at 1580 Fitzgerald Avenue**

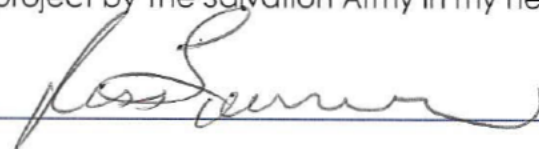
The Salvation Army is planning to build a new church!

This will allow us to continue meeting the growing needs of our congregation and to provide programs and services to the community.

We need your help! We are planning to ask the City of Courtenay to allow these relaxations in their building bylaws. We are asking for the following:

1. Reduce the rear building setback to 6.7 m from 10 m. It is now 1.66 m.
2. Reduce the west building setback to 1.35 - 4 m from 9 m. It is now 1.14 m.
3. Reduce the rear landscaping to 0 m from 6 m. It is now 1.66 m.
4. Reduce the west landscaping to 1.35 - 4 m from 7.5 m. It is now 1.14 m.
5. Reduce the east landscaping to 1.4 m from 7.5 m.

This is to confirm that I am aware of this project by The Salvation Army in my neighbourhood and I fully support it.

ROSS TURNER 
Name

1560 FITZGERALD AVE 
Address Phone

Comments:

I SUPPORT THE PROJECT.
BEEN GOOD NEIGHBORS.



Giving Hope Today

The Salvation Army
Comox Valley Ministries
1580 Fitzgerald Ave.
Courtenay, B.C. V9N 2S2

Telephone: (250) 338-8221
Fax: (250) 338-8209
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**Re Application for a Development Variance Permit for
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KEN WILLES Ken Willes
Name
1614 FITZGERALD AVE V2N 2S2
Address Phone

Comments:

I AGREE

Notice of Development Variance Permit No. 2306

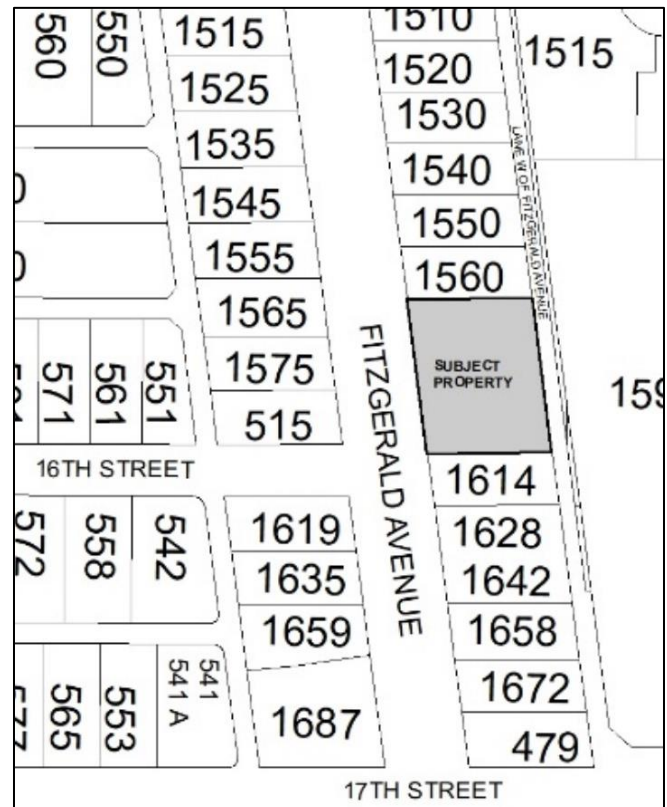
Development Variance Permit application for the redevelopment of 1580 Fitzgerald Avenue.

File No.: 3090-20-2306/DVP00048

Council will consider Development Variance Permit No. 2306, at the June 12, 2024 regular Council meeting at 4pm, to allow for the redevelopment/replacement of the church at 1580 Fitzgerald Avenue (LOT A, (DD P54316) SECTION 41, COMOX DISTRICT, PLAN 7449).

The property is zoned Public Assembly One (PA-1). The proposed Development Variance Permit would vary the following sections of Zoning Bylaw No. 2500, 2007:

- *Section 8.26.2 – Minimum Lot Size – A lot shall have an area of not less than, from 2,500m² to 1,600m².*
- *Section 8.26.5(2) – Rear Yard Building Setback – from 10.0m to 6.8m.*
- *Section 8.26.5(3) – Side Yard Building Setback – for the north west side yard from a minimum of 9.0m to 1.2m.*
- *Section 8.26.10(1) – Landscape and Screening Requirements – where a lot adjoins a residential zone, a minimum landscaped area shall be provided along the inside of all affected property lines from 7.5 m to 0.7 m for the north west side property line and from 7.5 m to 1.0m for the south east side property line.*
- *Section 8.26.10(2) – Landscape and Screening Requirements – where a lot adjoins a street, a landscaped area shall be provided along the entire frontage of the property facing the street, from 6.0m to 0.0m.*



Get more information

The proposed permit is available for viewing at Courtenay City Hall, 830 Cliffe Avenue, May 31 – June 12, 8:30 am – 4:30 pm Monday to Friday, Except holidays, OR online: Courtenay.ca/devapps (more information on reverse of page)



**City of
Courtenay**

City of Courtenay Development Services

830 Cliffe Avenue, Courtenay BC, V9N 2J7

Page 63 of 40 Phone 250-703-4839 | Email planning@courtenay.ca

Watch Meeting In-person Wednesday June 12, 2024

starting at 4:00 p.m.

At the CVRD's CIVIC Room

770 Harmston Ave., Courtenay



Watch Meeting Online Wednesday June 12, 2024

starting at 4:00 p.m.

Streamed live on the City of Courtenay's YouTube channel



Submit Written Comments by 1:00 p.m. on Wednesday June 12, 2024

Email: planning@courtenay.ca

Mail: Development Services

830 Cliffe Avenue, Courtenay, BC

V9N 2J7



Michaela Jones
THQ Project Manager
The Salvation Army
2 Overlea Blvd
Toronto, Ontario
M4H 1P4

Arcadis Canada Inc.
1285 West Pender Street
Suite 100
Vancouver, British Columbia
V6E 4B1
Canada
Phone: 604 683 8797
www.arcadis.com

Date: April 10, 2024
Our Ref: 30209312

**Subject: Confirmation of Design Compliance with Instrument Conditions (revised)
1580 Fitzgerald Avenue, Courtnay, BC**

Dear Michaela,

As requested by The Salvation Army, Arcadis has prepared this letter in response to the City of Courtenay's (Courtenay) request for information further to the Application for Development Variance Permit for 1580 Fitzgerald Avenue, in Courtnay, British Columbia (the Site). This request was sent to Phi Architecture Inc. (Phi) on January 19, 2024. This letter has been prepared to confirm that the proposed development design for the Site is consistent with the contaminated sites legal instrument Certificate of Compliance (CoC), including the Requirements and Conditions as outlined in Schedule B of the CoC.

Background

The Site is located within a residential area in Courtnay, British Columbia. The Site has a significant slope to ground level with an approximate 2.2 m drop from the southwest property line along Fitzgerald Avenue to northeast Site property line along the laneway, and an approximate 1.0 m drop from the northwest property line to the southeast property line. The Site is currently undergoing redevelopment without land use change. The redevelopment has included the removal of the prior existing multi-use church building, and will include the construction of a new, larger multi-use church building.

Environmental investigation work conducted between 2011 and 2013, identified that leaks in the former (2) Site heating oil underground storage tanks (USTs), had caused contamination in soil, groundwater and soil vapour at the Site. The USTs had previously been removed in December 2010 at which time the soil and groundwater contamination was first identified. Remediation conducted in 2013 removed the majority of soil and associated groundwater contamination, however a minor amount of soil and groundwater contamination was left in place as this contamination extended under the former Site building and also extended partially into the laneway and was not accessible without compromising the structural stability of the building and roadway. Post remedial groundwater monitoring in 2015 identified that groundwater contamination continued to exist at the Site albeit at much lower levels than measured prior to remediation. A human health and ecological risk assessment was completed in 2013 and again in 2016 which identified that the residual soil, groundwater and soil vapour contamination did not present an unacceptable risk to human or ecological health.

On receiving the risk assessment conclusions that no unacceptable environmental risk was present at the Site, a submission was made to the then BC Ministry of Environment in late 2016, requesting that CoC be issued for the

Confirmation of Design Compliance with Instrument Conditions

1580 Fitzgerald Avenue, Courtenay, BC

The Salvation Army

Site confirming that the Site had been satisfactorily remediated. The CoC was issued by the Ministry on December 16, 2016.

As part of the development approval process, Courtenay has requested confirmation that the proposed development design for the Site is consistent with the contaminated sites legal instrument Certificate of Compliance (CoC), including the Requirements and Conditions as outlined in Schedule B of the CoC.

Objective and Scope of Work

As identified above, the objective of this letter and the underlying work is to confirm that the current development proposal adheres to the CoC, including the Requirements and Conditions as outlined in Schedule B of the CoC. In supporting this confirmation, Arcadis completed the following:

- Review of the development plans provided by Phi (**Attachment 1**), specifically details regarding the final grade and elevation of the proposed building lower foundation
- Review of the CoC, specifically the Requirements and Conditions as outlined in Schedule B of the CoC (**Attachment 2**)
- Review of the following Site investigation, remediation, and risk assessment reports:
 - Detailed Site Investigation and Site Remediation, 1580 & 1590 Fitzgerald Avenue, Courtenay, BC. Franz Environmental Inc. December 2013. (Franz 2013)
 - 1580 Fitzgerald Avenue Post Remediation Monitoring. Franz Environmental Inc. January 2015. (Franz 2015)
 - The Salvation Army, Human Health and Ecological Risk Assessment, 1580 & 1590 Fitzgerald Avenue, Courtenay, BC. Arcadis Canada Inc. February 2016. (Arcadis 2016)
- Preparation of this letter

Review of Development Plans

As identified above, Arcadis reviewed the Development Plans as provided by Phi, in order to determine the physical parameters of the proposed development, within specific attention to the details regarding the final grade and elevation of the proposed building lower foundation and the proposed paved areas. The Development Plans as provided to Arcadis by Phi are included as **Attachment 1** to this letter.

Arcadis noted that the proposed development will mainly comprise:

- a new building on the western half of the Site,
- a row of paved parking spaces along the northern extent of the property abutting the laneway,
- a paved parking area on the eastern half of the Site, with perimeter planting areas along the north and east perimeter, and
- a planted area along the southern extent of the property abutting the sidewalk along Fitzgerald Avenue, with the exception of a walkway (accessing the Site building) and a driveway (accessing the eastern parking area).

A review of the elevations for the above-noted developmental areas identified that the northern parking area was consistent or slightly above the elevation of the laneway, while the eastern parking area will be at the elevation or slightly below that of Fitzgerald Avenue, and just over 2 m above the elevation of the laneway.

Confirmation of Design Compliance with Instrument Conditions

1580 Fitzgerald Avenue, Courtnay, BC

The Salvation Army

A review of the elevation for the building foundation identified that the foundation will be largely below the surrounding grade with the exception of the northern extent of the building which will be slightly above the surrounding grade at the east corner, and slightly below grade at the west corner. The elevation for the top of the crawlspace floor slab (the lowest elevation within the building) is identified as 16.11 m (per Phi email, all elevations herein to geodetic datum CGVD28). The plans identify that this slab is continuous across the footprint of the Site building and that there are no unfinished portions of the building foundation.

Review of Certificate of Compliance

The Salvation Army applied for and received a CoC for the Site which is dated December 16, 2016 (**Attachment 2**). The CoC is a “risk based” CoC in that its application was supported by a risk assessment (Arcadis 2016). All CoCs are provided for specific bounds in legal property, and land use. The bounds for the CoC provided for the Site are as follows:

- **Property Designation:** The CoC is specific to the legal property parcel identified within Schedule A of the CoC, specifically Lot A, (DD P54316) Section 41, Comox District, Plan 7449. On review of the proposed development plan, the development plan falls within this legal property parcel.
- **Designated Land Use:** The CoC is specific to the type of land use for which it was obtained, as described in Schedule C of the CoC. The land use specified in Schedule C is Residential Land (RL) use. As Residential Land, as defined under the BC Contaminated Sites Regulation (CSR) includes uses such as community centres. While the primary use of the Site will be as a church, which is a CSR defined Commercial Land (CL) use, the CoC specified land use (RL) will allow the Salvation Army to use the property for more sensitive uses such as summer schools, child daycare, and community centre type activities, should the need arise.

In addition to the specific bounds of the CoC, the CoC may also specify requirements and conditions of future land use within those bounds, as provided in Schedule B of the CoC. The conditions which are specific to limiting future development of the Site are as follows:

- **Maintaining a Barrier Over the Management Area:** The area of the Site over which contaminated soil remains in place is referred to as the “Management Area” (**Figure 1**). Within the Management Area, a minimum of one of the following conditions must be maintained:
 - The ground surface is covered with an immovable barrier (e.g. asphalt paving or concrete slab) impermeable to human access.
 - A minimum soil thickness of 1 m of soil meeting RL land use standards is in place overlying the contaminated soil at depth below.

On review of the proposed development plan, over 95% of the area of the Management Area will be either paved or covered by the Site building, such that no human exposure to the underlying soil could potentially occur within this portion of the Management Area. Less than 5% of the management area (planting area within the northern boundary of the raised parking area) will contain area for plantings, identified as small shallow rooting shrubs. The elevation of grade at this portion of the Site will be over 2m above grade existing at the time of investigation & risk assessment, and likely separated from grade by concrete structure. Accordingly, the proposed development is compliant with this condition.

- **No Deep Rooting Plants within the Management Area:** Within the Management Area, no plants with roots which will exceed 1 m in depth from ground surface, should be planted. On review of the proposed development plan, over 95% of the area of the Management Area will be either paved or covered by the

Confirmation of Design Compliance with Instrument Conditions

1580 Fitzgerald Avenue, Courtnay, BC

The Salvation Army

Site building. Less than 5% of the management area (planting area within the northern boundary of the raised parking area) will contain area for plantings, identified as small shallow rooting shrubs. Accordingly, the proposed development is compliant with this condition.

- **Foundation Depth Limit:** As stated in Schedule B, “Basements in any new buildings onsite will not extend beyond the depth of the existing basement (0.6 m depth) evaluated in the risk assessment.”. It is noted in Schedule B that this condition is based on vapour attenuation factors selected based on assumptions of the structures, locations, and depths of buildings and trenches existing (at the time of risk assessment) or expected at the Site. This condition is somewhat poorly articulated as it is noted that the Site building at the time of risk assessment had several different basement levels, with varied depths below ground level due to the sloping nature of the ground surface of the Site at that time. Accordingly, the depth, or more appropriately the elevation(s) which should serve as the lower limit(s) or bound(s) for future development is unclear from the immediate wording within Schedule B. Accordingly, Arcadis reviewed key supporting environmental reports, on which the CoC conditions were based, to determine the limits (specific to potential soil vapour exposure) to which this Schedule B condition is referring. This review is provided in the following section.

Review of Environmental Reports

As identified above, in order to accurately determine the intent of foundation depth limitations provided within Schedule B of the CoC, it was necessary to review the foundational documentation which was used to develop that condition. Accordingly, Arcadis reviewed the Site investigation and remediation report (Franz 2013), the post remediation monitoring report (Franz 2015) and the risk assessment report (Arcadis 2016). Specifically, Arcadis reviewed the condition of the residual soil and groundwater contamination, which is the source of soil vapour contamination, the levels of soil vapour contamination which were either modeled or measured, and the resulting assessment of environmental risk related to soil vapour contamination at the Site. These reviews are discussed in the following sections.

Residual Soil and Groundwater Contamination

Investigation and remediation results identified that soil and groundwater at the Site has become contaminated with petroleum hydrocarbons (PHCs) as a result of the former leaking onsite heating oil USTs. A review of the 2013 investigation and remediation report (Franz 2013) identified that field observations and analytical results for groundwater and excavation water samples from both the investigation and remediation work confirmed that groundwater contamination had been delineated, extending from the former USTs outward, primarily in the northeast direction (BH9), which was inferred to be the direction of the flow of groundwater. This contamination was noted as being delineated, extending no further than 10 m from the former USTs at the centre of the Site. Post remedial groundwater monitoring identified that the PHC contamination continued to persist in the same location onsite (BH9). The groundwater itself was noted as being shallow, ranging from approximately 1.5 mbgs (within the laneway) to 2.8 mbgs (at the highest measured elevation point on the Site).

A review of the 2013 investigation and remediation report (Franz 2013) identified that field observations and analytical results for soil samples from both the investigation and remediation work confirmed that residual (non-remediated) soil PHC contamination remained beneath former Site building and the laneway. The residual soil contamination does not extend above an elevation of 14.17 m across the Site (Franz 2013, Fig. 5 & Fig. 17). This was equivalent to depths of approximately 2.0 mbgs in the northeastern extent of the Site along laneway, the lowest point of ground surface at the Site, and approximately 2.8 mbgs close to the southeast end of the building

Confirmation of Design Compliance with Instrument Conditions

1580 Fitzgerald Avenue, Courtnay, BC

The Salvation Army

(the southeastern extent of the contaminated soil plume) at the time of the investigation and remediation work. It is also approximately 1.94 m below the lowest point of the previous building basement floor surface (16.11 m elevation). As soil contamination was caused through contact with dissolved and free-phase PHCs via groundwater transport, impacts in soil should not extend above the vadose zone overlying the high groundwater elevation. Accordingly, the top elevation of soil contamination can be reliably identified in relation to known high groundwater levels.

Evaluation of Soil Vapour Contamination

A review of the 2013 investigation and remediation report (Franz 2013) identified that field observations and analytical results for soil vapour samples from both the investigation and remediation work confirmed that soil vapour PHC contamination was not identified in the onsite probes but was identified in one offsite probe (BH15SV in the alleyway) prior to remediation. It is noted that this contamination exists only with the unattenuated measured concentration. Following the application of the most conservative attenuation factor it was calculated that the resulting vapour concentration in the breathing zone was not indicative of contamination. In two follow-up post-remediation soil vapour sampling events, no vapour contamination either un-attenuated or attenuated was measured in the onsite and offsite vapour probes.

Assessment of Soil Vapour Risk

The Site risk assessment uses an assumption that any future basement at the Site will not extend deeper than the depth of the previous church basement (Arcadis 2015, Section 2.2). This assumption is used consistent with the fact that sub-slab soil vapour assessed below the former building sub-slab (BH21), were compliant with applicable soil vapour standards. Review of the sole sample collected sub-slab (post-remediation) indicated that at worst, measured volatile PHC concentrations (unattenuated) were 55% of the applicable standards.

Soil vapour data from the adjacent shallow (1.0 mbgs) vapour probe (BH18) were also reviewed. Further review of the soil vapour data from BH18 indicated that the unattenuated volatile PHC concentrations were at worst < 60% of the applicable standards pre-remediation, and <5% post-remediation.

After review of the historical reports, it is Arcadis' opinion that identifying the previously existing basement depth as a limitation to potential future Site development is overly conservative considering post-remediation groundwater and soil vapour sampling results. Based on the review of the assessment of soil vapour risk, the historically measured concentrations of PHCs in soil vapour, and the potential for application of attenuation factors in evaluating vapour risk, Arcadis has identified that the any new development could safely place its basement floor up to 1m deeper than the deepest point on the previously existing basement floor. The restriction for a new developments lowest floor elevation to not go below the existing floor elevation represents an intrinsic safety factor of just over 350 (i.e. soil vapour would have to be greater than 350 times what has been historically measured for there to be an issue with the soil vapour within the breathing zone of a building basement built with this restriction). Accordingly, it is reasonable to use the lowest elevation (16.11 m) of the previously existing (at the time of risk assessment) building basement floor as the lowest elevation to which a future building could be constructed and remain consistent with the assumptions used and stated within the risk assessment, and be consistent with the wording and conditions of Schedule B of the CoC. The proposed development is compliant with the foundation depth limitation condition of Schedule B of the CoC.

Confirmation of Design Compliance with Instrument Conditions
1580 Fitzgerald Avenue, Courtnay, BC
The Salvation Army

Conclusion

After review of the proposed development plan, the Site CoC, and historical environmental reports, Arcadis can confirm that the current development proposal adheres to the CoC, including the Requirements and Conditions as outlined in Schedule B of the CoC.

Limitations

Arcadis prepared this report for the exclusive use of The Salvation Army and their agents. The City of Courtenay may also rely on the information provided herein for the sole purpose of supporting the current development of the Site. Any use which a third party makes of this report, or any reliance on, or decisions to be made based on it, are the responsibility of such third parties. The authors accept no responsibility for damages, if any, suffered by any third party because of decisions made or actions based on this report.

The material presented in this report reflects Arcadis' judgement in light of the information available to us at the time of preparation. Third party information reviewed and used to formulate this report is assumed to be complete and correct. Arcadis used this information in good faith and will not accept any responsibility for deficiencies, misinterpretation, or incompleteness of the information contained in documents prepared by third parties.

No investigative method can eliminate the possibility of obtaining partially imprecise or incomplete information; it can only reduce the possibility to an acceptable level. Professional judgement was exercised in gathering and analyzing the information obtained. However, it is believed that the level of detail carried out for this investigation program is appropriate to meet the project objectives.

Sincerely,
Arcadis Canada Inc.



Art Hildebrand, P.Eng., CSAP
Senior Environmental Engineer

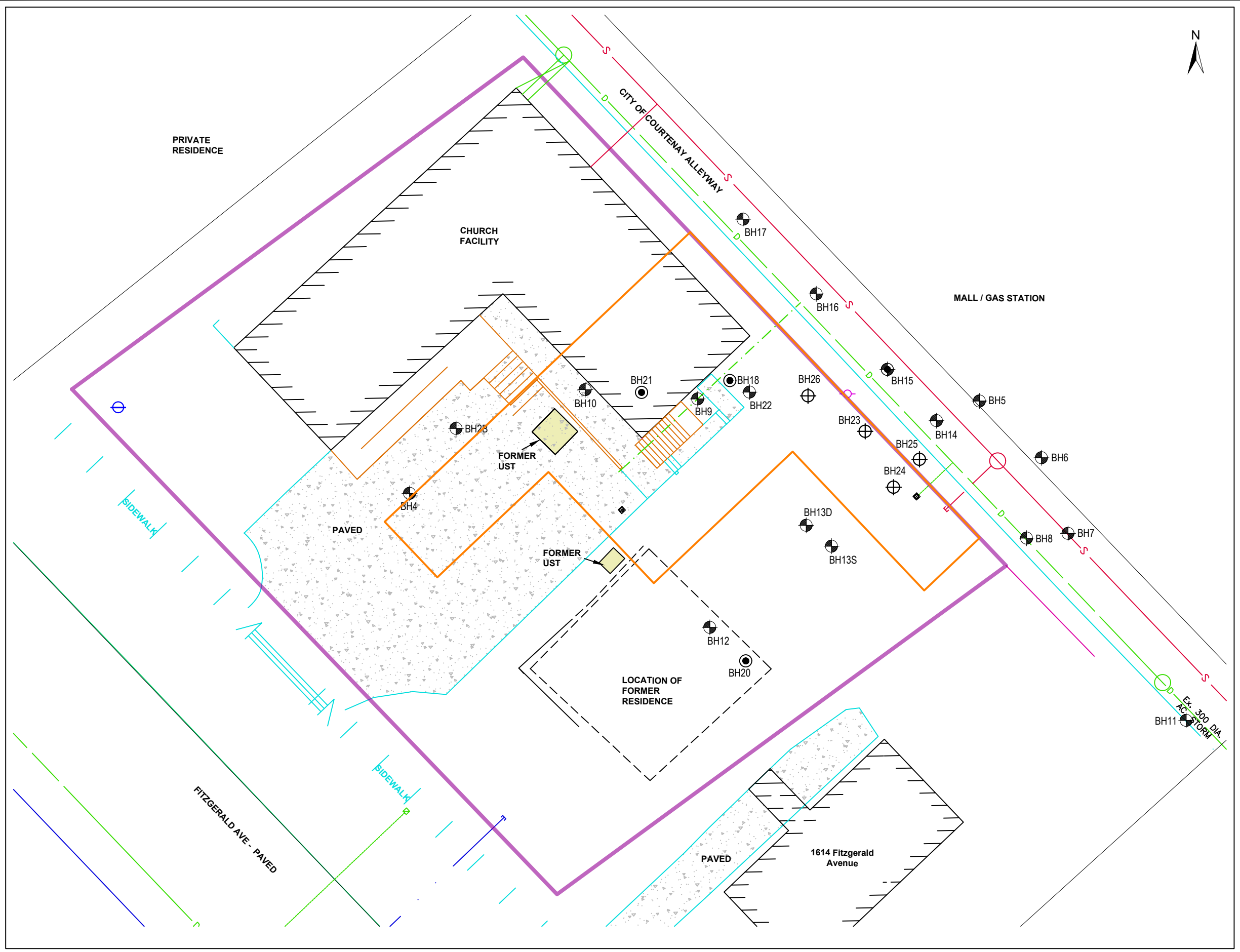
Email: arthur.hildebrand@arcadis.com
Mobile: 778-222-4781

CC. Phillipa Atwood, Phi Architecture Inc. (pippa@phiarchitecture.ca)

Enclosures:

- Attachment 1 Phi Development Plans (2024-02-22)
- Attachment 2 Certificate of Compliance (Site ID: 12695)

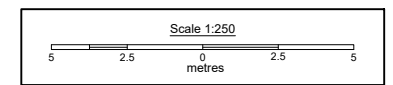
Figure_2 (4) G:\PROJECTS\2030 - 1580 Fitzgerald Ave (Sally Ann)\Addendum\CAD\2030-1001_recover_recover.dwg



LEGEND

- Site Boundary
- Management Area
- Former UST
- Existing Building
- Utility - Storm Sewer
- Utility - Sanitary Sewer
- Hydro Pole
- Manhole
- Borehole
- Monitoring Well
- Soil Vapour Probe
- Nested Soil Vapour Probe
- Wall Sample
- Municipal Water Connection (Approx. Location)

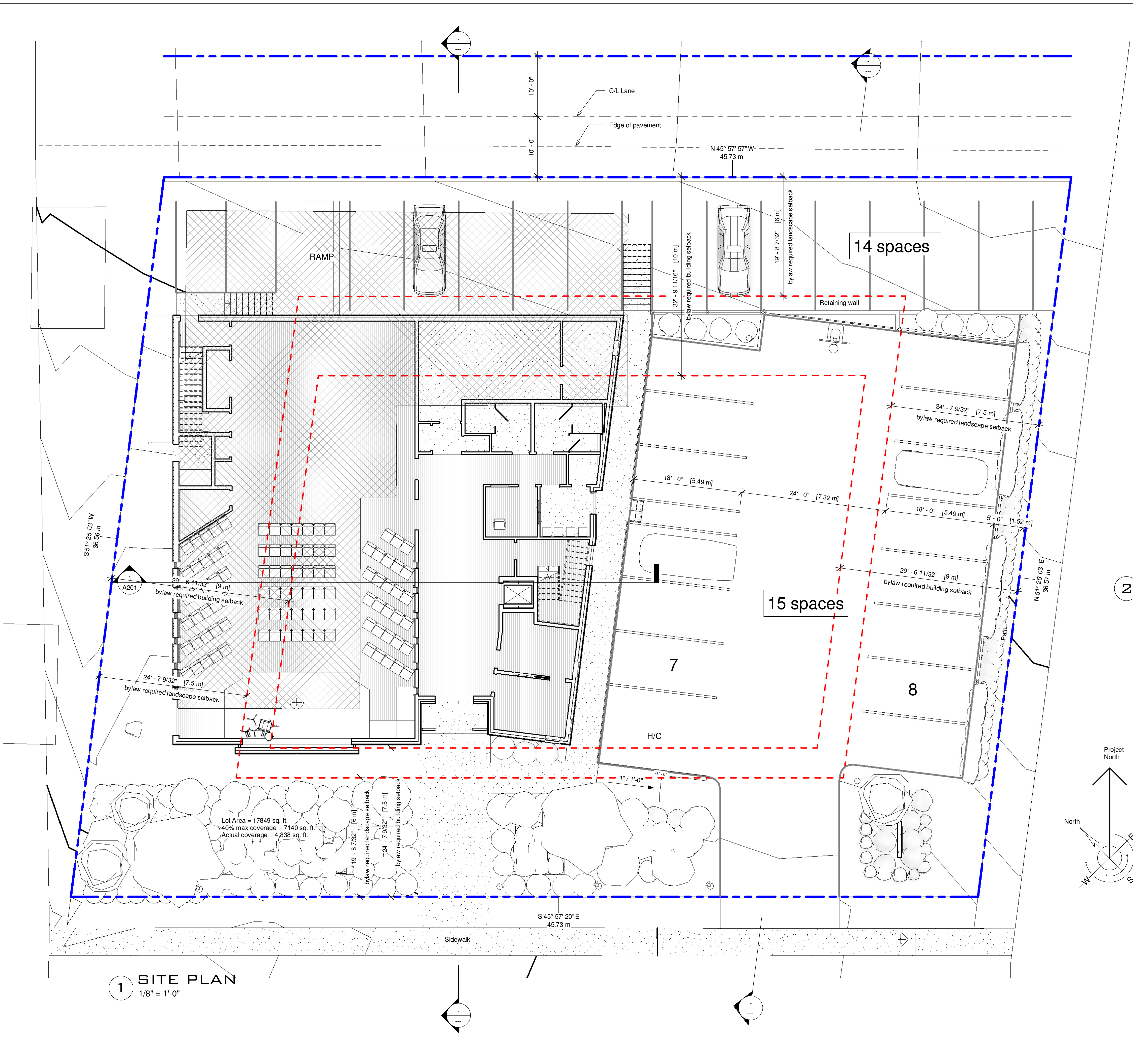
Notes:
Drawing is for illustrative purposes only. Not for construction



Title:	Management Area (Site 19695, CoC, Schedule B)
Project:	1580 AND 1590 FITZGERALD AVE, COURTENAY, B.C.
Client:	THE SALVATION ARMY
Date:	APRIL 2024
FIGURE 1	

Attachment 1
Phi Development Plan

**PRELIMINARY
 NOT FOR
 CONSTRUCTION**



2 SITE 3D

1 SITE PLAN
 1/8" = 1'-0"

Lot Area = 17849 sq. ft.
 40% max coverage = 7140 sq. ft.
 Actual coverage = 4,836 sq. ft.

No.	Description	Date
REVISIONS:		
ISSUED:		
For		Date

**SALVATION ARMY
 - CV COMMUNITY
 CHURCH**

SITE PLAN

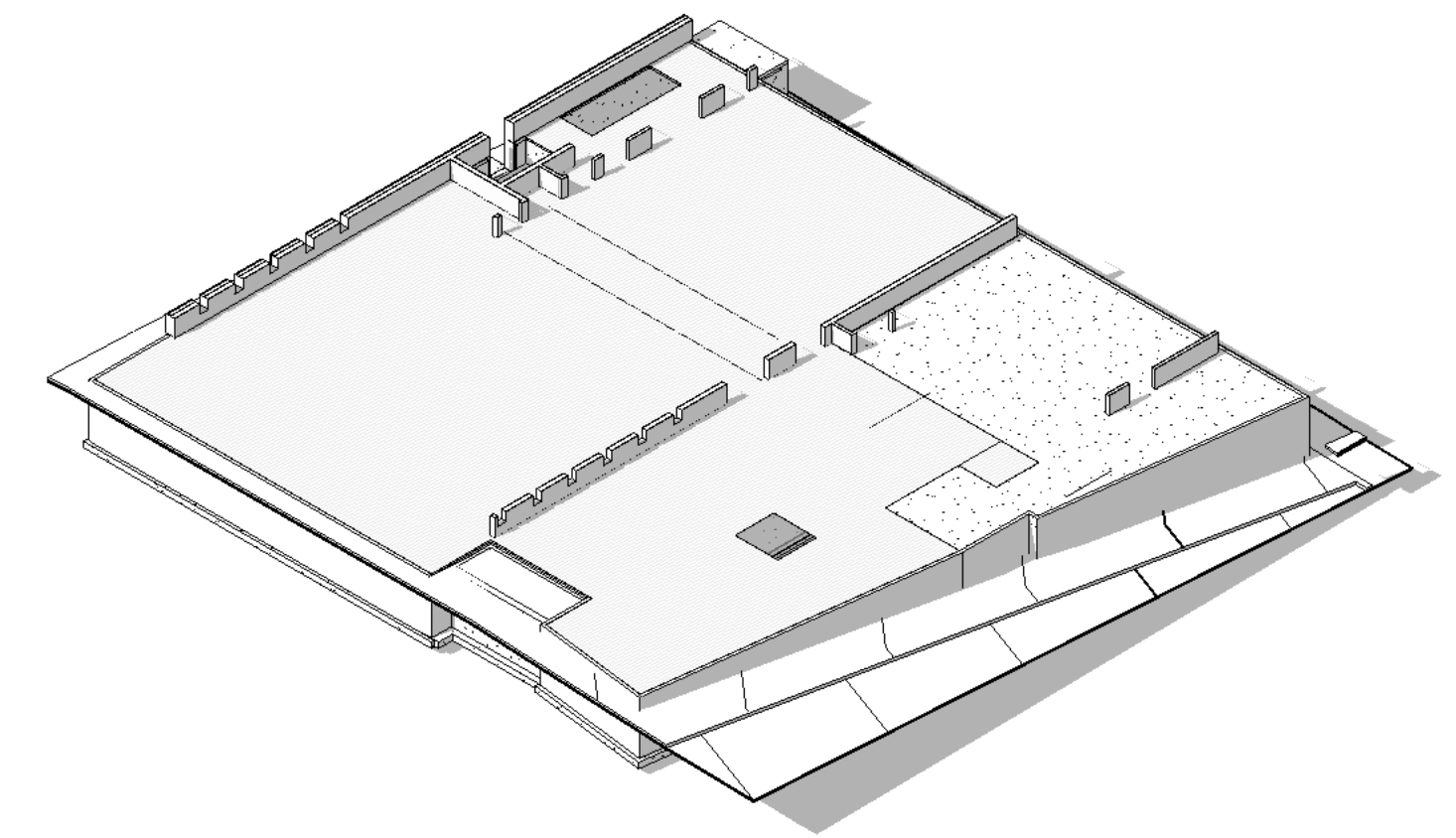
Drawn by	Author	Scale	1/8" = 1'-0"
Checked by	Checker	Date	Issue Date

A001

Project number	Sheet
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Plot date:
 2024-02-22
 10:43:04 AM

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2 FOUNDATION 3D

PRELIMINARY
 NOT FOR
 CONSTRUCTION

No.	Description	Date
REVISIONS:		
ISSUED:		
For		Date
Client review		Aug 03, 2022
Consultant schematics		Sept 18, 2022

**SALVATION ARMY
 - CV COMMUNITY
 CHURCH**

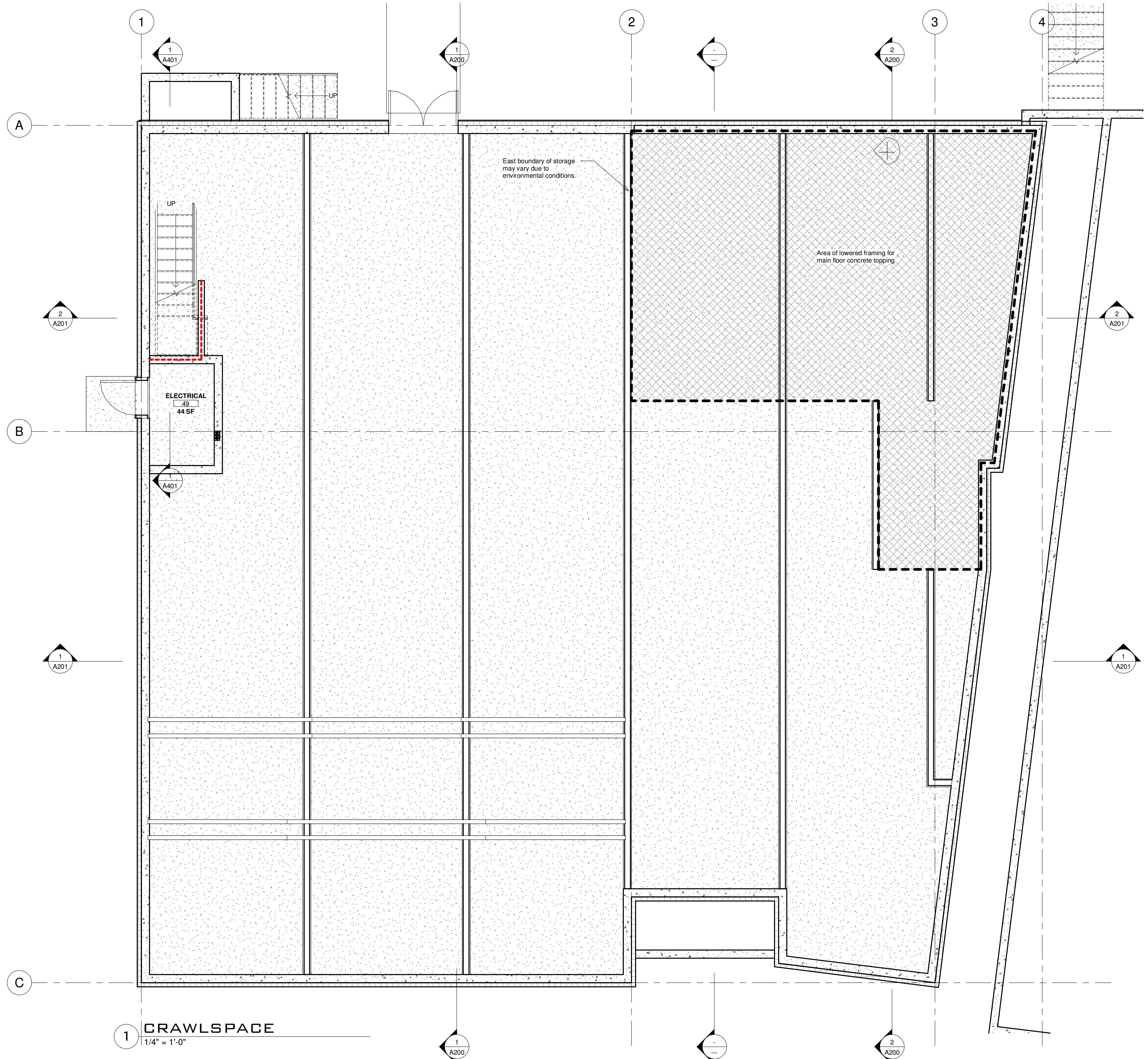
**CRAWLSPACE
 PLAN**

Drawn by	Author	Scale	1/4" = 1'-0"
Checked by	Checker	Date	Issue Date

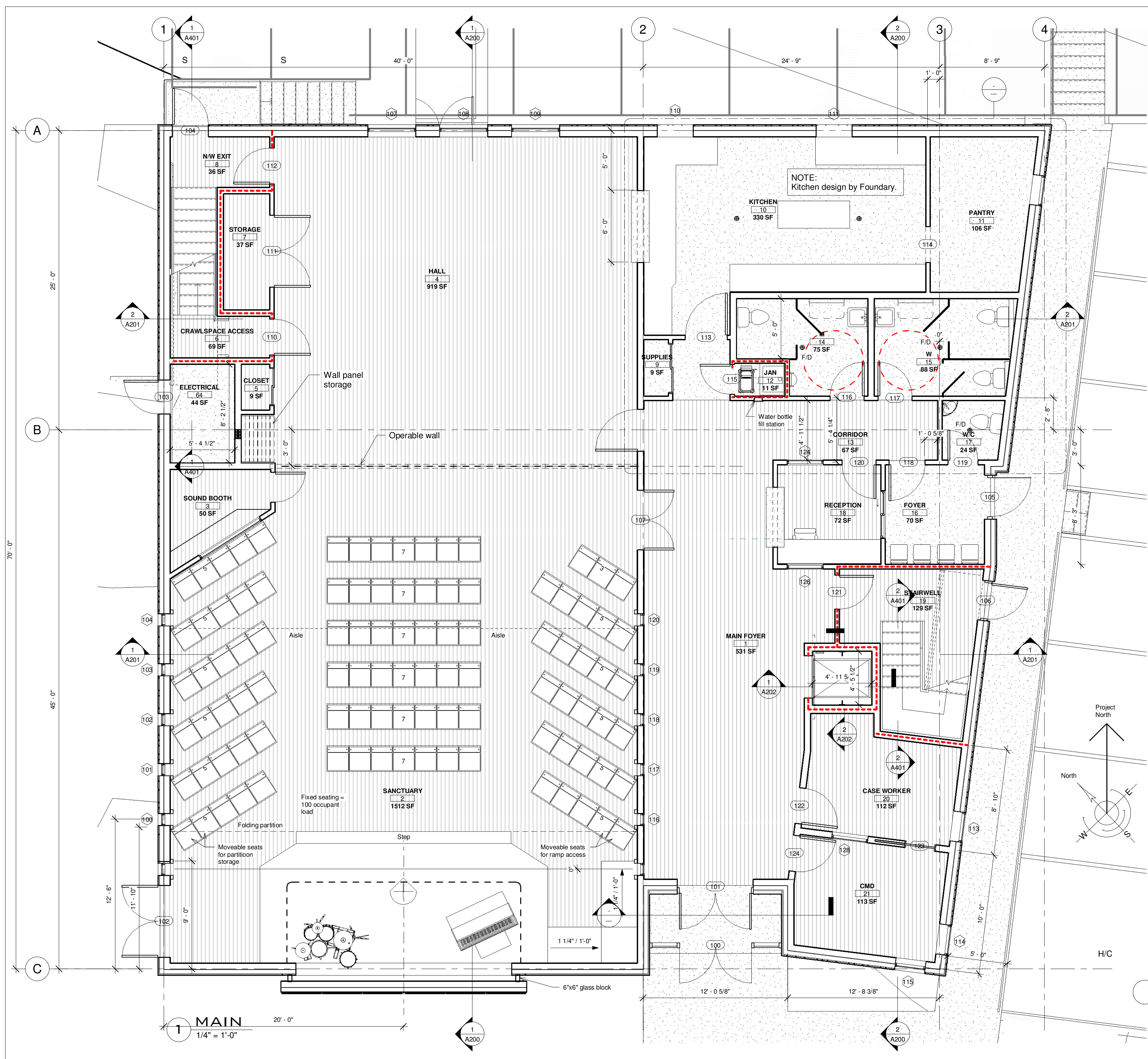
A101

Project number	Sheet
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Plot date:
 2024-02-22
 10:43:04 AM



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NOTE:
Kitchen design by Foundary.

MAIN FLOOR DOOR SCHEDULE							
Mark	Width	Height	Fire Rating	Construction Type	Frame Material	Level	Comments
100	6'-0"	7'-0"				MAIN	
101	6'-0"	7'-0"				MAIN	
102	6'-0"	7'-0"				MAIN	
103	2'-10"	7'-0"				MAIN	
104	3'-0"	7'-0"				MAIN	
105	3'-3"	7'-0"				MAIN	
106	3'-0"	7'-0"			Wood	MAIN	
107	5'-0"	7'-0"				MAIN	
110	3'-0"	7'-0"				MAIN	
111	6'-0"	7'-0"				MAIN	
112	3'-0"	7'-0"				MAIN	
113	3'-6"	7'-0"				MAIN	
114	0"	0"				MAIN	
115	2'-6"	6'-8"				MAIN	
116	2'-10"	7'-0"				MAIN	
117	2'-10"	7'-0"				MAIN	
118	3'-0"	7'-0"				MAIN	
119	2'-6"	6'-8"				MAIN	
120	2'-10"	7'-0"				MAIN	
121	2'-10"	7'-0"				MAIN	
122	3'-0"	7'-0"				MAIN	
123	2'-6"	6'-8"				MAIN	
124	3'-0"	7'-0"				MAIN	
228	2'-6"	6'-8"				MAIN	
229	3'-6"	7'-0"				MAIN	
232	4'-0"	7'-0"				MAIN	
233	3'-0"	7'-0"				MAIN	

MAIN FLOOR WINDOW SCHEDULE				
Mark	Width	Height	Level	Comments
100	1'-0"	8'-0"	MAIN	
101	1'-0"	8'-0"	MAIN	
102	1'-0"	8'-0"	MAIN	
103	1'-0"	8'-0"	MAIN	
104	1'-0"	8'-0"	MAIN	
107	4'-0"	4'-0"	MAIN	
108	4'-0"	4'-0"	MAIN	
109	4'-0"	4'-0"	MAIN	
110	3'-0"	3'-6"	MAIN	
111	3'-0"	3'-6"	MAIN	
112	2'-6"	5'-0"	MAIN	
113	2'-6"	5'-0"	MAIN	
114	2'-6"	5'-0"	MAIN	
115	2'-6"	5'-0"	MAIN	
116	1'-0"	8'-0"	MAIN	Interior
117	1'-0"	8'-0"	MAIN	Interior
118	1'-0"	8'-0"	MAIN	Interior
119	1'-0"	8'-0"	MAIN	Interior
120	1'-0"	8'-0"	MAIN	Interior
121	2'-6"	2'-6"	MAIN	
122	1'-0"	8'-0"	MAIN	
123	1'-0"	8'-0"	MAIN	Interior
124	3'-0"	3'-0"	MAIN	Interior
126	3'-0"	3'-0"	MAIN	Interior

TOTALS:
Main 4631 sq. ft.
Second 3448 sq. ft.
total = 8079 sq. ft.

**PRELIMINARY
NOT FOR
CONSTRUCTION**

LEGEND
1/4" = 1'-0"

No.	Description	Date
REVISIONS:		
ISSUED:		
For	Date	
SALVATION ARMY - CV COMMUNITY CHURCH		
MAIN FLOOR PLAN		
Drawn by	Author	Scale 1/4" = 1'-0"
Checked by	Checker	Date Issue Date
A102		
Project number	Sheet	

Plot date:
2024-02-22
10:43:06 AM

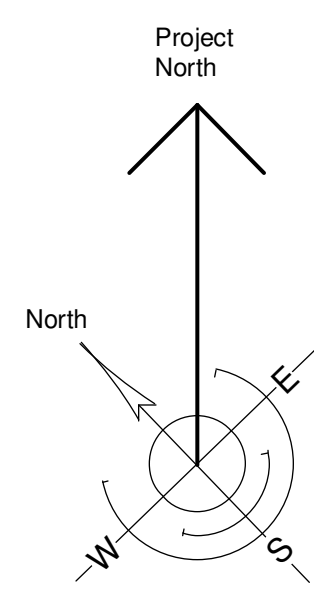


SECOND FLOOR DOOR SCHEDULE

Mark	Width	Height	Fire Rating	Construction Type	Frame Material	Level	Comments
200	3'-0"	7'-0"				SECOND	
201	2'-10"	7'-0"				SECOND	
202	2'-10"	7'-0"				SECOND	
203	2'-10"	7'-0"				SECOND	
204	2'-10"	7'-0"				SECOND	
205	3'-0"	7'-0"		Solid core dutch door	Wood	SECOND	Dutch door
206	3'-0"	7'-0"				SECOND	
207	3'-0"	7'-0"				SECOND	
209	3'-0"	7'-0"				SECOND	
210	3'-0"	7'-0"				SECOND	
211	3'-0"	7'-0"				SECOND	
212	3'-0"	7'-0"				SECOND	
213	6'-0"	7'-0"				SECOND	
214	6'-0"	7'-0"				SECOND	
215	2'-6"	6'-8"				SECOND	
216	2'-10"	7'-0"				SECOND	
217	2'-10"	7'-0"				SECOND	
218	2'-10"	7'-0"				SECOND	
219	2'-10"	7'-0"				SECOND	
220	2'-10"	7'-0"				SECOND	
221	4'-0"	7'-0"				SECOND	
222	2'-10"	7'-0"				SECOND	
224	2'-6"	6'-8"				SECOND	

SECOND FLOOR WINDOW SCHEDULE

Mark	Width	Height	Level	Comments
200	3'-4"	4'-0"	SECOND	
201	2'-6"	5'-0"	SECOND	
202	2'-6"	5'-0"	SECOND	
203	2'-6"	5'-0"	SECOND	
204	4'-0"	4'-0"	SECOND	
205	4'-0"	4'-0"	SECOND	
206	3'-0"	5'-0"	SECOND	
207	3'-0"	4'-0"	SECOND	
208	3'-0"	4'-0"	SECOND	
209	3'-0"	4'-0"	SECOND	
210	3'-0"	4'-0"	SECOND	
211	3'-0"	4'-0"	SECOND	
212	3'-0"	4'-0"	SECOND	
213	3'-0"	4'-0"	SECOND	
214	4'-0"	4'-0"	SECOND	
215	4'-0"	4'-0"	SECOND	
216	4'-0"	4'-0"	SECOND	
217	4'-0"	4'-0"	SECOND	
219	3'-0"	3'-0"	SECOND	Sound
220	3'-0"	3'-0"	SECOND	Interior
221	3'-0"	3'-0"	SECOND	Interior
222	6'-0"	4'-0"	SECOND	Sound, Wired glass.
223	3'-0"	3'-0"	SECOND	Interior
224	3'-0"	3'-0"	SECOND	Interior
225	3'-0"	3'-0"	SECOND	Interior
225	2'-6"	2'-6"	SECOND	Wired glass
226	1'-6"	6'-0"	SECOND	Wired glass
234	2'-6"	2'-6"	SECOND	
235	2'-6"	2'-6"	SECOND	



--- Fire Separation
LEGEND
 1/4" = 1'-0"

**PRELIMINARY
 NOT FOR
 CONSTRUCTION**

No.	Description	Date
REVISIONS:		
ISSUED:		
For	Date	
Client review	Aug 03, 2022	
Consultant schematics	Sept 18, 2022	

**SALVATION ARMY
 - CV COMMUNITY
 CHURCH**

**SECOND FLOOR
 PLAN**

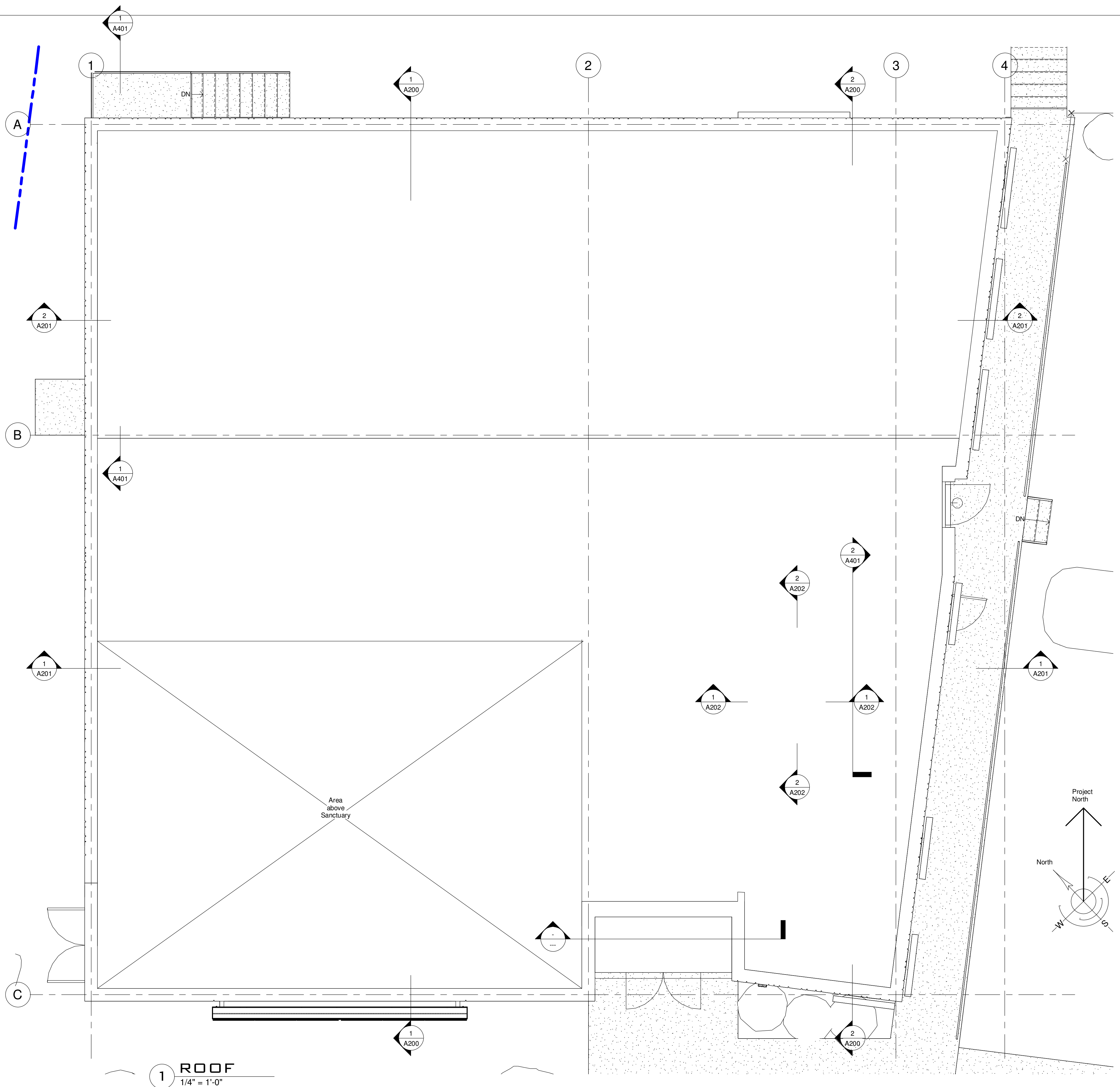
Drawn by	Author	Scale	1/4" = 1'-0"
Checked by	Checker	Date	Issue Date

A103
 Project number Sheet

Plot date:
 2024-02-22
 10:43:06 AM

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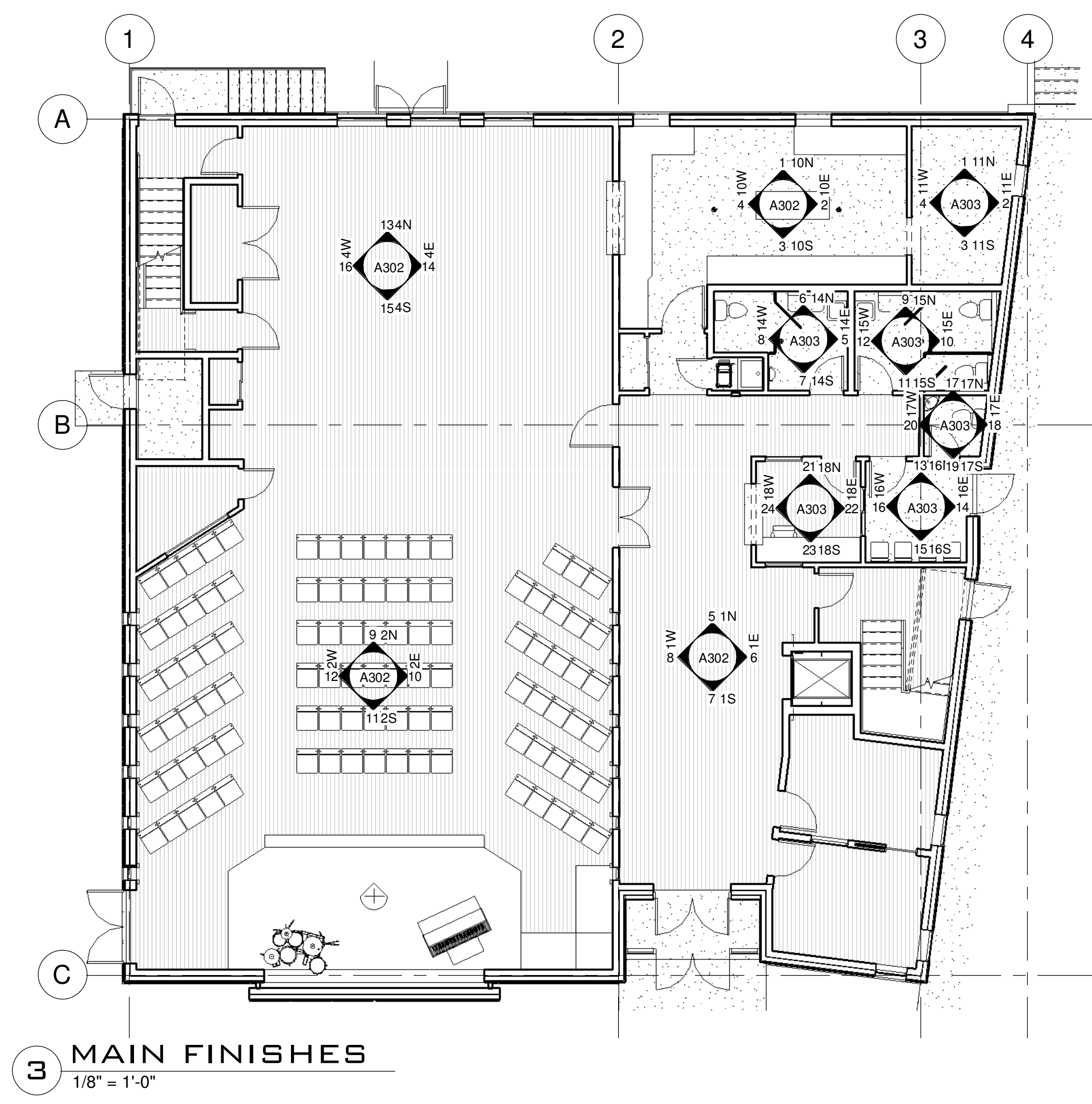
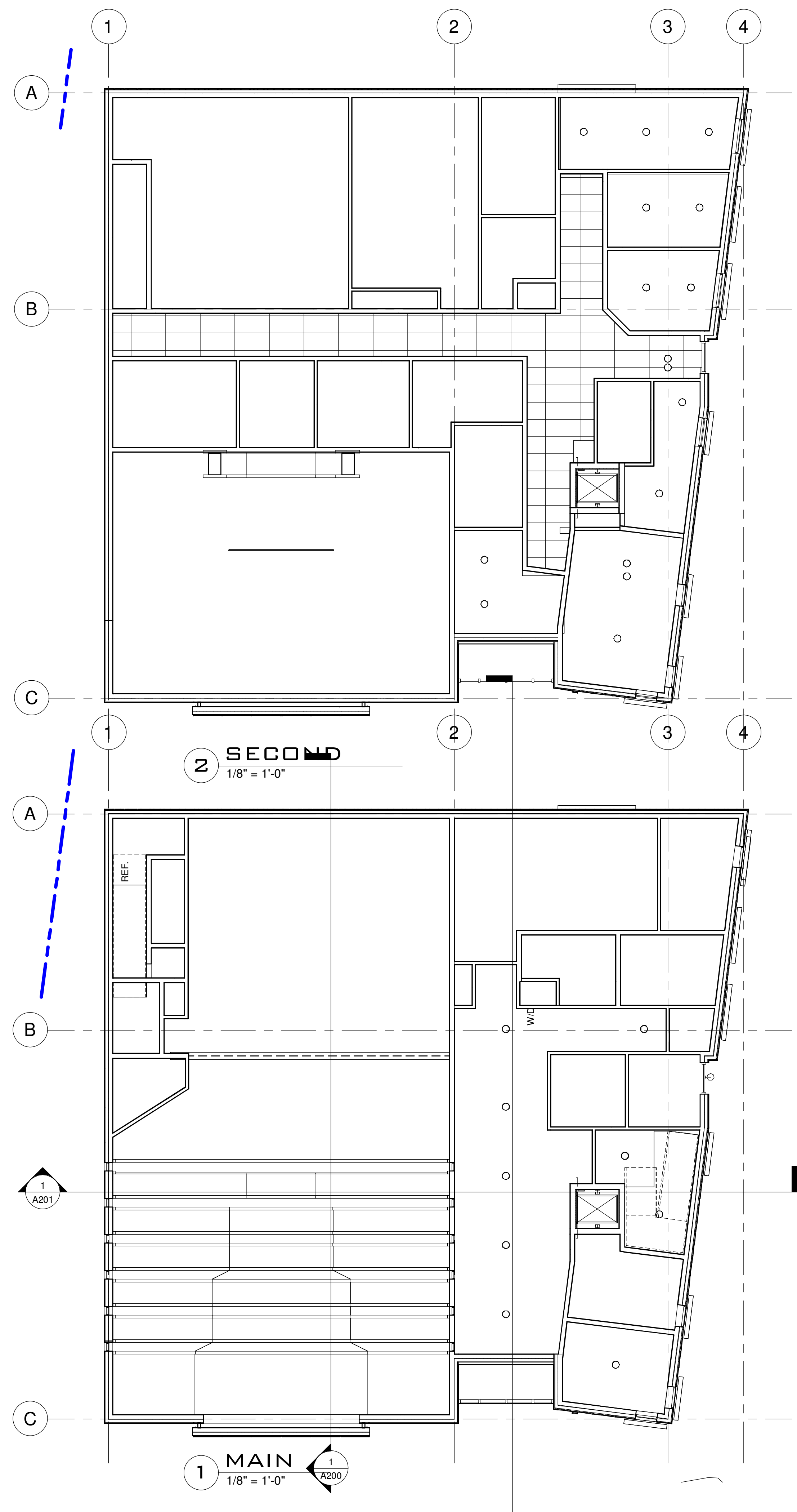
1 ROOF
 1/4" = 1'-0"

No.	Description	Date
REVISIONS:		
ISSUED:		
For		Date
<p>SALVATION ARMY - CV COMMUNITY CHURCH</p> <p>ROOF PLAN</p>		
Drawn by	Author	Scale 1/4" = 1'-0"
Checked by	Checker	Date Issue Date
A104		
Project number	Sheet	

Plot date:
 2024-02-22
 10:43:06 AM

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 CONSTRUCTION**



No.	Description	Date
REVISIONS:		
ISSUED:		
For	Date	

**SALVATION ARMY
 - CV COMMUNITY
 CHURCH**

**RCP & INTERIOR
 FINISHES**

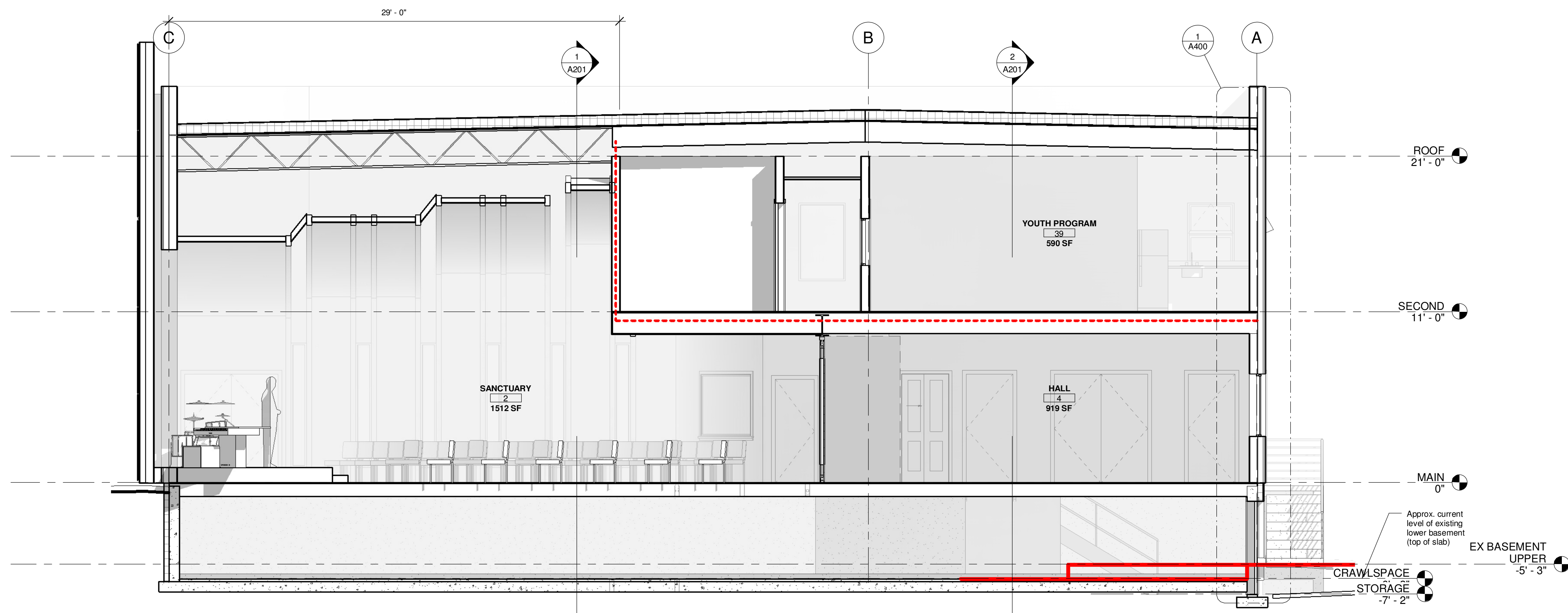
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Checked by	Checker	Date	Issue Date

A105

Project number	Sheet
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Plot date:
 2024-02-22
 10:43:07 AM

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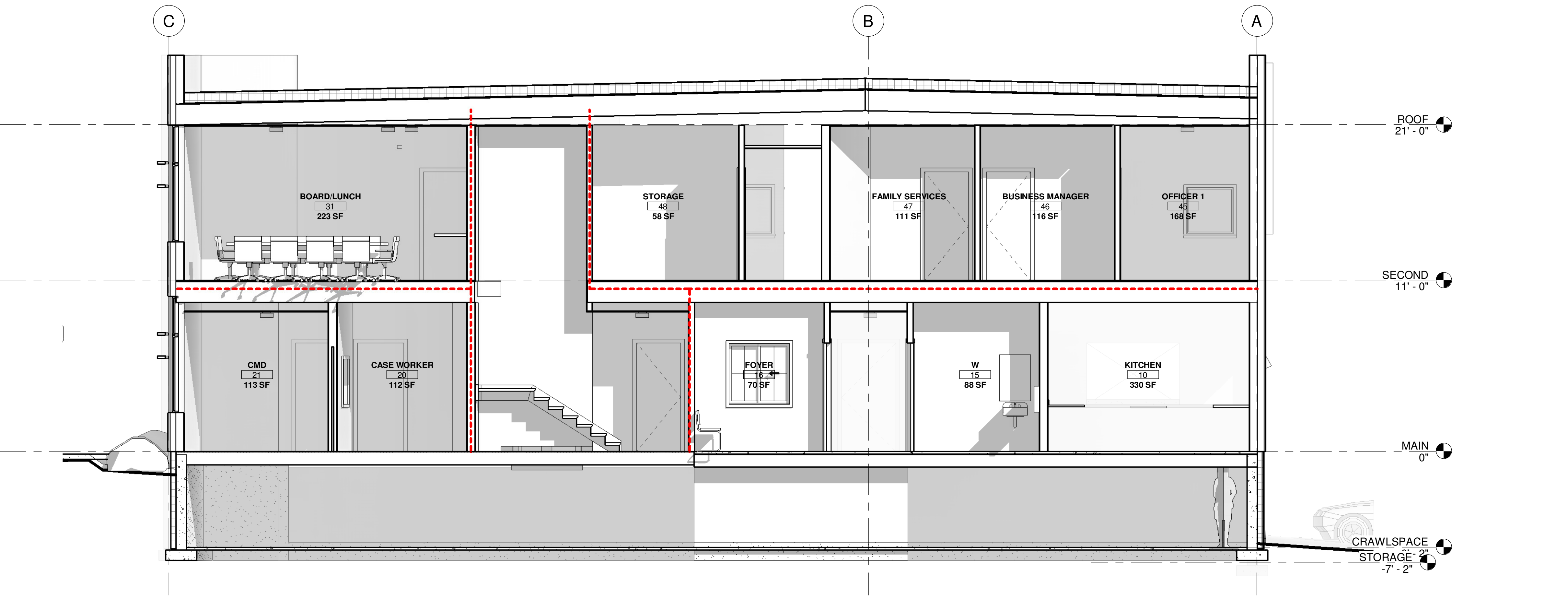


1 N/S SECTION 1
 1/4" = 1'-0"

--- Fire Separation

LEGEND
 1/4" = 1'-0"

**PRELIMINARY
 NOT FOR
 CONSTRUCTION**



2 N/S SECTION 2
 1/4" = 1'-0"

No.	Description	Date
REVISIONS:		
ISSUED:		
For		Date

**SALVATION ARMY
 - CV COMMUNITY
 CHURCH**

SECTIONS

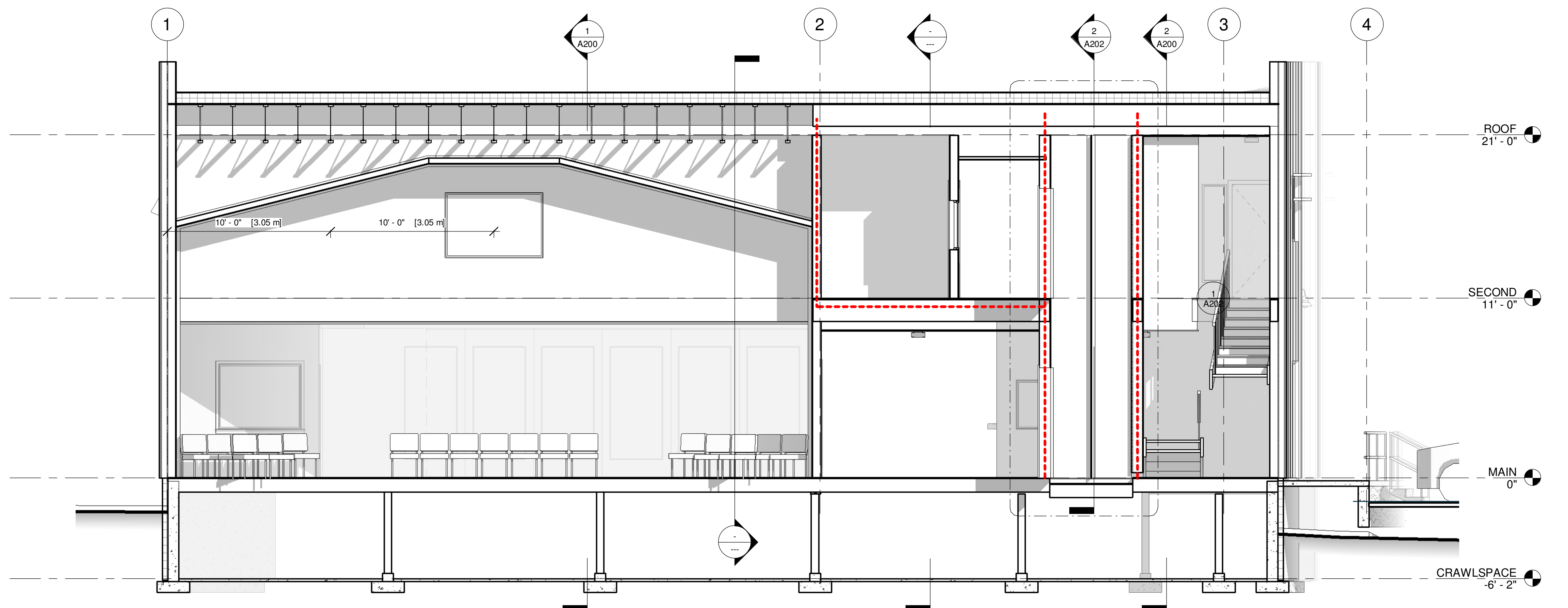
Drawn by	Author	Scale	1/4" = 1'-0"
Checked by	Checker	Date	Issue Date

A200

Project number	Sheet
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Plot date:
 2024-02-22
 10:43:09 AM

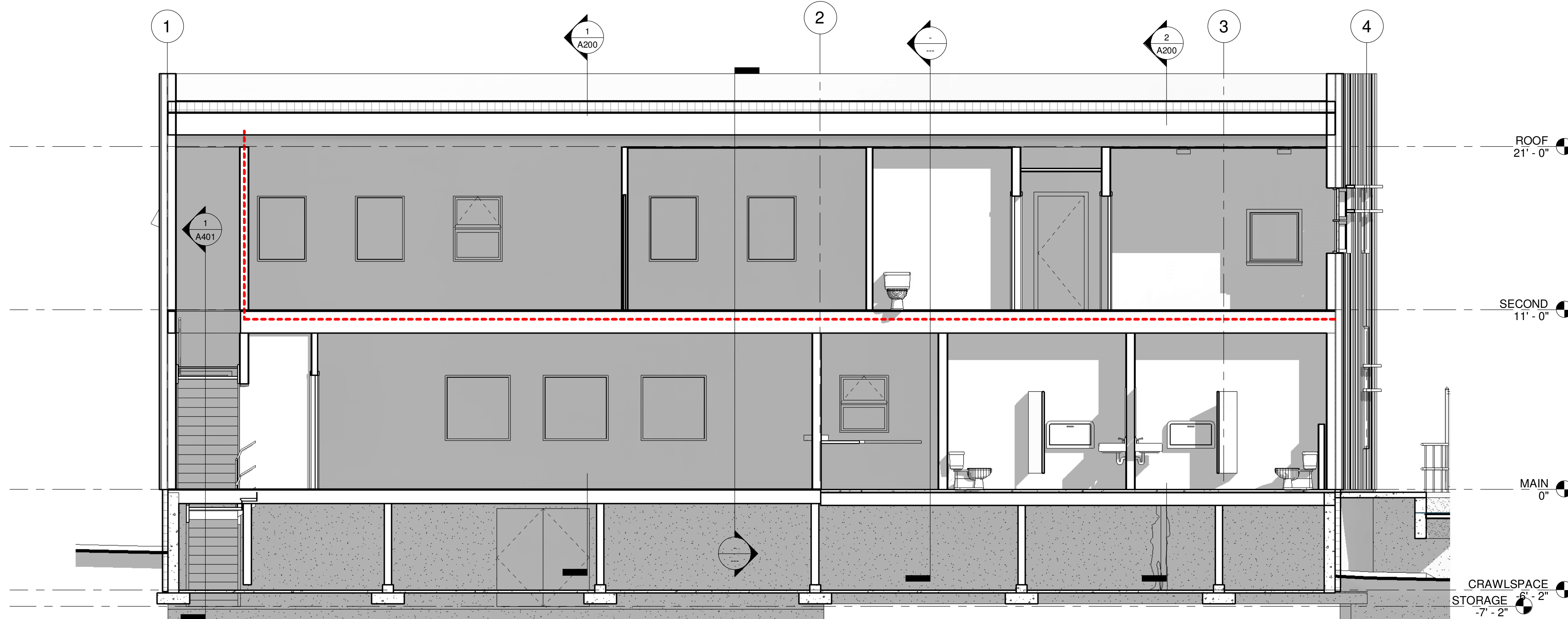
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1 E/W SECTION 1
 1/4" = 1'-0"

--- Fire Separation

LEGEND
 1/4" = 1'-0"



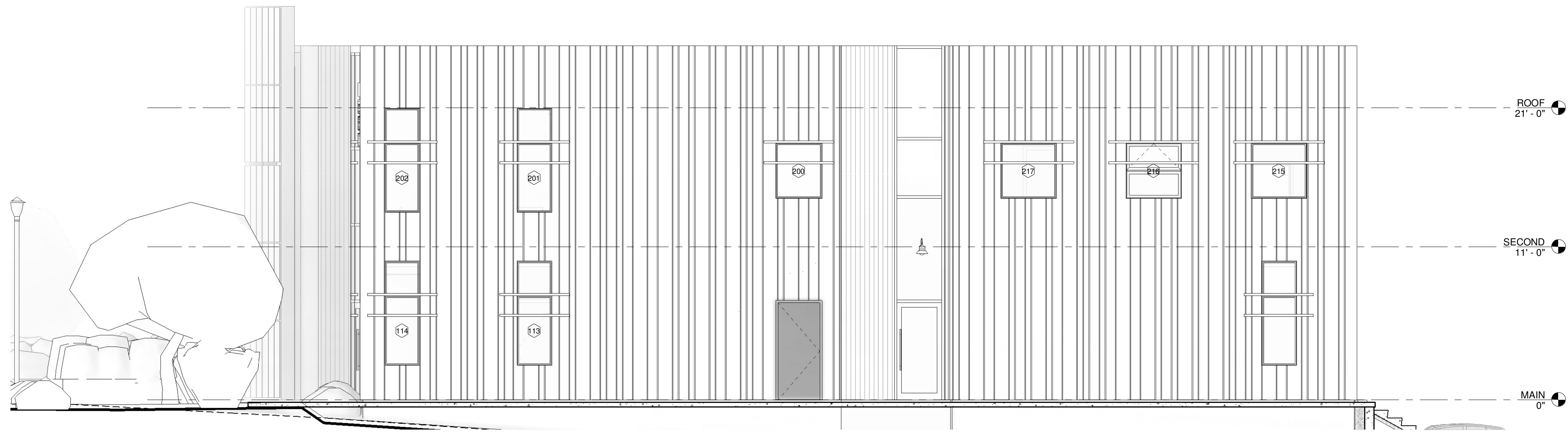
2 E/W SECTION 2
 1/4" = 1'-0"

PRELIMINARY
 NOT FOR
 CONSTRUCTION

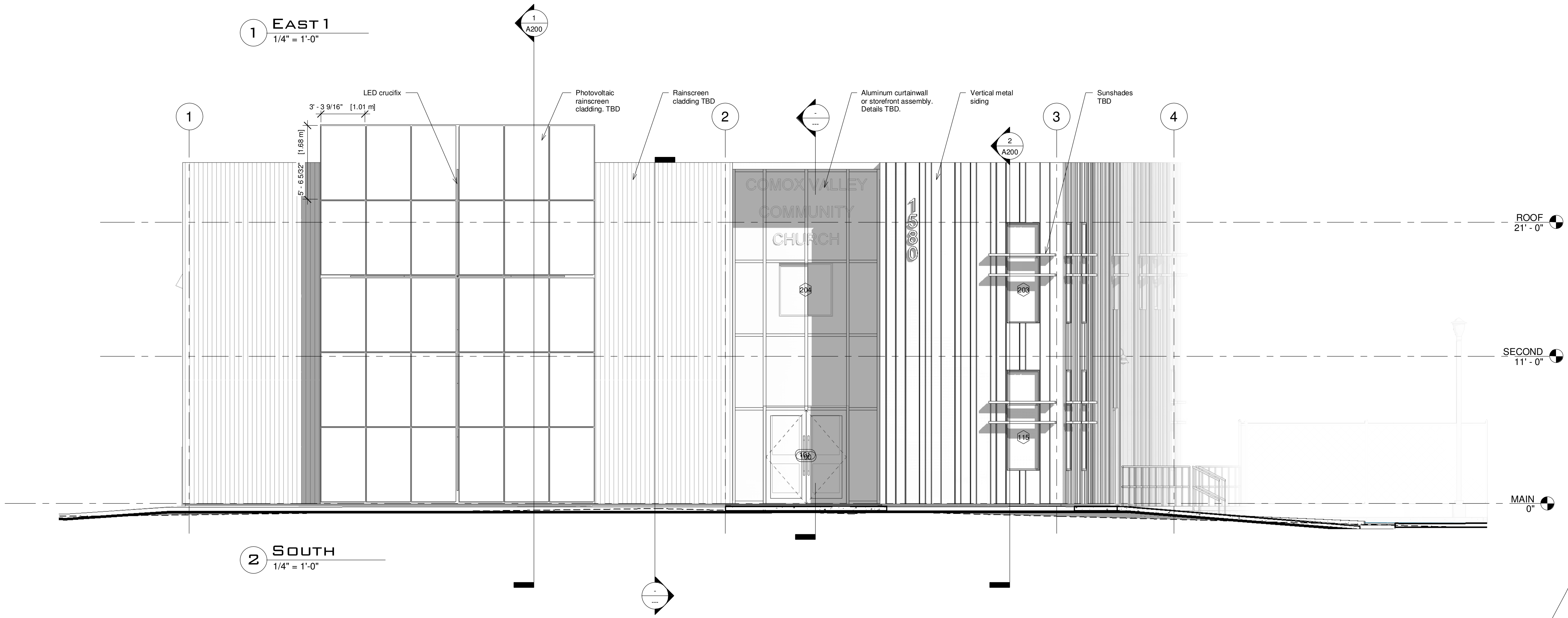
No.	Description	Date
REVISIONS:		
ISSUED:		
For		Date
SALVATION ARMY - CV COMMUNITY CHURCH		
SECTIONS		
Drawn by	Author	Scale 1/4" = 1'-0"
Checked by	Checker	Date Issue Date
A201		
Project number	Sheet	

Plot date:
 2024-02-22
 10:43:10 AM

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1 EAST 1
 1/4" = 1'-0"



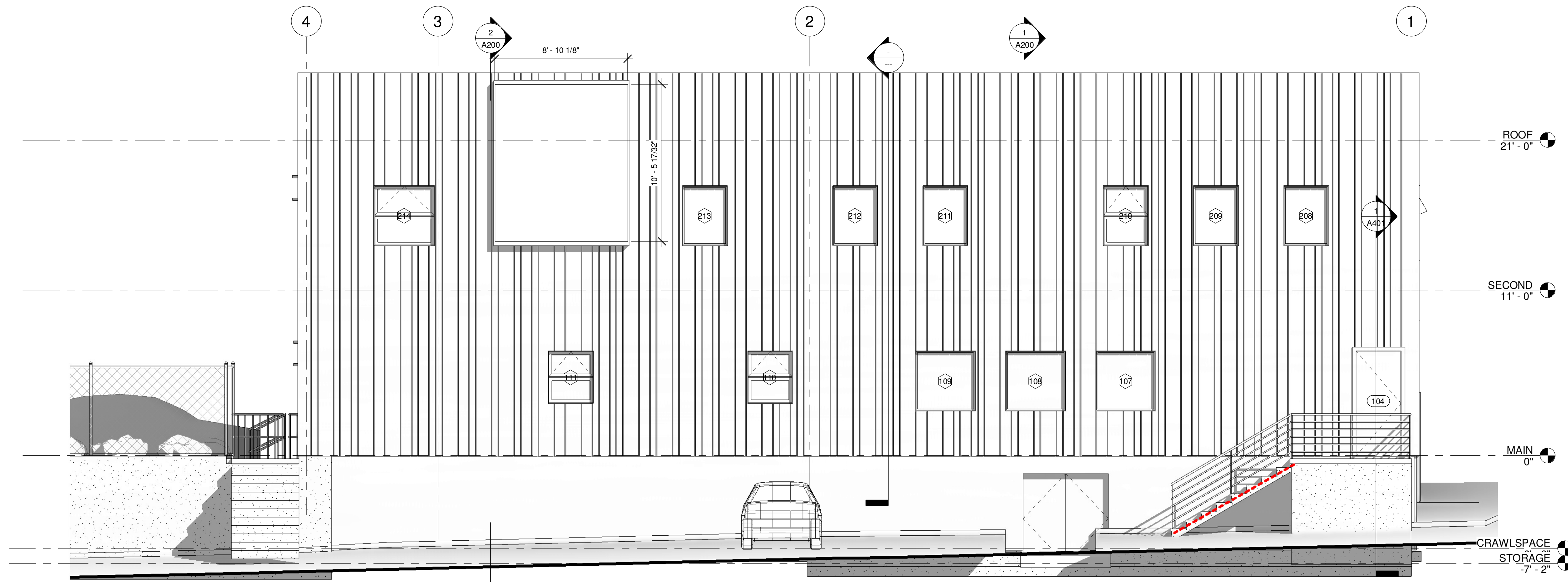
2 SOUTH
 1/4" = 1'-0"

PRELIMINARY
 NOT FOR
 CONSTRUCTION

No.	Description	Date
REVISIONS:		
ISSUED:		
For		Date
SALVATION ARMY - CV COMMUNITY CHURCH		
ELEVATIONS		
Drawn by	Author	Scale 1/4" = 1'-0"
Checked by	Checker	Date Issue Date
A300		
Project number	Sheet	

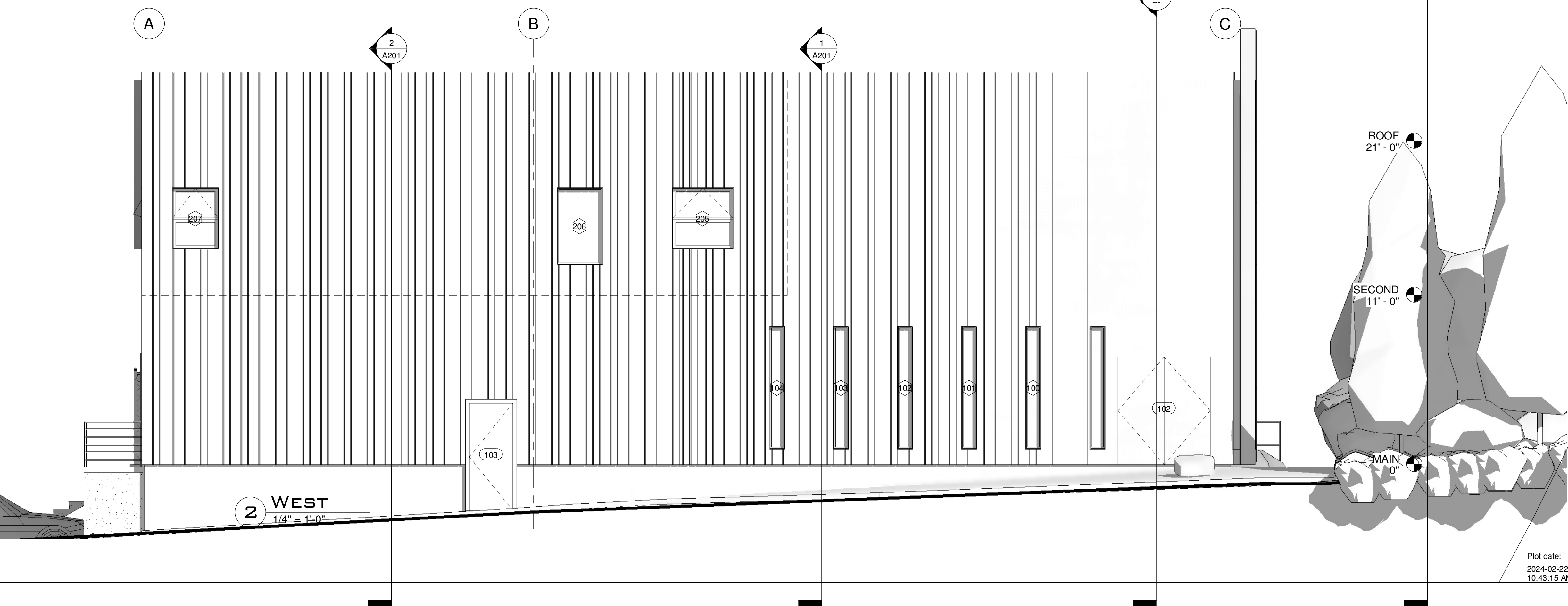
Plot date:
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1 NORTH
 1/4" = 1'-0"

PRELIMINARY
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 CONSTRUCTION

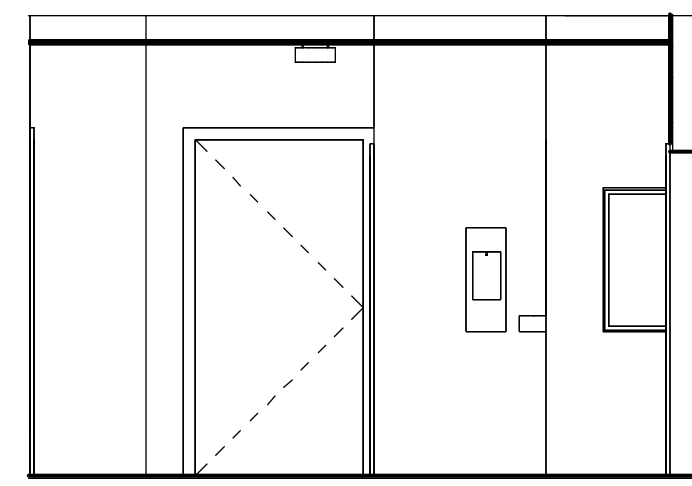


2 WEST
 1/4" = 1'-0"

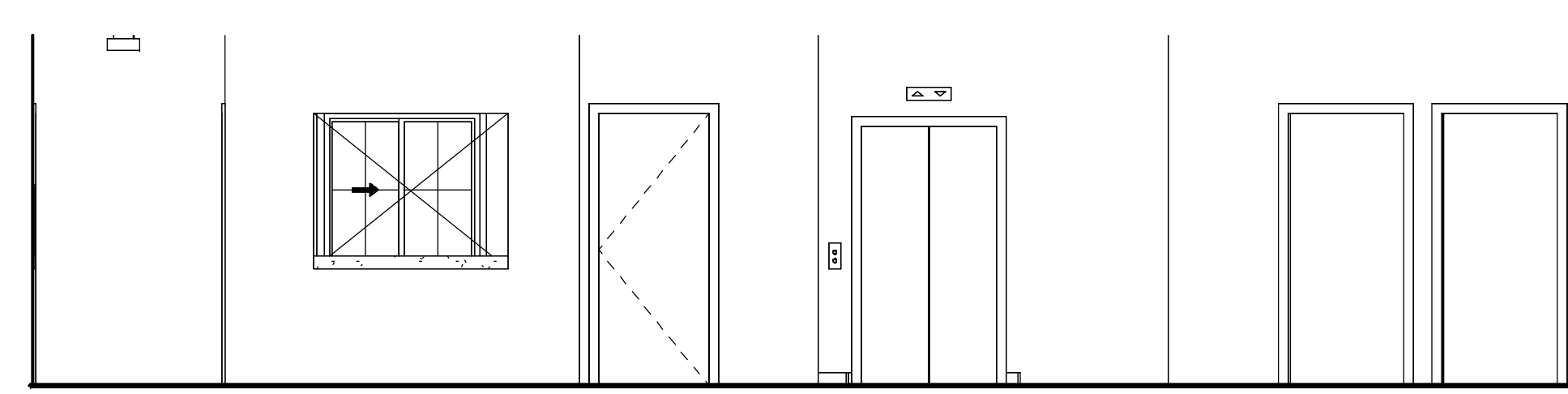
No.	Description	Date	
REVISIONS:			
ISSUED:			
For		Date	
<p>SALVATION ARMY - CV COMMUNITY CHURCH</p> <p>ELEVATIONS</p>			
Drawn by	Author	Scale	1/4" = 1'-0"
Checked by	Checker	Date	Issue Date
A301			
Project number	Sheet		

Plot date:
 2024-02-22
 10:43:15 AM

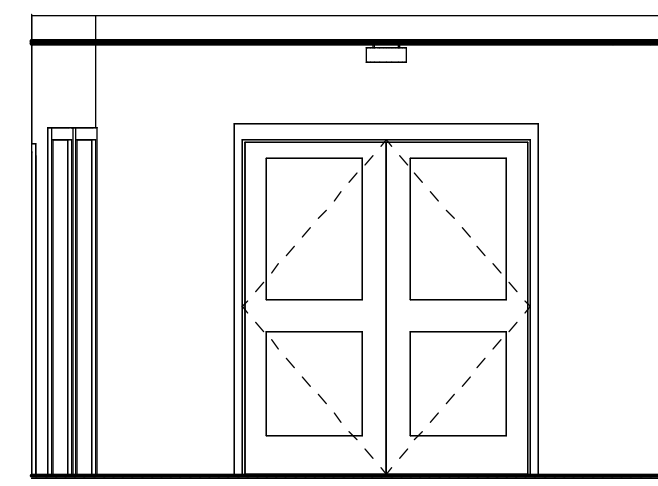
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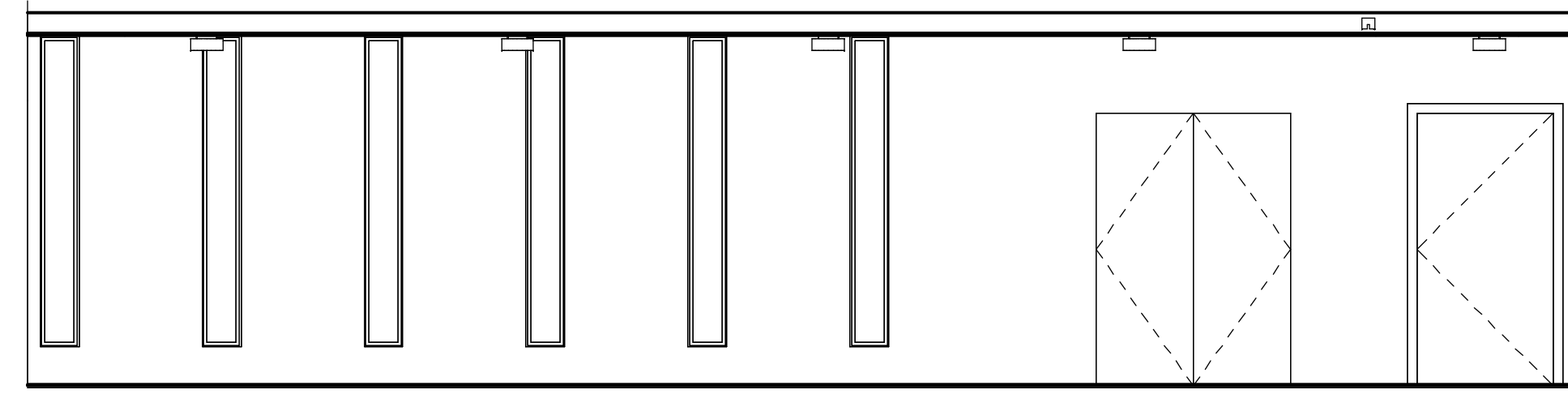
5 1N
1/4" = 1'-0"



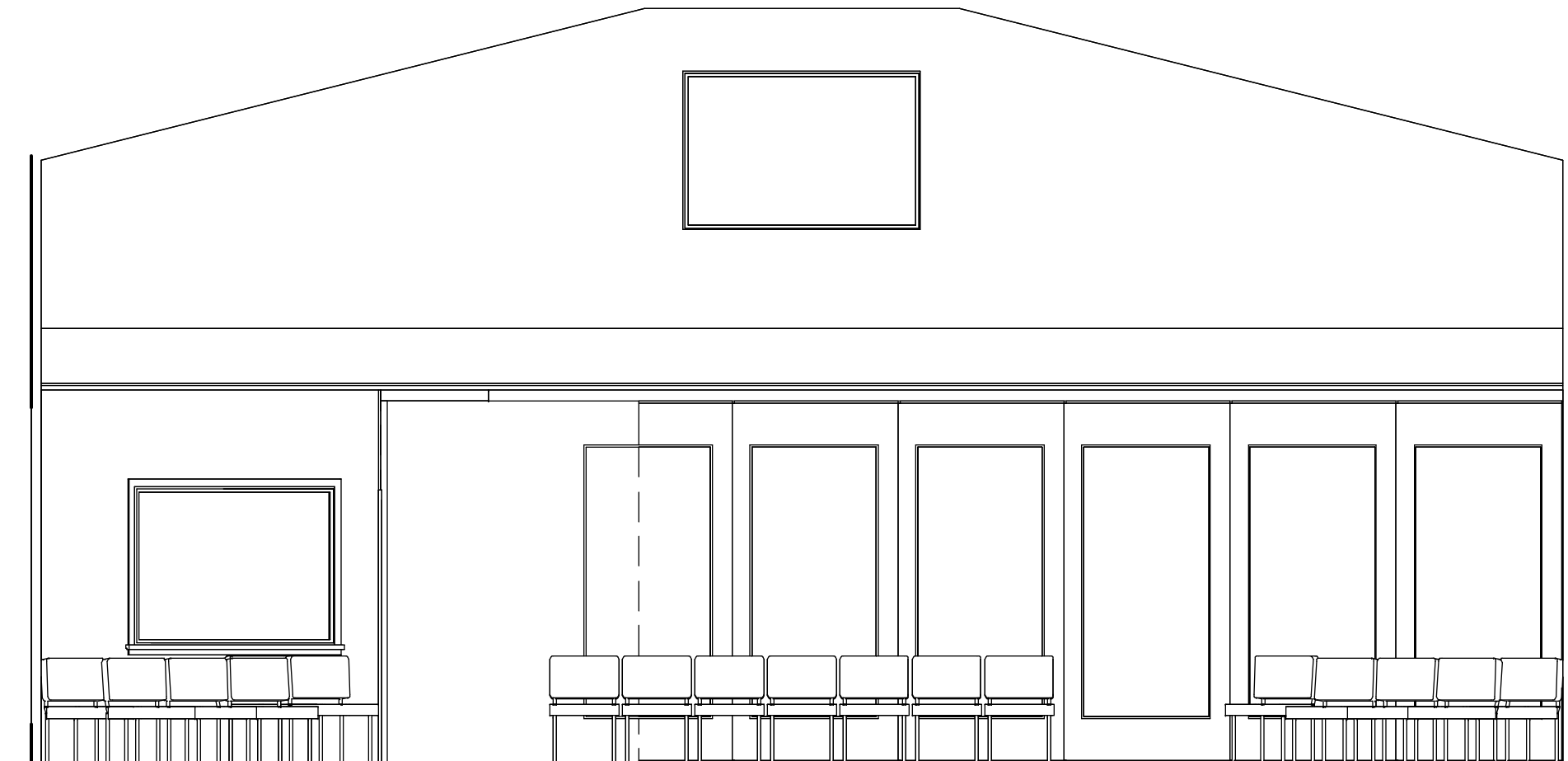
6 1E
1/4" = 1'-0"



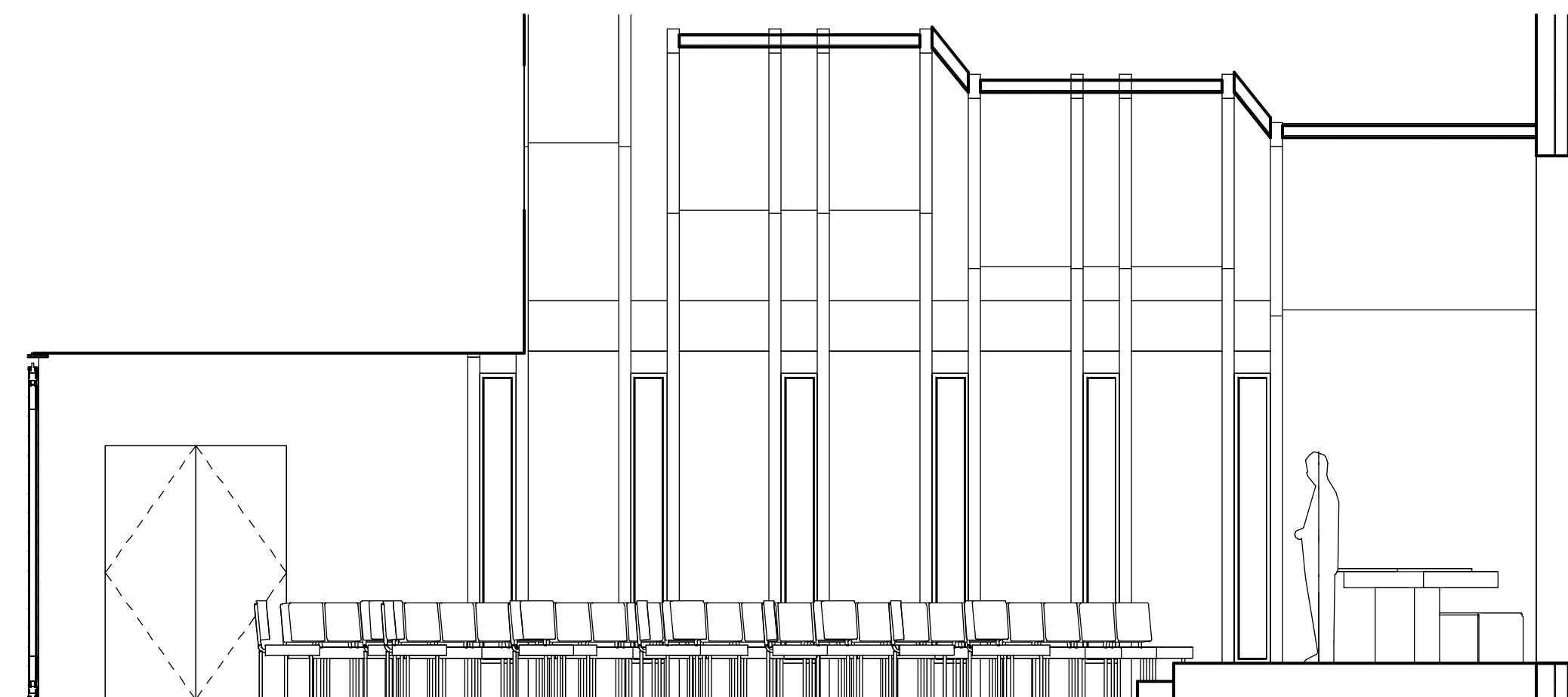
7 1S
1/4" = 1'-0"



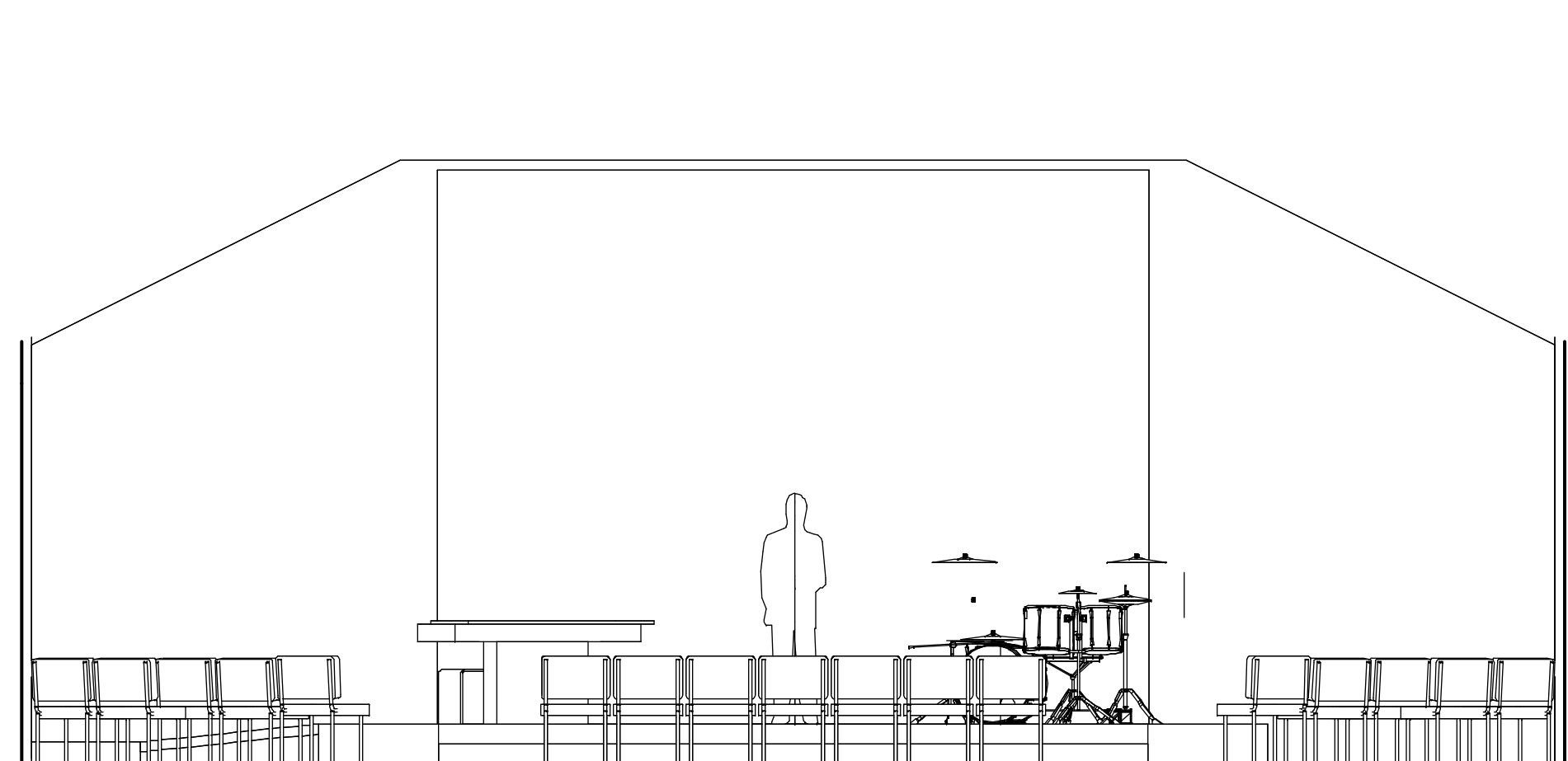
8 1W
1/4" = 1'-0"



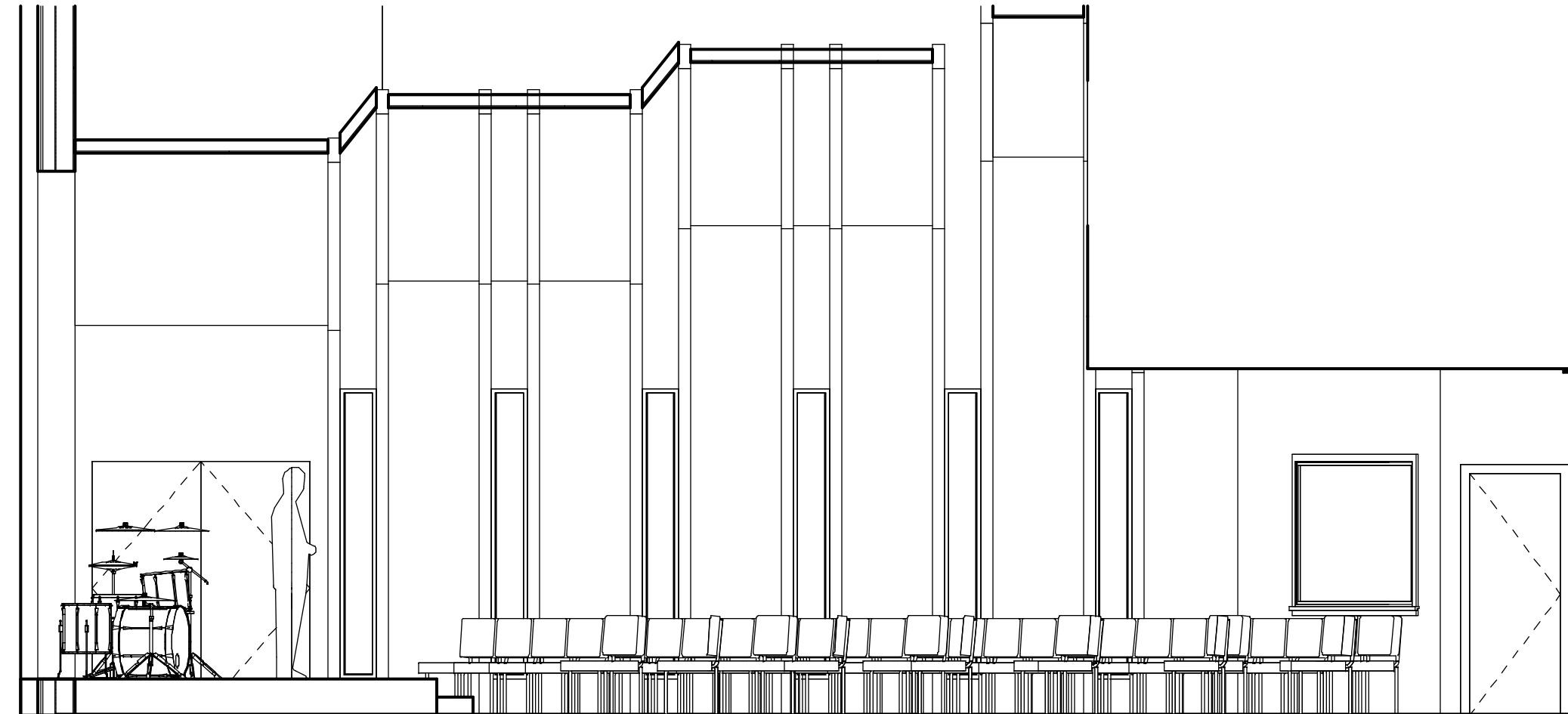
9 2N
1/4" = 1'-0"



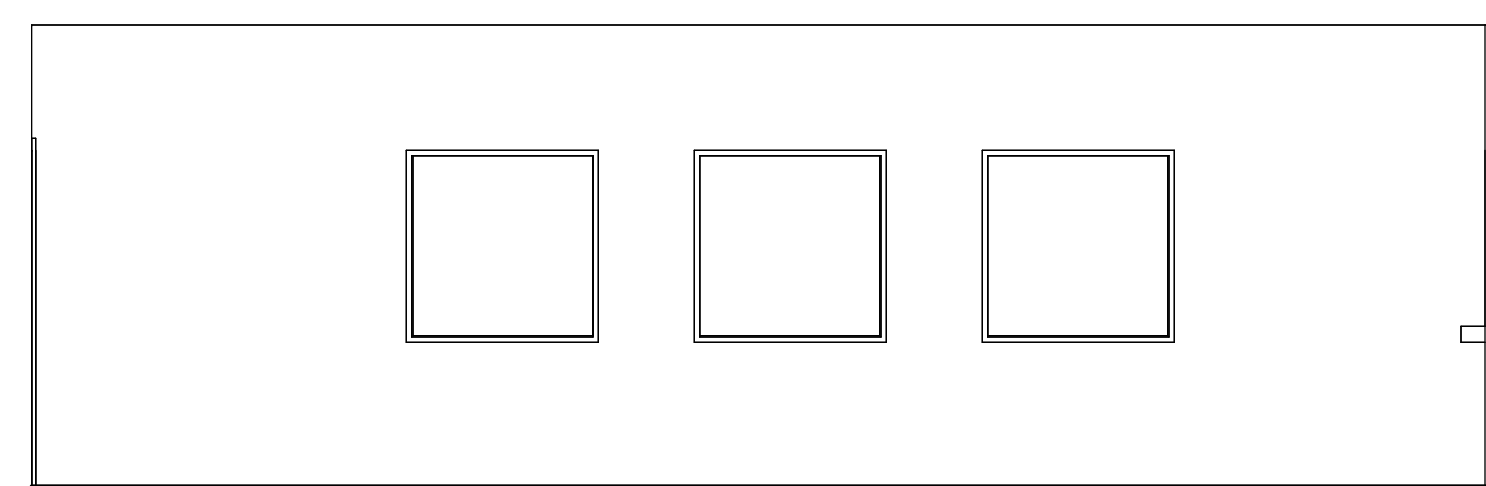
10 2E
1/4" = 1'-0"



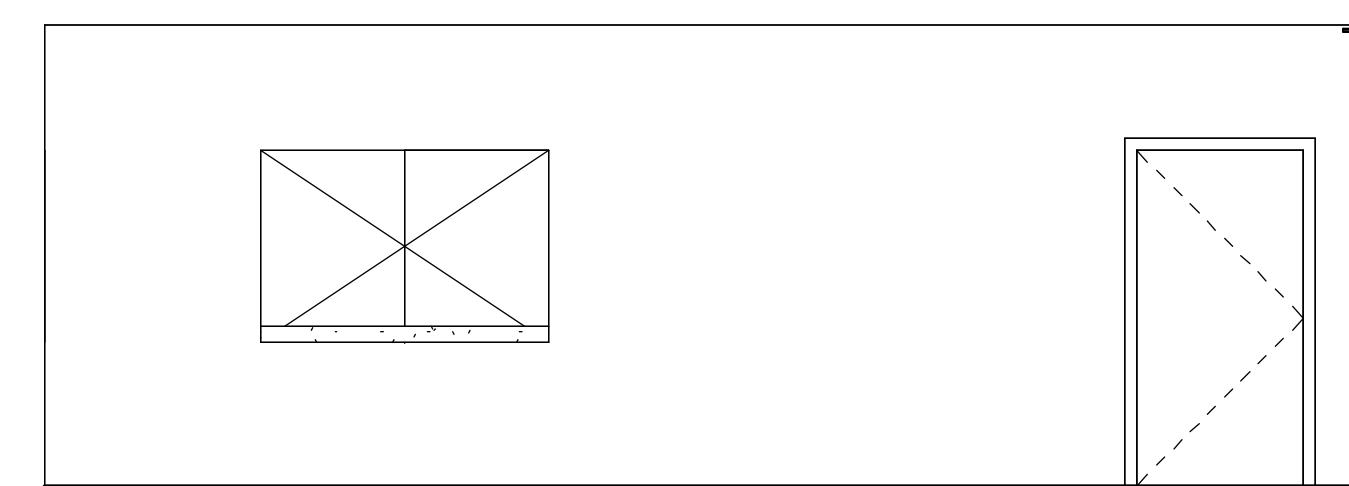
11 2S
1/4" = 1'-0"



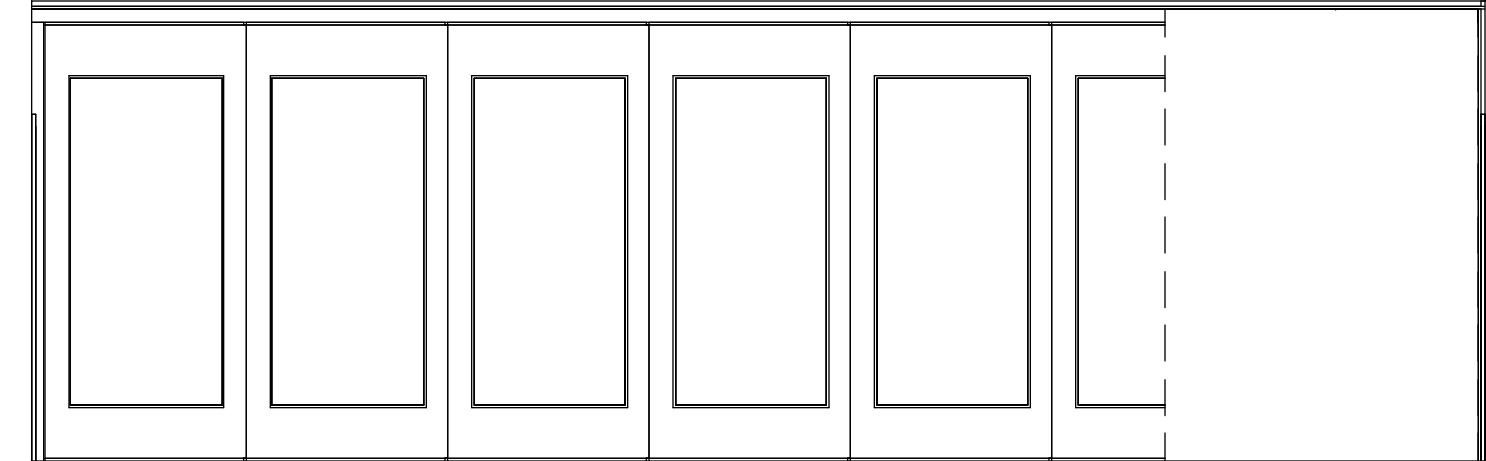
12 2W
1/4" = 1'-0"



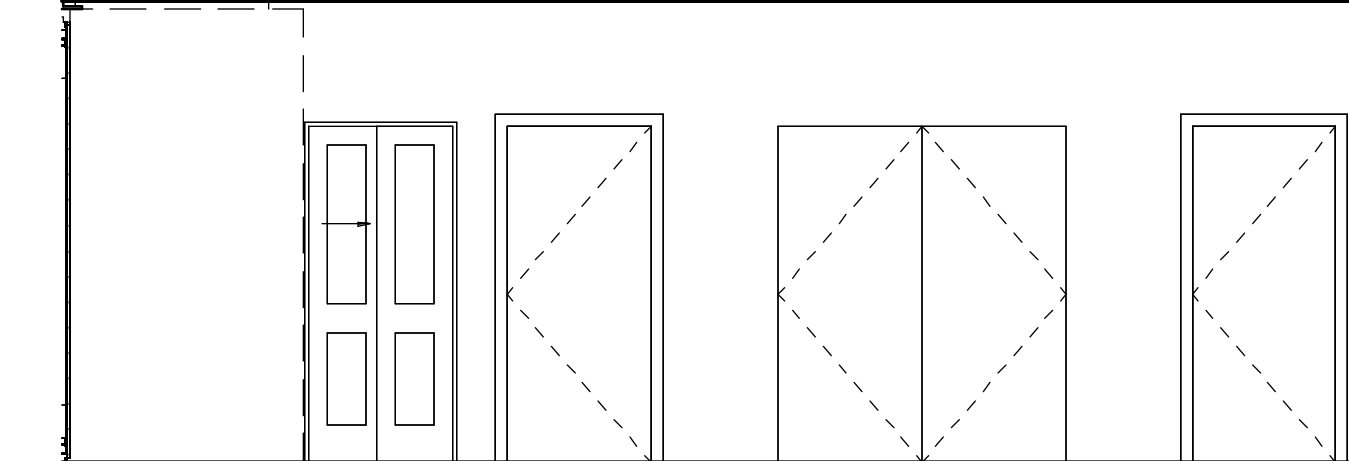
13 4N
1/4" = 1'-0"



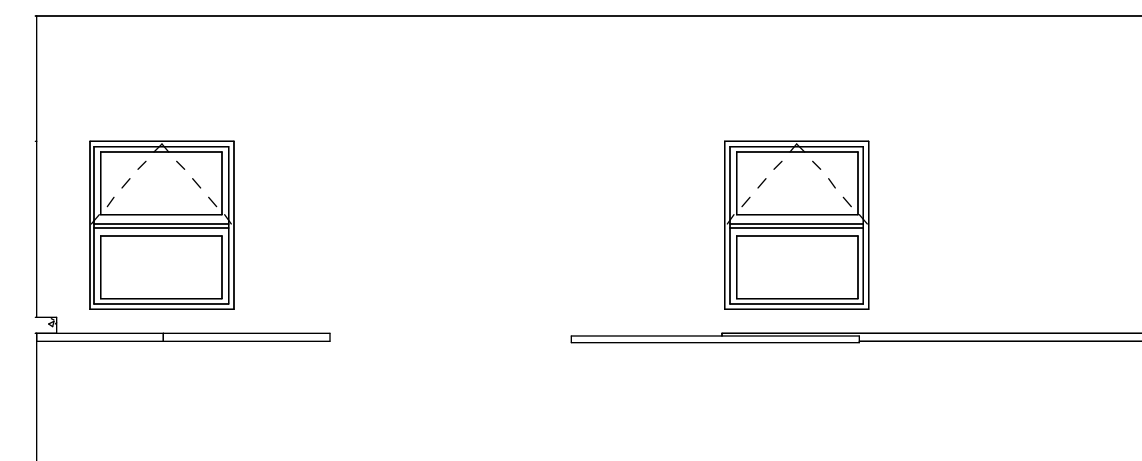
14 4E
1/4" = 1'-0"



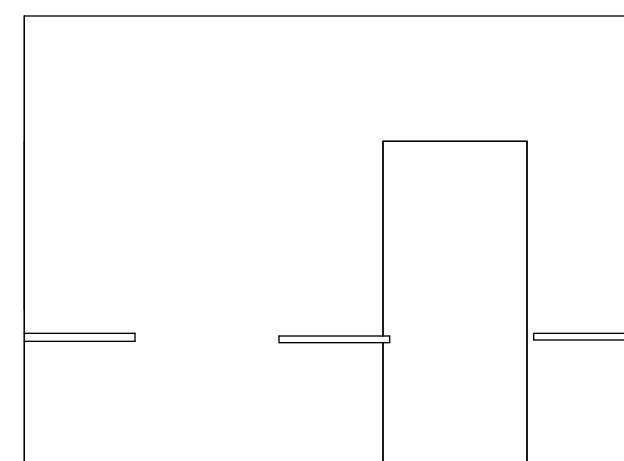
15 4S
1/4" = 1'-0"



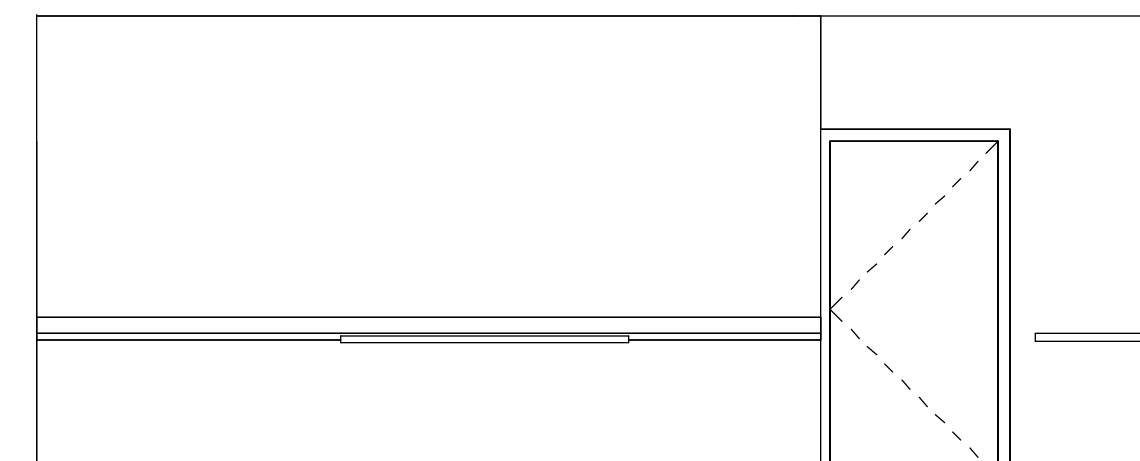
16 4W
1/4" = 1'-0"



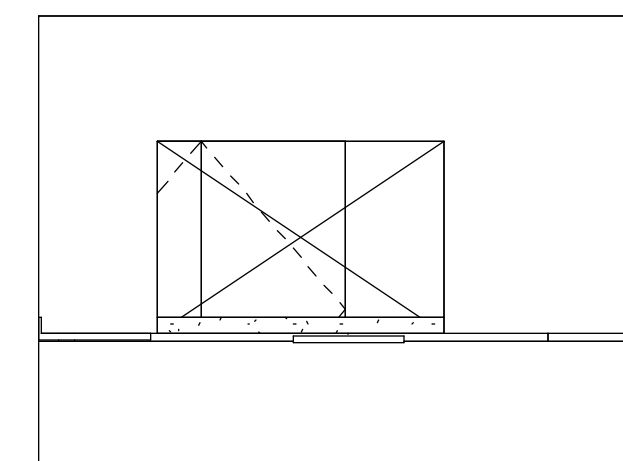
1 10N
1/4" = 1'-0"



2 10E
1/4" = 1'-0"



3 10S
1/4" = 1'-0"



4 10W
1/4" = 1'-0"

No.	Description	Date
REVISIONS:		
ISSUED:		
For		Date

**SALVATION ARMY
 - CV COMMUNITY
 CHURCH**

**INTERIOR
 ELEVATIONS**

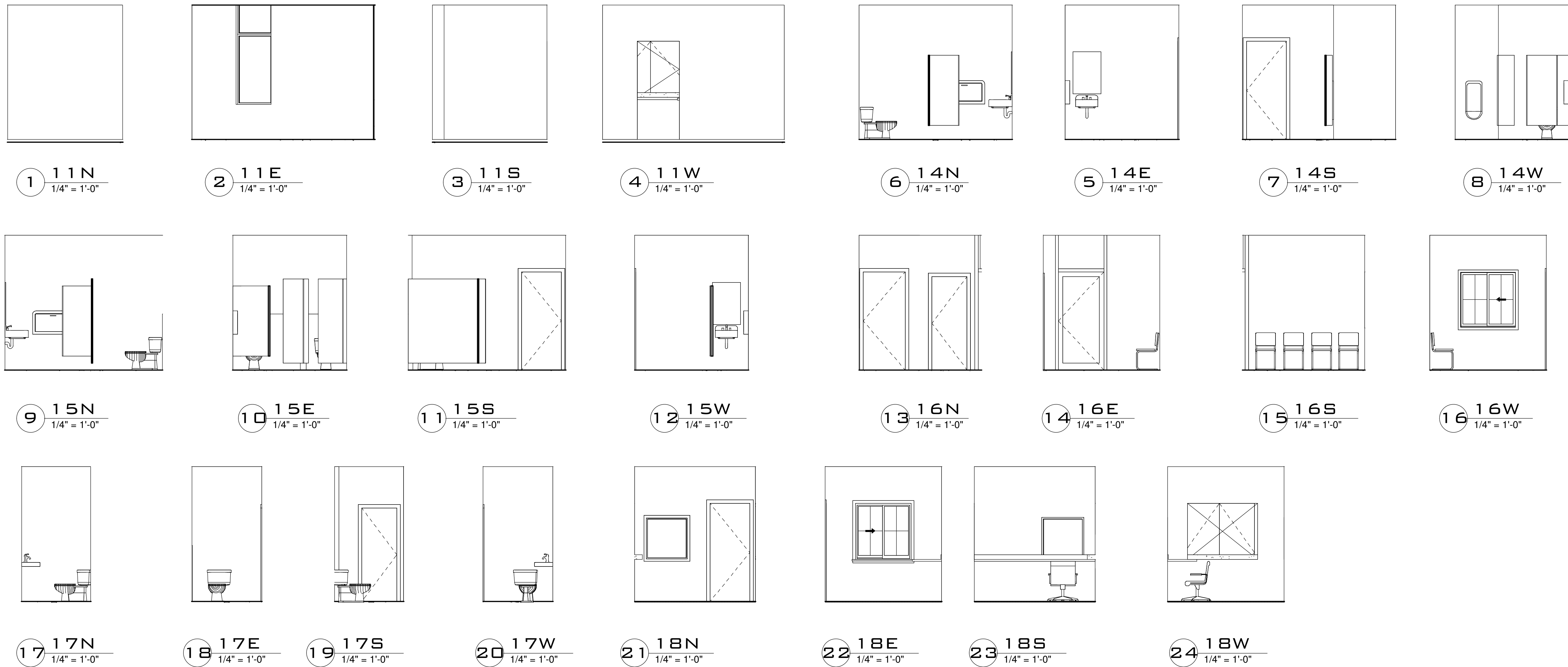
Drawn by	Author	Scale	1/4" = 1'-0"
Checked by	Checker	Date	Issue Date

A302

Project number	Sheet
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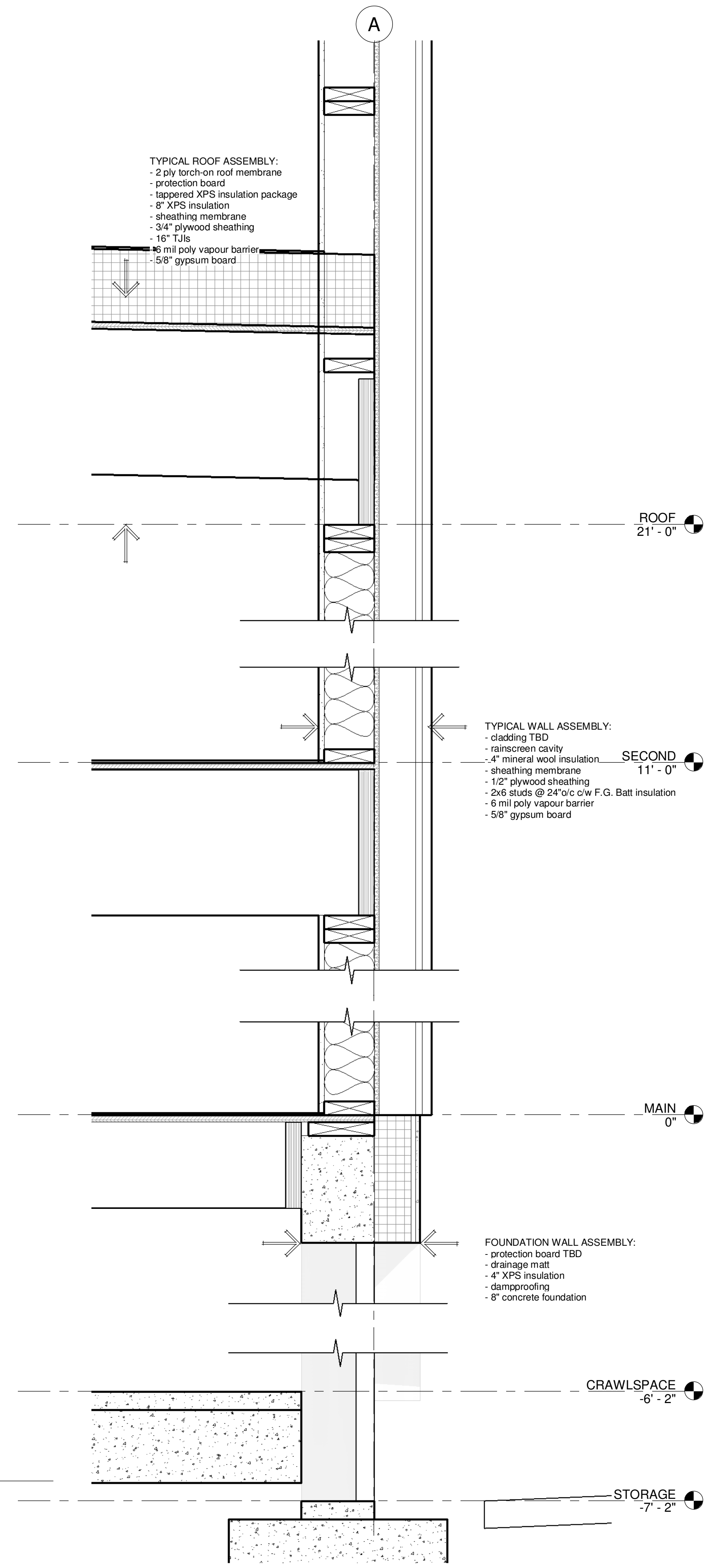
MAIN FLOOR ROOM SCHEDULE								
Number	Name	Area	Floor Finish	Base Finish	Wall Finish	Ceiling Finish	Level	Comments
1	MAIN FOYER	531 SF	Vinyl plank				MAIN	
2	SANCTUARY	1512 SF	Vinyl plank				MAIN	
3	SOUND BOOTH	50 SF	Vinyl plank				MAIN	
4	HALL	919 SF	Vinyl plank				MAIN	
5	CLOSET	9 SF	Vinyl plank				MAIN	
6	CRAWLSPACE ACCESS	69 SF	Vinyl plank				MAIN	
7	STORAGE	37 SF	Vinyl plank				MAIN	
8	N/W EXIT	36 SF	Vinyl plank				MAIN	
9	SUPPLIES	9 SF	Vinyl plank				MAIN	
10	KITCHEN	330 SF	Concrete				MAIN	
11	PANTRY	106 SF	Concrete				MAIN	
12	JAN	11 SF	Concrete				MAIN	
13	CORRIDOR	67 SF	Vinyl plank				MAIN	
14	M	75 SF	Concrete				MAIN	
15	W	88 SF	Concrete				MAIN	
16	FOYER	70 SF	Concrete				MAIN	
17	W/C	24 SF	Concrete				MAIN	
18	RECEPTION	72 SF	Vinyl plank				MAIN	
19	STAIRWELL	129 SF					MAIN	
20	CASE WORKER	112 SF	Vinyl plank				MAIN	
21	CMD	113 SF	Vinyl plank				MAIN	
64	ELECTRICAL	44 SF					MAIN	

SECOND FLOOR ROOM SCHEDULE								
Number	Name	Area	Floor Finish	Base Finish	Wall Finish	Ceiling Finish	Level	Comments
30	Room	578 SF					SECOND	
31	BOARD/LUNCH	223 SF					SECOND	
32	Room	8 SF					SECOND	
33	OFFICER 2	124 SF					SECOND	
34	ADMIN	92 SF					SECOND	
35	WORK	103 SF					SECOND	
36	HEAD STRONG 1	106 SF					SECOND	
37	STORAGE	86 SF					SECOND	
38	HEAD STRONG 2	143 SF					SECOND	
39	YOUTH PROGRAM	590 SF					SECOND	
40	CHILDREN	332 SF					SECOND	
41	Room	20 SF					SECOND	
42	Room	13 SF					SECOND	
43	W/C	73 SF					SECOND	
44	YOUTH COORDINATOR	115 SF					SECOND	
45	OFFICER 1	168 SF					SECOND	
46	BUSINESS MANAGER	116 SF					SECOND	
47	FAMILY SERVICES	111 SF					SECOND	
48	STORAGE	58 SF					SECOND	

No.	Description	Date
REVISIONS:		
ISSUED:		
For	Date	
SALVATION ARMY - CV COMMUNITY CHURCH		
INTERIOR ELEVATIONS		
Drawn by	Author	Scale 1/4" = 1'-0"
Checked by	Checker	Date Issue Date
A303		
Project number	Sheet	

Plot date:
2024-02-22
10:43:17 AM

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1 TYPICAL WALL
 1 1/2" = 1'-0"

PRELIMINARY
 NOT FOR
 CONSTRUCTION

No.	Description	Date
REVISIONS:		
ISSUED:		
For		Date

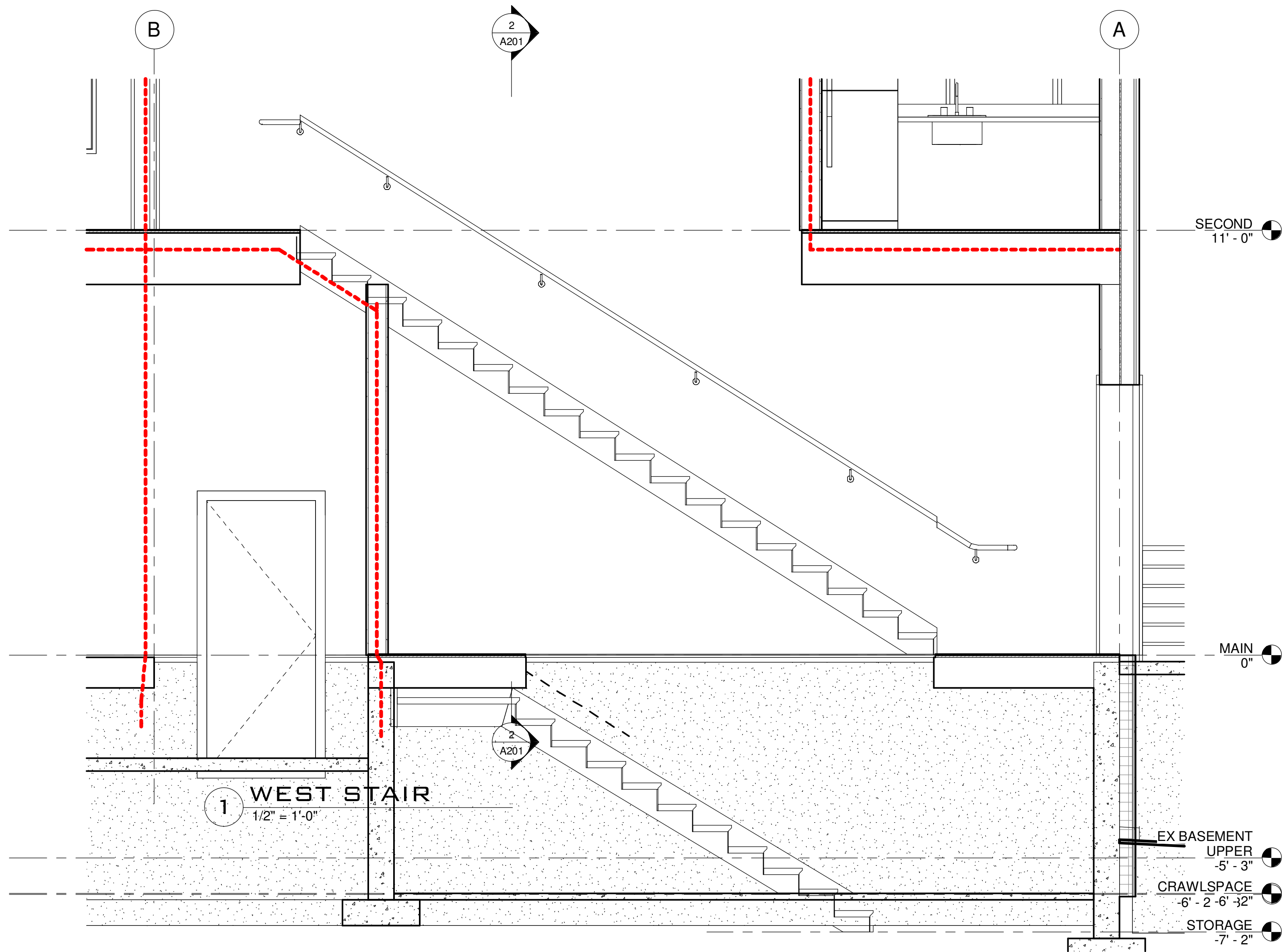
**SALVATION ARMY
 - CV COMMUNITY
 CHURCH**

DETAILS

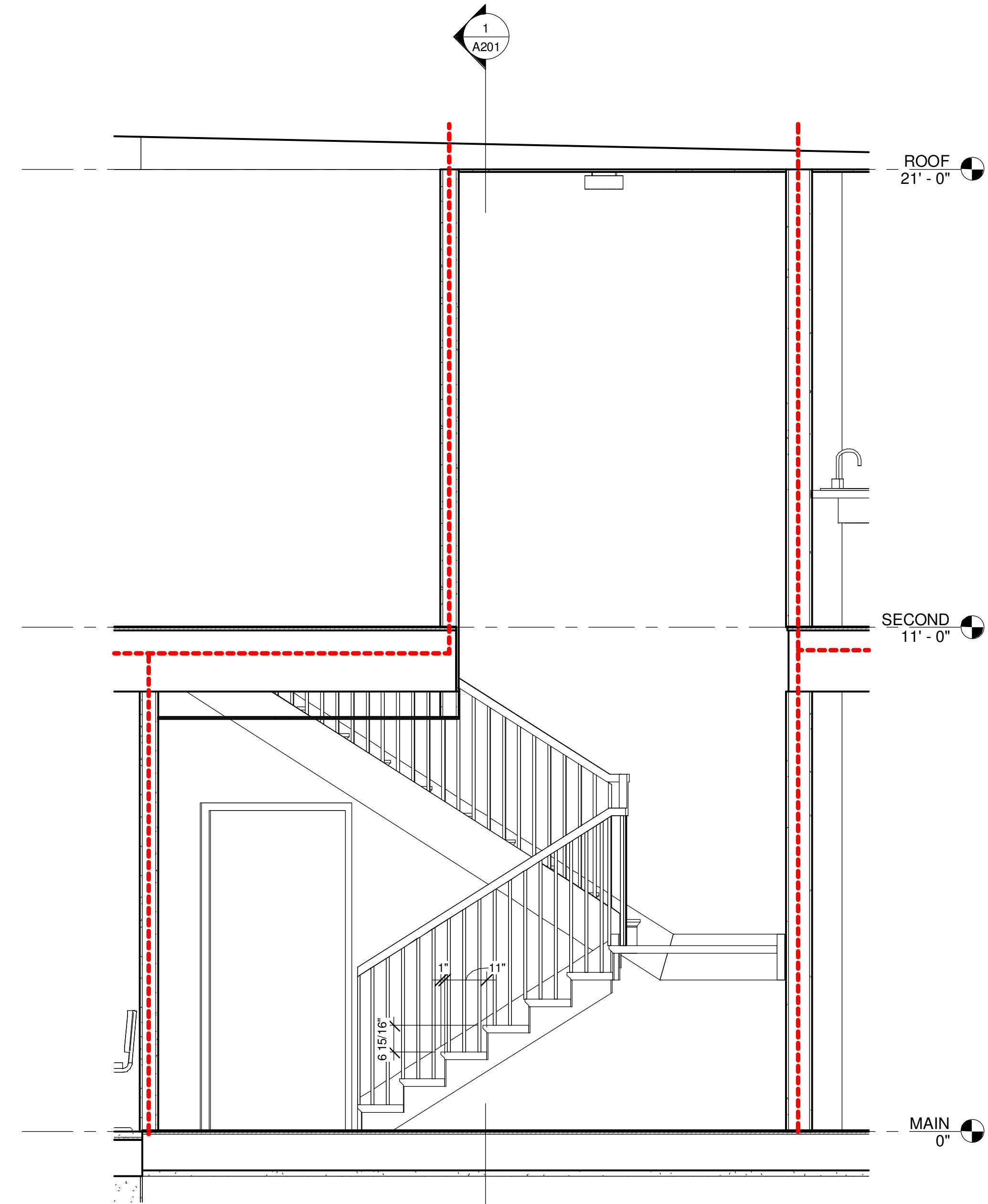
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Checked by	Checker	Date	Issue Date
A400		Project number	Sheet

Plot date:
 2024-02-22
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1 WEST STAIR
 1/2" = 1'-0"



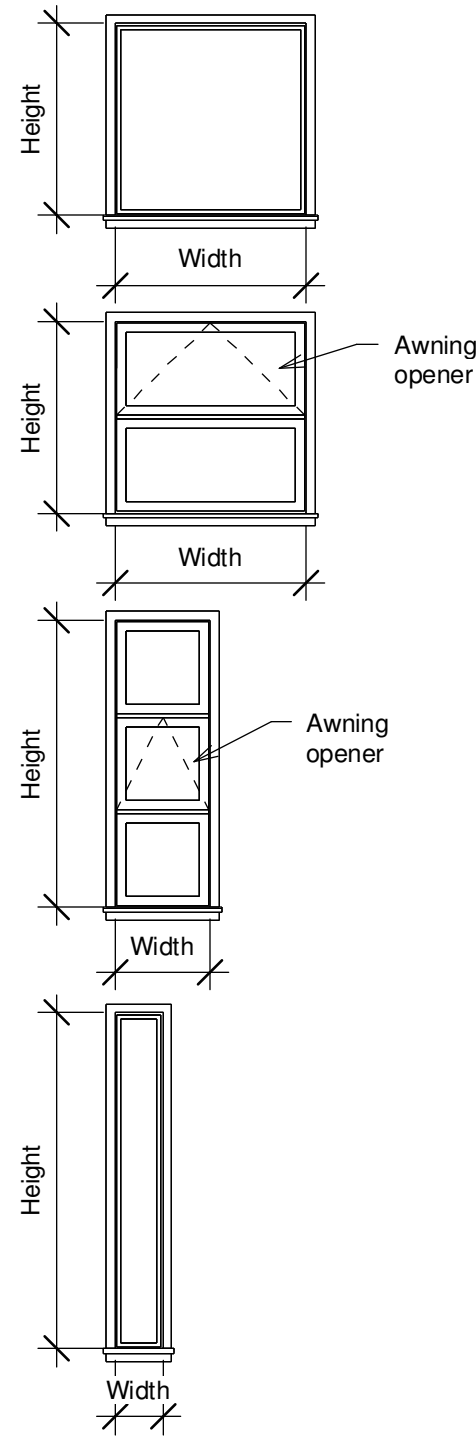
2 EAST STAIR
 1/2" = 1'-0"

No.	Description	Date
REVISIONS:		
ISSUED:		
For		Date
SALVATION ARMY - CV COMMUNITY CHURCH		
DETAILS		
Drawn by	Author	Scale 1/2" = 1'-0"
Checked by	Checker	Date Issue Date
A401		
Project number	Sheet	

Plot date:
 2024-02-22
 10:43:18 AM

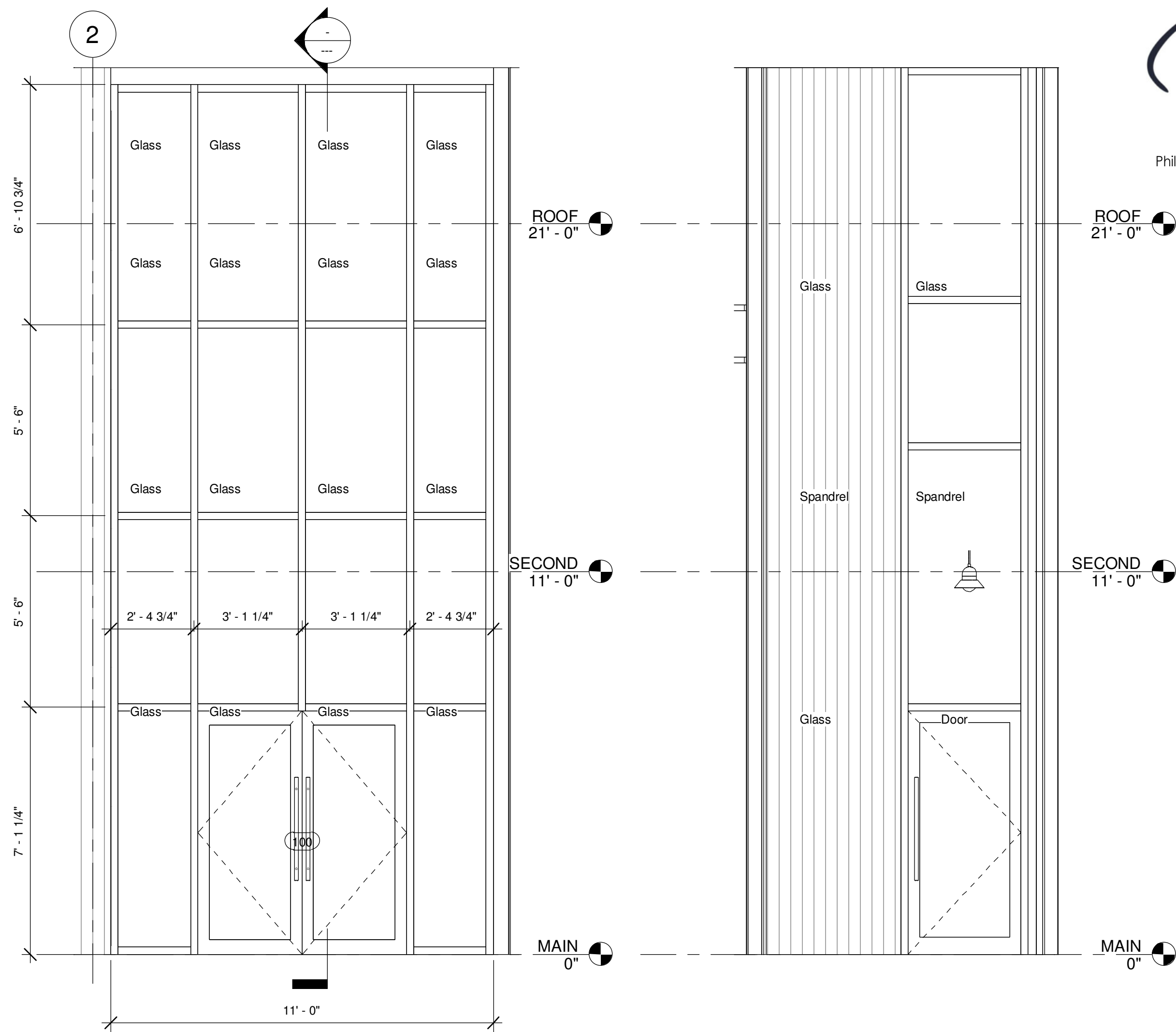
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MASTER WINDOW SCHEDULE				
Mark	Width	Height	Level	Comments
100	1'-0"	8'-0"	MAIN	
101	1'-0"	8'-0"	MAIN	
102	1'-0"	8'-0"	MAIN	
103	1'-0"	8'-0"	MAIN	
104	1'-0"	8'-0"	MAIN	
107	4'-0"	4'-0"	MAIN	
108	4'-0"	4'-0"	MAIN	
109	4'-0"	4'-0"	MAIN	
110	3'-0"	3'-6"	MAIN	
111	3'-0"	3'-6"	MAIN	
112	2'-6"	5'-0"	MAIN	
113	2'-6"	5'-0"	MAIN	
114	2'-6"	5'-0"	MAIN	
115	2'-6"	5'-0"	MAIN	
116	1'-0"	8'-0"	MAIN	Interior
117	1'-0"	8'-0"	MAIN	Interior
118	1'-0"	8'-0"	MAIN	Interior
119	1'-0"	8'-0"	MAIN	Interior
120	1'-0"	8'-0"	MAIN	Interior
121	2'-6"	2'-6"	MAIN	
122	1'-0"	8'-0"	MAIN	
123	1'-0"	8'-0"	MAIN	Interior
124	3'-0"	3'-0"	MAIN	Interior
126	3'-0"	3'-0"	MAIN	Interior
128	3'-0"	3'-0"	MAIN	Interior
190	0"	0"	MAIN	
191	0"	0"	MAIN	
193	2'-6"	2'-6"	MAIN	
200	3'-4"	4'-0"	SECOND	
201	2'-6"	5'-0"	SECOND	
202	2'-6"	5'-0"	SECOND	
203	2'-6"	5'-0"	SECOND	
204	4'-0"	4'-0"	SECOND	
205	4'-0"	4'-0"	SECOND	
206	3'-0"	5'-0"	SECOND	
207	3'-0"	4'-0"	SECOND	
208	3'-0"	4'-0"	SECOND	
209	3'-0"	4'-0"	SECOND	
210	3'-0"	4'-0"	SECOND	
211	3'-0"	4'-0"	SECOND	
212	3'-0"	4'-0"	SECOND	
213	3'-0"	4'-0"	SECOND	
214	4'-0"	4'-0"	SECOND	
215	4'-0"	4'-0"	SECOND	
216	4'-0"	4'-0"	SECOND	
217	4'-0"	4'-0"	SECOND	
219	3'-0"	3'-0"	SECOND	Sound
220	3'-0"	3'-0"	SECOND	Interior
221	3'-0"	3'-0"	SECOND	Interior
222	6'-0"	4'-0"	SECOND	Sound, Wired glass.
223	3'-0"	3'-0"	SECOND	Interior
224	3'-0"	3'-0"	SECOND	Interior
225	3'-0"	3'-0"	SECOND	Interior
225	2'-6"	2'-6"	SECOND	
226	1'-6"	6'-0"	SECOND	Wired glass
232	4'-0"	4'-0"	MAIN	
233	2'-6"	2'-6"	MAIN	
234	2'-6"	2'-6"	SECOND	
235	2'-6"	2'-6"	SECOND	
236	2'-6"	2'-6"	MAIN	
237	6'-0"	4'-0"	MAIN	



1/4" = 1'-0"

MASTER DOOR SCHEDULE							
Mark	Width	Height	Fire Rating	Construction Type	Frame Material	Level	Comments
100	6'-0"	7'-0"				MAIN	
101	6'-0"	7'-0"				MAIN	
102	6'-0"	7'-0"				MAIN	
103	2'-10"	7'-0"				MAIN	
104	3'-0"	7'-0"				MAIN	
105	3'-3"	7'-0"				MAIN	
106	3'-0"	7'-0"			Wood	MAIN	
107	5'-0"	7'-0"				MAIN	
110	3'-0"	7'-0"				MAIN	
111	6'-0"	7'-0"				MAIN	
112	3'-0"	7'-0"				MAIN	
113	3'-6"	7'-0"				MAIN	
114	0"	0"				MAIN	
115	2'-6"	6'-8"				MAIN	
116	2'-10"	7'-0"				MAIN	
117	2'-10"	7'-0"				MAIN	
118	3'-0"	7'-0"				MAIN	
119	2'-6"	6'-8"				MAIN	
120	2'-10"	7'-0"				MAIN	
121	2'-10"	7'-0"				MAIN	
122	3'-0"	7'-0"				MAIN	
123	2'-6"	6'-8"				MAIN	
124	3'-0"	7'-0"				MAIN	
200	3'-0"	7'-0"				SECOND	
201	2'-10"	7'-0"				SECOND	
202	2'-10"	7'-0"				SECOND	
203	2'-10"	7'-0"				SECOND	
204	2'-10"	7'-0"				SECOND	
205	3'-0"	7'-0"		Solid core dutch door	Wood	SECOND	Dutch door
206	3'-0"	7'-0"				SECOND	
207	3'-0"	7'-0"				SECOND	
209	3'-0"	7'-0"				SECOND	
210	3'-0"	7'-0"				SECOND	
211	3'-0"	7'-0"				SECOND	
212	3'-0"	7'-0"				SECOND	
213	6'-0"	7'-0"				SECOND	
214	6'-0"	7'-0"				SECOND	
215	2'-6"	6'-8"				SECOND	
216	2'-10"	7'-0"				SECOND	
217	2'-10"	7'-0"				SECOND	
218	2'-10"	7'-0"				SECOND	
219	2'-10"	7'-0"				SECOND	
220	2'-10"	7'-0"				SECOND	
221	4'-0"	7'-0"				SECOND	
222	2'-10"	7'-0"				SECOND	
224	2'-6"	6'-8"				SECOND	
226	5'-8"	6'-0"				STORAGE	
228	2'-6"	6'-8"				MAIN	
229	3'-6"	7'-0"				MAIN	
232	4'-0"	7'-0"				MAIN	
233	3'-0"	7'-0"				MAIN	



1 SOUTH C/W
3/8" = 1'-0"

2 EAST C/W
3/8" = 1'-0"

Phi Architecture Inc.
 Philippa Alwood Architect, ABC, MRAIC, LEED AP
 5 Little Bear Way Royston BC V0R 2V0
 pippa@palwoodarchitect.ca
 250.703.0433 / 250.218.0724

No.	Description	Date
REVISIONS:		
ISSUED:		
For	Date	

SALVATION ARMY - CV COMMUNITY CHURCH

WINDOW & DOOR SCHEDULES

Drawn by	Author	Scale	As indicated
Checked by	Checker	Date	Issue Date

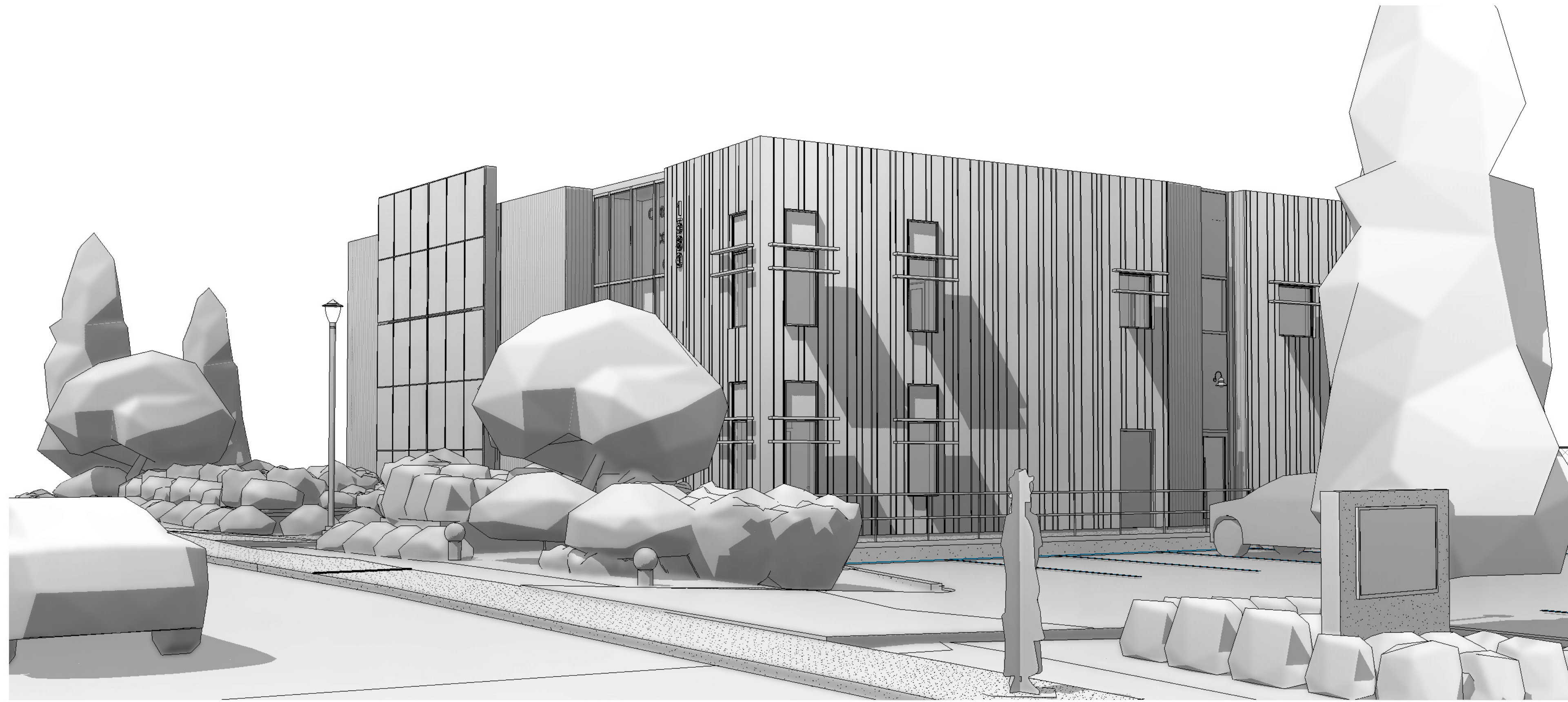
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Plot date:
2024-02-22
10:43:18 AM

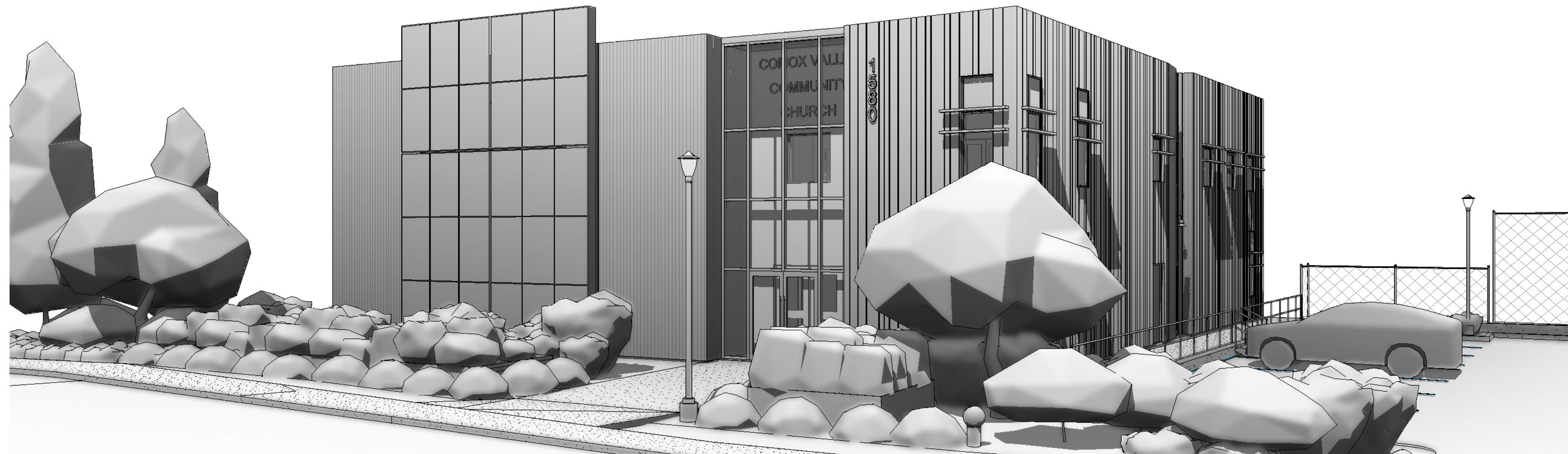
Project number	Sheet
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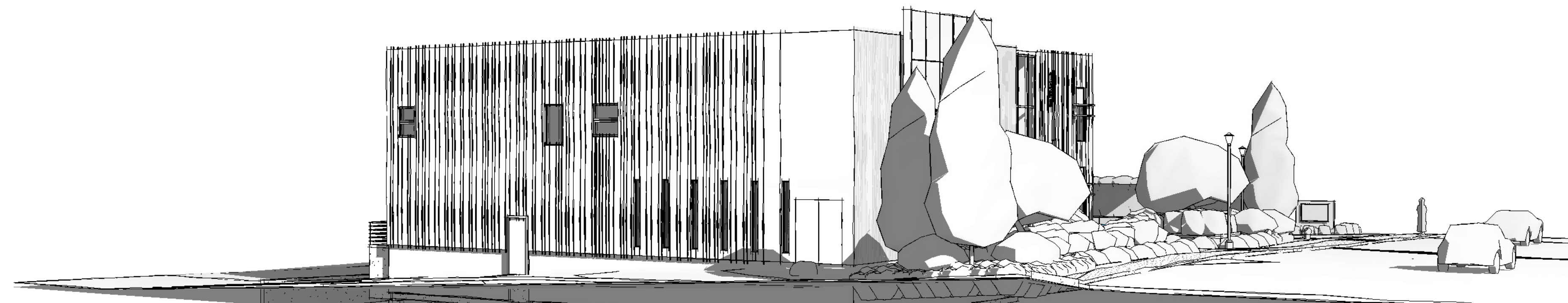
1 EXT EAST ROAD



2 EXT SOUTH



3 EXT WEST ROAD



PRELIMINARY
 NOT FOR
 CONSTRUCTION

No.	Description	Date
REVISIONS:		
ISSUED:		
For	Date	

**SALVATION ARMY
 - CV COMMUNITY
 CHURCH**

**PERSPECTIVES -
 EXTERIOR**

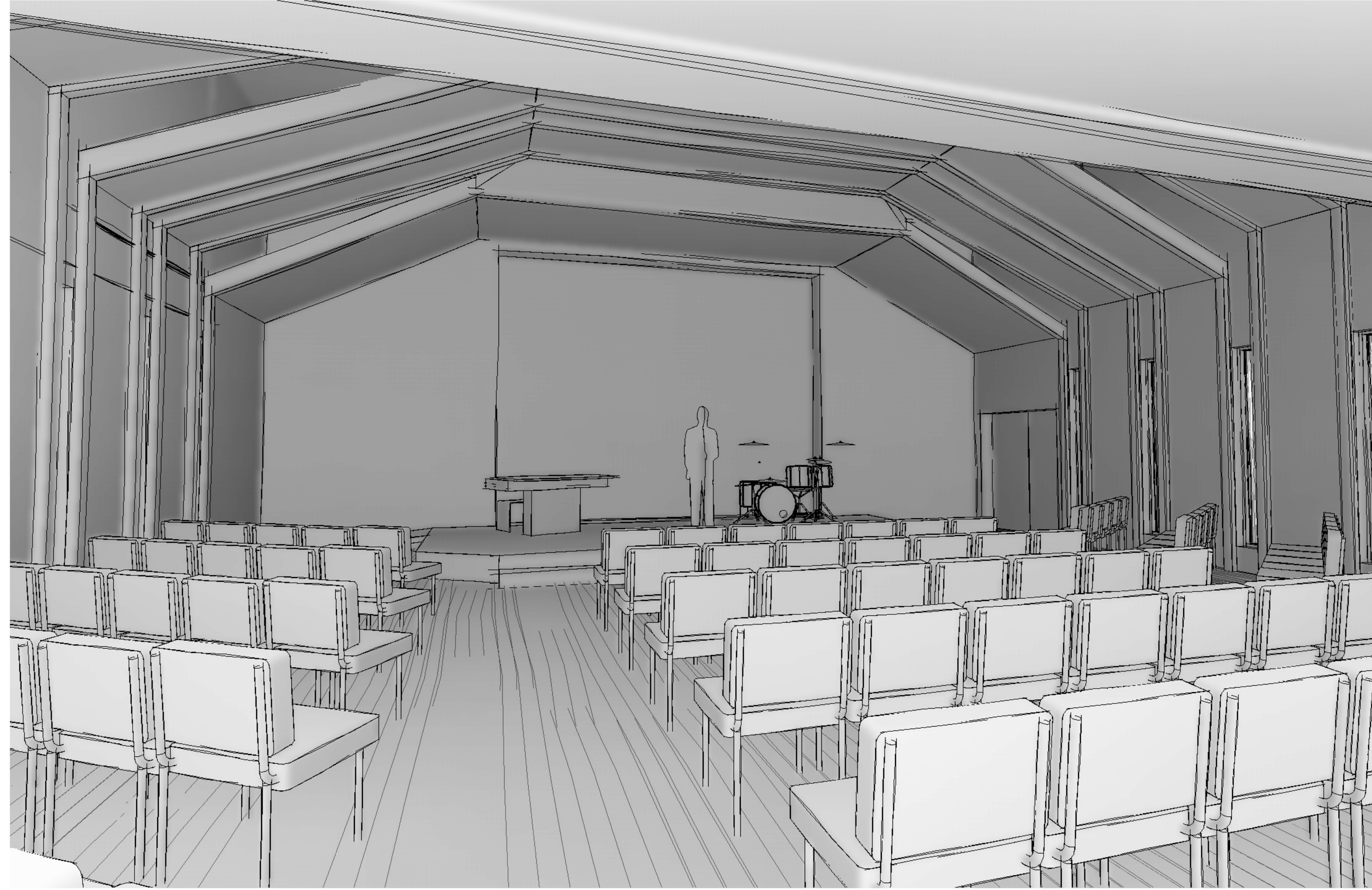
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Checked by	Checker	Date
		Issue Date

A900

Project number	Sheet
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Plot date:
 2024-02-22
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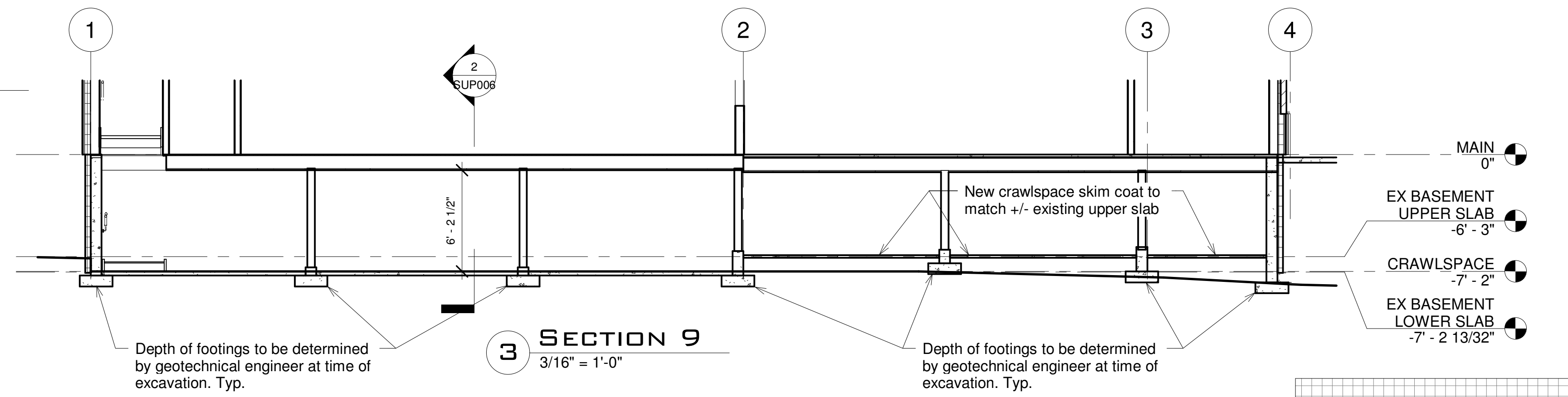
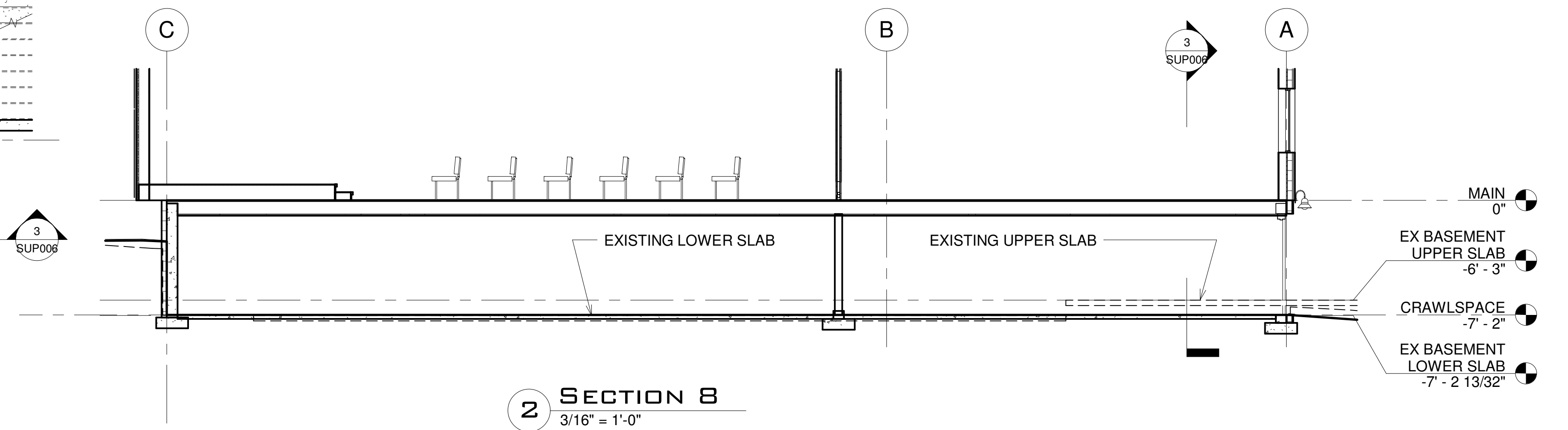
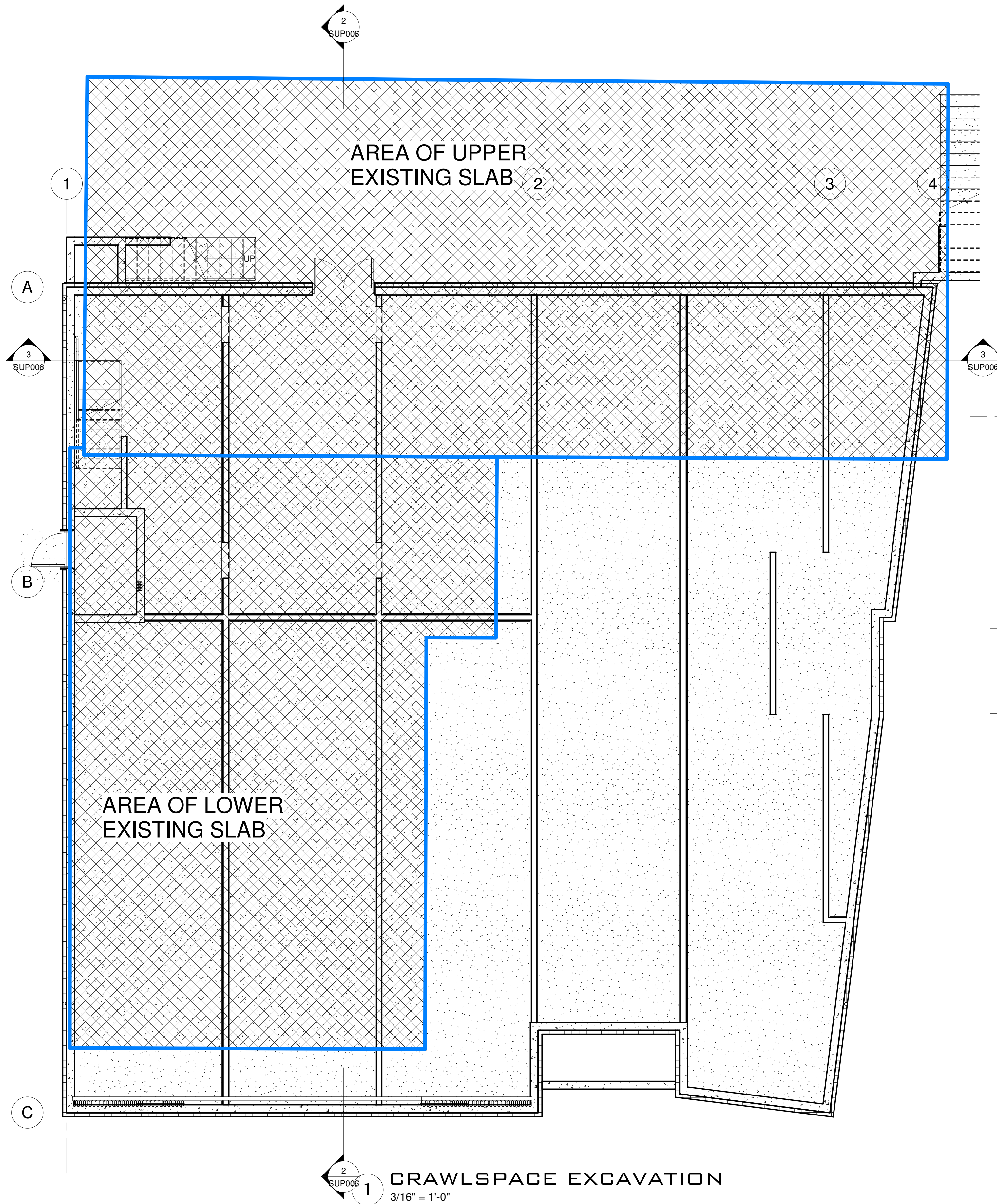
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No.	Description	Date	
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ISSUED:			
For		Date	
SALVATION ARMY - CV COMMUNITY CHURCH			
PERSPECTIVES - INTERIOR			
Drawn by	Author	Scale	
Checked by	Checker	Date	Issue Date
A901			
Project number	Sheet		

Plot date:
 2024-02-22
 10:43:24 AM

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No.	Description	Date
REVISIONS:		
ISSUED:		
For	Date	

**SALVATION ARMY
 - CV COMMUNITY
 CHURCH
 NORTHWEST
 CORNER
 EXCAVATION**

Drawn by	Author	Scale	3/16" = 1'-0"
Checked by	Checker	Date	Issue Date

SUP006

Project number Sheet

Plot date:
 2024-04-10
 4:54:13 PM

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Attachment 2

Certificate of Compliance (Site ID: 12695)



REGISTERED MAIL

Victoria File: 26250-20/12695
Site ID: 12695

December 16, 2016

Andrzej Wodkiewicz
The Governing Council of the Salvation Army, Canada West
2 Overlea Blvd.,
Toronto, ON M4H 1P4

Dear Andrzej Wodkiewicz:

Re: Certificate of Compliance - 1580-1590 Fitzgerald Avenue, Courtney, British Columbia

Please find enclosed a Certificate of Compliance respecting the site referenced above.

In addition to the conditions set out in Schedule B of the Certificate of Compliance, please be advised of the following:

1. Information about the site will be included in the Site Registry established under the *Environmental Management Act*.
2. The provisions of this Certificate of Compliance are without prejudice to the right of the Director to make orders or impose requirements as the Director may deem necessary in accordance with applicable laws. Nothing in this Certificate of Compliance will in any way restrict or impair the Director's power in this regard.
3. A qualified environmental consultant should be available to identify, characterize and appropriately manage:
 - (a) any environmental media that may be contaminated, or
 - (b) soil which may exceed the standards triggering a Contaminated Soil Relocation Agreement set out in section 40 of the Contaminated Sites Regulation and may be encountered during any future subsurface work at the site.
4. Groundwater wells that are no longer required must be properly decommissioned in accordance with the *Water Act's* Groundwater Protection Regulation.

5. Please note that future site development may create preferential pathways for vapour. In this event, further assessment and remediation of vapour may be warranted.
6. Please note that the attached Certificate of Compliance does not address obligations of employers regarding worker health and safety under the Workers Compensation Act and Occupational Health and Safety Regulation. Development of site-specific work procedures in accordance with Workers' Compensation Board regulations may be warranted. Please direct related questions to Worksafe BC.

Issuance of this Certificate of Compliance is a decision that may be appealed under Part 8 of the *Environmental Management Act*.

If you require clarification of any aspect of this Certificate of Compliance, please contact the undersigned at 604-582-5348 (toll free via Enquiry BC at 1-800-663-7867).

Yours truly,



Lavinia Zanini, P.Geol.
Senior Contaminated Sites Officer

Enclosure

cc: Courtenay City Hall,
830 Cliffe Avenue, Courtenay, B.C., V9N 2J7

Richard Wells, P.Eng., Approved Professional - Keystone Environmental Ltd.
Suite 320 - 4400 Dominion Street, BC, V5G 4G3

Marc Cameron, Approved Professional, Core6 Environmental
777 Hornby Street, Suite 1410, Vancouver, BC V6C 1S4

CSAP Society, 613 - 744 West Hastings Street, Vancouver, BC, V6C 1A5



Ministry of
Environment

CERTIFICATE OF COMPLIANCE
(Pursuant to Section 53 of the *Environmental Management Act*)

THIS IS TO CERTIFY that as of the date indicated below, the site identified in Schedule A of this Certificate of Compliance has been satisfactorily remediated to meet the applicable Contaminated Sites Regulation remediation standards and criteria.

This Certificate of Compliance is qualified by the requirements and conditions specified in Schedule B.

The substances for which remediation has been satisfactorily completed and for which this Certificate of Compliance is valid are listed in Schedule C.

I have issued this Certificate of Compliance based on a review of relevant information including the documents listed in Schedule D. I, however, make no representation or warranty as to the accuracy or completeness of that information.

A Director may rescind this Certificate of Compliance if requirements and conditions imposed in the Certificate of Compliance are not complied with or any fees payable under Part 4 of the Act or regulations are outstanding.

This Certificate of Compliance should not be construed as an assurance that there are no hazards present at the site.

December 16, 2016
Date Issued


Lavinia Zanini, P.Geo.
For Director, *Environmental Management Act*

Schedule A

The site covered by this Certificate of Compliance is located at 1580-1590 Fitzgerald Avenue, Courtenay, British Columbia which is more particularly known and described as:

Lot A, (DD P54316) Section 41, Comox District, Plan 7449
PID: 001-830-392

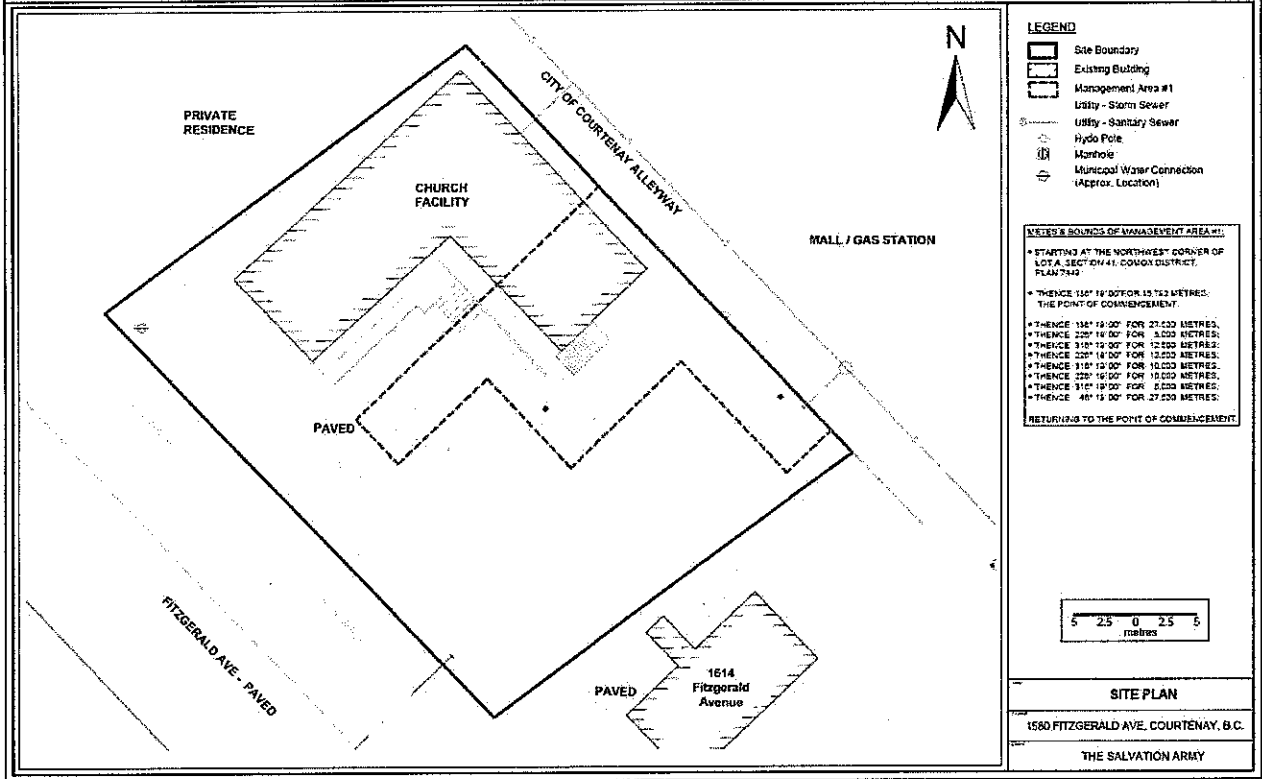
The approximate centre of the site using the NAD (North American Datum) 1983 convention is:

Latitude: 49° 41' 1.5"
Longitude: 124° 59' 36.8"

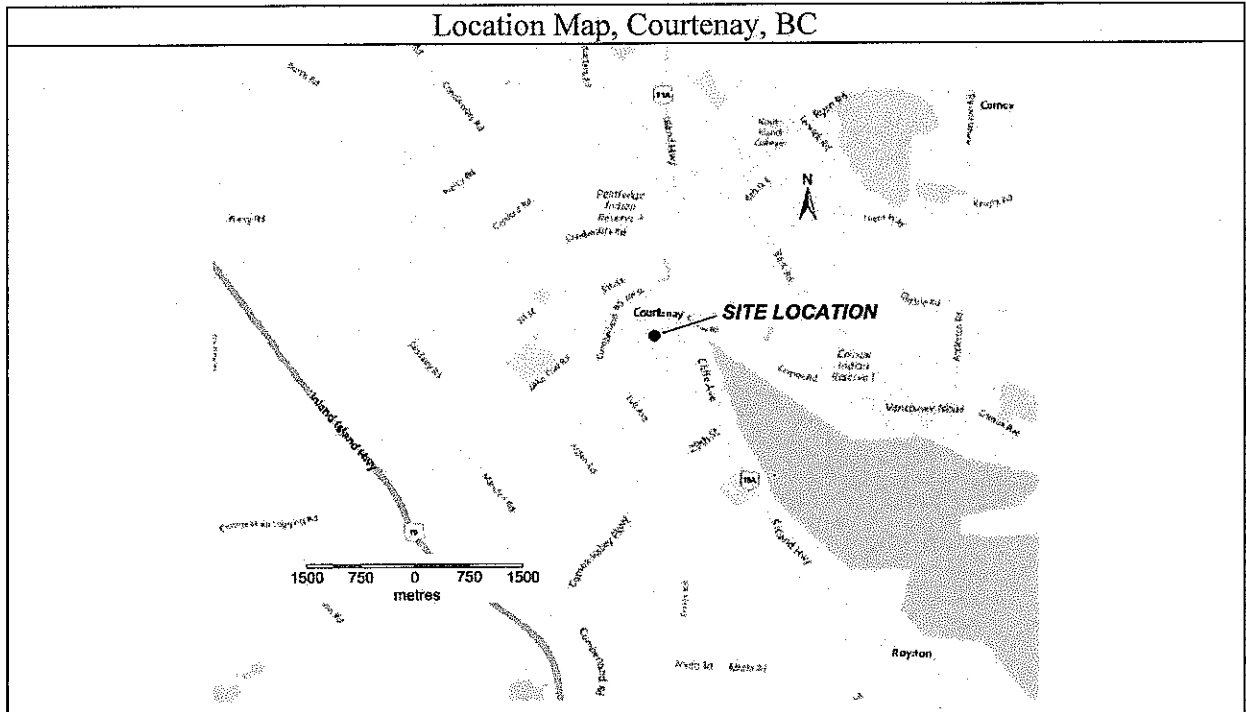
December 16, 2016
Date Issued


Lavinia Zanini, P.Geo.
For Director, *Environmental Management Act*

Site Plan, Courtenay, BC



Location Map, Courtenay, BC



December 16, 2016
Date Issued

L. Zanini
Lavinia Zanini, P.Geo.
For Director, Environmental Management Act

Schedule B

Requirements and Conditions

1. Any changes in land, vapour, or water uses must be promptly identified by the responsible person in a written submission to the Director. An application for an amendment or new Certificate of Compliance may be necessary. The uses to which this condition applies are described in Schedule C and in the site investigation documents listed in Schedule D.

The documents listed in Schedule D indicate that vapour attenuation factors were applied to meet a Contaminated Sites Regulation numerical and risk-based standards at the site. These vapour attenuation factors were selected based on assumptions about the structures, locations and depths of buildings and trenches existing or expected at the site. These assumptions include the following:

- a) Basements in any new buildings onsite will not extend beyond the depth of the existing basement (0.6 m depth) evaluated in the risk assessment.

Any inconsistencies that arise between the structures, locations and depths of proposed or constructed buildings at the site and the range of structures, locations and depths assumed in the selection of vapour attenuation factors in the documents listed in Schedule D must be promptly identified by the responsible person or persons in a written submission to the Director. An application for an amendment or new Certificate of Compliance may be necessary.

2. The principal risk controls which must be present or implemented and must be maintained at the site include the following:
 - a) Current intrinsic controls consisting of compliant surface soil (current natural barriers, >1 m thickness) will remain in place over the identified management area (Management Area 1) as follows:

Starting at the Northwest corner of Lot A, Section 41, Comox District, Plan 7449:

Thence 136°19'00" for 15.792 Metres; the point of commencement.

Thence 136°19'00" for 27.500 Metres;

Thence 226°19'00" for 5.000 Metres;

Thence 316°19'00" for 12.500 Metres;

Thence 226°19'00" for 12.500 Metres;

Thence 316°19'00" for 10.000 Metres;

Thence 226°19'00" for 10.000 Metres;

Thence 316°19'00" for 5.000 Metres;

December 16, 2016

Date Issued


Lavinia Zanini, P. Geo.
For Director, *Environmental Management Act*

Thence 46°19'00" for 27.500 Metres;
Returning to the point of commencement.

- b) Site groundwater must not be used as a potable water resource.
 - c) Deep rooting vegetation will not be established over the identified Management Area 1
3. If requested by the Director, the responsible person<s> must provide a signed statement indicating whether the principal risk controls listed in clause 2 of this Schedule have been and continue to be met. This may include providing a signed statement by an Approved Professional.
 4. Performance verification must be undertaken as specified in the Performance Verification Plan listed in Schedule D or as specified in a modification of the plan approved by the Director.
 5. Where required under a Performance Verification Plan for the site, records of performance verification actions and results must be maintained by the responsible person<s> or their agent. The records must be available for inspection by the Director.
 6. The Director must be notified promptly by the persons responsible for the site if performance verification actions indicate that any institutional and engineering controls required in clause 2 of this Schedule are not being met. The following information must be submitted to the Director with the notification, or as soon as practicable thereafter:
 - (a) The time period over which institutional and engineering controls did not meet the requirements of Schedule B;
 - (b) The nature of the excursions;
 - (c) The temporary or permanent corrective measures implemented or to be implemented;
 - (d) An implementation schedule; and
 - (e) Supporting documentation.
 7. If requested by the Director, a report signed by an Approved Professional must be submitted for review to the Director and must include the following:
 - (a) An evaluation of the performance of the institutional and engineering controls;
 - (b) Recommendations for modification of any plans referenced above, along with supporting rationale;
 - (c) Interpretation of current and cumulative results of the performance verification actions undertaken according to the plans described in clause 4 above; and
 - (d) Supporting documentation.

December 16, 2016
Date Issued


Lavinia Zanini, P.Geo.
For Director, *Environmental Management Act*

Schedule C

Substances and Uses

Substances remediated in soil for residential land soil use:

To meet numerical remediation standards:

- Naphthalene and phenanthrene; and,
- Benzene and xylene.

To meet risk-based remediation standards:

- VPHs, LEPHs and HEPHs; and,
- Ethylbenzene.

Substances remediated in water for freshwater aquatic life water use:

To meet numerical remediation standards:

- EPHw10-19; and,
- Naphthalene.

To meet risk-based remediation standards:

- LEPHw; and,
- Pyrene.

Substances remediated in water for protection of drinking water use:

To meet numerical remediation standards:

- EPHw₁₀₋₁₉

To meet risk-based remediation standards:

- EPHw₁₀₋₁₉; and
- Ethylbenzene.

December 16, 2016

Date Issued


Lavinia Zanini, P.Geo.
For Director, *Environmental Management Act*

Schedule D

Documents

- *Summary of Site Condition*, prepared by Richard Wells, Marc Cameron, dated 11 September 2016;
- *1580 & 1590 Fitzgerald Avenue, Courtenay, BC, Addendum Report – Supplemental Site Investigation*, prepared by ARCADIS Canada Inc., dated 24 May 2016;
- *Performance Verification Plan (PVP) for: 1580 and 1590 Fitzgerald Avenue, Courtenay, B.C., Human Health and Ecological Risk Assessment*, prepared by ARCADIS Canada Inc., dated 20 February 2016;
- *The Salvation Army, Human Health and Ecological Risk Assessment, 1580 & 1590 Fitzgerald Avenue, Courtenay, BC*, prepared by ARCADIS Canada Inc., dated 20 February 2016;
- *Addendum Report - Stage 1 PSI 1580 Fitzgerald Avenue, Courtenay, BC*, prepared by ARCADIS Canada Inc., dated 31 July 2015;
- *1580 Fitzgerald Avenue Post Remediation Monitoring*, prepared by Franz Environmental Inc., dated 29 January 2015;
- *Update on Remedial Methods and Progress, 1580 Fitzgerald Avenue and Adjacent City of Courtenay Alleyway, Courtenay, British Columbia*, prepared by Franz Environmental Inc., dated 19 December 2013;
- *Update on Remedial Methods and Progress, 1580 Fitzgerald Avenue and Adjacent City of Courtenay Alleyway, Courtenay, British Columbia*, prepared by Franz Environmental Inc., dated 30 September, 2013;
- *Update on Remedial Methods and Progress, 1580 Fitzgerald Avenue and Adjacent City of Courtenay Alleyway, Courtenay, British Columbia*, prepared by Franz Environmental Inc., dated 31 January 2013;
- *Update on Remedial Methods and Progress, 1580 Fitzgerald Avenue and Adjacent City of Courtenay Alleyway, Courtenay, British Columbia*, prepared by Franz Environmental Inc., dated 24 August 2012;
- *Detailed Site Investigation and Site Remediation, 1580 & 1590 Fitzgerald Avenue, Courtenay, BC*, prepared by Franz Environmental Inc. dated December 2013;
- *Stage 2 PSI 1580 & 1590 Fitzgerald Avenue, Courtenay, BC*, prepared by Franz Environmental Inc., dated September 2011; and
- *Stage 1 PSI, 1580 Fitzgerald Avenue, Courtenay, BC*, prepared by Franz Environmental Inc., dated January 2011.

December 16, 2016
Date Issued


Lavinia Zanini, P.Geo.
For Director, *Environmental Management Act*



The Corporation of the City of Courtenay

Staff Report

To: Council

File No.: 1960-20

From: Director of Financial Services

Date: June 12, 2024

Subject: Permissive Tax Exemption Policy Options

PURPOSE:

To seek Council guidance on changes to the City's Permissive Tax Exemption (PTE) use and policy.

BACKGROUND:

At the October 25th 2023 Council meeting Council gave the following direction

THAT Council direct staff to update the Permissive Tax Exemption Policy by June 2024;

AND

THAT once the Permissive Tax Exemption Policy has been updated, Council consider a grant in the amount of the proposed permissive tax exemption for Aaron House Ministries, Folio 1960.006.

and

WHEREAS quality childcare is a vital part of any community, supporting families, underpinning economic development and providing a safe and nurturing environment for children; and

WHEREAS the 2019 Comox Valley Child Care Action Plan, developed by the Comox Valley Regional District in partnership with the City of Courtenay recommended that "all communities consider permissive exemptions for childcare services", something the Town of Comox has been doing for decades;

THEREFORE BE IT RESOLVED THAT staff prepare a report for Council outlining the options for, and impact of, providing permissive tax exemptions for community owned non-profit childcare organizations.

DISCUSSION:

In 2023 the City granted 97 unique permissive tax exemptions, 73 of these were granted on a 1-year basis (normal process). Per Part 7, Division 7 of the Community Charter, a municipality may exempt a property from taxation. The exemption guidelines are broad, but they can be broken down into some main components. Some properties will be exempt under Part 7 Division 6 of the Community Charter which covers statutory exemptions. These properties are mostly Schools, Hospitals and Church buildings (Church land is a permissive exemption).

Examples

- Property owned or held by a charitable, philanthropic, and non-profit corporation and used for the purposes of the corporation
- Property owned by a local authority and used for the purposes of that local authority (e.g. property owned by Municipality X but located within the boundaries of Municipality Y)

- Properties owned or held by a public authority that is not statutorily exempt from taxes
- Properties occupied by a public authority or non-profit organization but owned by a different public authority
- Properties owned by a person (including a business, society or corporation) providing a partnering agreement, but only in relation to the provision of the agreement.
- Public parks owned and held by an athletic or service club
- Art galleries or museums owned by a charitable or philanthropic organization
- Not-for-profit seniors and community housing
- Property located around a place of public worship but not eligible for a statutory exemption (church land or parking lot)

The City's current exemption methodology exempts properties from all taxes charged by various taxing authorities (City, Regional District, Regional Hospital District, School District, library, BC Assessment Authority, Municipal Finance Authority and the Downtown Courtenay Business Improvement Area).

Permissive Tax Exemptions are a long-standing authority granted to Councils to aid organizations within their respected communities. Although a well-established method of aiding organizations, the PTE process is not without its own challenges.

Pros

- Long established method of providing benefit
- Public notification required

Cons

- Limited benefit as only property owners/lease holders qualify
- Challenging to flow benefits to lease holders (if landlord is uncooperative)
- Limited ability to change level of benefit
- Inflexible due to timing of required bylaw consideration
- Provides exemption for other taxing authorities
- Limited transparency as exemptions based on future estimations
- Administratively cumbersome
- If an organization vacates or sells their building, the tax exemption cannot be changed if already authorized by bylaw until the next year.

Community Benefit Direction Challenge

A PTE is a tool for Council to direct financial support and in turn provide community benefit. A PTE is based on property value and the associated taxes charged. Typically, an organization that provides benefit to the Comox Valley will receive a maximum of 40% exemption while an organization that provides benefit strictly to Courtenay residents will receive a 100% exemption. The 40% exemption does not change if the service provided is of extraordinary value or not, meaning that PTE's have challenges focusing quantity of benefit based on type of service provided.

Example – An organization like the Comox Valley Food Bank has a small space that is used to organize and provide community services and receives a 40% exemption. This organization provides a service in the Comox Valley that recipients do not pay for, but is limited to a small PTE value as they only own/occupy a small footprint. PTE 2024 - \$2,328

Example – An organization like the Courtenay Marina Society that has a limited number of members and limited number of moorage slips that are available to those who have a boat. This organization receives a 100% exemption with a value of \$13,624 in 2024.

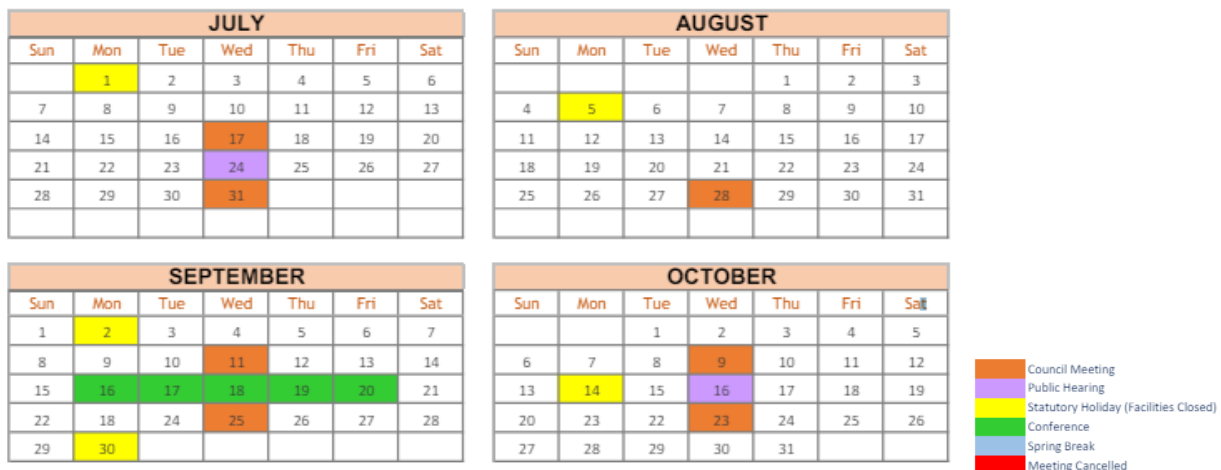
The City has limited tools to address the varying levels of support through the PTE process as the policy must be broad enough to capture an adequate number of eligible properties while still somewhat restrictive to not provide too much assistance to organizations that might not require it.

The current PTE system does not require a link between the exemption granted by the City and an outcome of the organization, the assumption is that relief of property taxation simply allows resources to flow to programs and services that the organization provides. No consideration is given if an organization has access to alternative funding for property taxation, many Provincial contracts provide funding for operating expense such as property tax. By providing tax relief that could be eligible for other funding the City is inadvertently providing tax relief when it is not required.

Timing Challenge

A PTE must be established by bylaw prior to October 31st to be effective for the following year. The PTE bylaws require a 2-week notification period between first reading and adoption per Div 7, Sec 227 of the *Community Charter* in accordance with section 94 (public notice). This 2-week notification period typically does not align with Council meeting deadlines resulting in the adoption of the bylaw taking an extra Council meeting cycle.

For 2024 the last scheduled Council meeting in October is the 23rd, to meet the notification period the latest Council could give first reading to the bylaw is the September 25th Council meeting. This would not permit time to make additions to the PTE bylaw as additional properties require notification while detractors from the bylaw do not require renotification. Preferable Council could be presented with a PTE bylaw at the September 11th Council meeting, the deadline to have PTE bylaws submitted to this meeting for consideration falls on Friday August 30th. This would provide the period September 12-20th to gather feedback and make recommendations to Council for adjustment. Council would have to provide an amendment to the PTE bylaw at the Sept 25th meeting to facilitate the appropriate amount of time prior to adoption, still the timeline is tight.



Creation of the annual PTE bylaws is not a simple process as staff are collecting and analysing information for 97 properties and organizations, granted 24 of these properties are captured within a 5 or 10 year bylaw, leaving 73 to be address annually (2024). Staff have been noticing small but increasing challenges with organizations getting information on time, this has been attributed to staff turnover in some organizations along with simply capacity issues in others. Other challenges include the receipt of information to organizations that then needs to be passed onto the City such as budgets and financial statements. Further information collection occurs typically between February and July, with staff following up many times with various organizations. Staff are not in a position to simply let organizations fail to be included in a PTE bylaw if they have been receiving the benefit for many years, as it will result in a “rush / push” to get these organizations included after the fact. This puts staff in a challenging position of hounding organizations for information to ensure they meet the requirements of the PTE program.

Once Council adopts a PTE bylaw, it must be forwarded to BC Assessment Authority (BCA) prior to the annual October 31, deadline. BCA enters the properties approved in the bylaw into their system for exemption in the following year. The City does not receive confirmation of this information until BCA releases the Completed roll early in the following year. City staff review each of the 97 exemptions submitted to ensure they have been property represented in the completed roll and it is not uncommon to find errors. Each error must be identified and fixed through communication with BCA, at times it can come down to a differing of interpretation, the most challenging properties are often those that are within a larger property, such as leased office.

Other Taxing Authorities

Currently a PTE provides exemption to all taxing authorities, this works well for the recipient of the PTE however other taxing authorities do not reduce their requisition. Other taxing authorities' requisition amounts simply gets redistributed to the remaining tax paying properties as city funds are used to pay.

As an illustration, if a Regional District's requisition was \$100 and we have five properties in the City, with 1 receiving a PTE. Normally each property would pay \$20 towards the \$100 RD requisition however, a property has a PTE and the \$100 requisition is now shared between the remaining four properties. Their subsequent share of the requisition now increased by \$5 to \$25 each towards the RD requisition of \$100.

For 2024 the total City property tax forgone through PTE is \$427,120 while all other taxing authorities' forgone collections equal \$316,738, however these taxing authorities still require full payment of their respective levy. The foregone collections of \$316,738 is redistributed to the remaining non-exempted properties.

A grant program would better allow the City to provide tax relief for specific portions of property taxes, this could be achieved by matching the grant funds awarded to be equal to the City's property tax amount.

Discussion Points/Council Direction

- Should the City continue with PTE program or explore alternative options such as a grant program.
- Should exemptions continue to be granted to the following:
 - Courtenay Marina
 - Courtenay Airpark
 - Lands surrounding places of worship
 - Affordable housing owned by a not-for-profit
 - Organizations with access to Federal/Provincial funding for property taxation
 - Grandfathered properties at 100% exemption rates

- How will the Regional Social Development Grant Service impact the future of PTE
- Should financial needs tests be considered
- Should an upper and/or lower limit be placed on funding

POLICY ANALYSIS:

Currently policy # 1960.00.01 Permissive Property Tax Exemptions is the policy that guides tax exemptions for the City. Policy 1960.00.01 was last updated August 2017. The August 2017 update solidified the 40% maximum exemption as some organizations received 60% exemption. Prior revision occurred in May 2013 which brought in the 40% maximum exemption for organizations providing services not confined to the City of Courtenay and the 2% limit of prior year's tax levy. Further the May 2013 update reaffirmed stipulations that exemptions shall not be granted to organizations that provide services that would compete with private sector provided services.

Exemption Limit

The annual Financial Plan bylaw is required to include information on how permissive tax exemptions are used. The City has identified that permissive tax exemptions shall not exceed 2% of the total tax levy of the previous year per financial plan bylaw and policy.

- The 2023 property tax levy was \$31,195,000 x 2% provides a PTE cap of \$623,900, for 2024 the expected PTE value is \$429,494 which consumes 69% of the PTE capacity.
- The 2024 property tax levy is \$34,007,000 x 2% provides a PTE cap of \$680,140 for 2025.

The City has never reached the 2% limit, and the policy does not provide direction on what should occur if the limit is reached. Options could include removing the newest applicants until the limit is reached or equally reducing total exemptions to all organizations slightly to reach the 2% limit.

100% PTE Exemption

The August 2017 update provided clarity that new PTE applications that provided services beyond the City borders would be limited to a 40% exemption. Council gave direction that the update to the policy would not be applied retroactively which provided relief to existing organizations that would have seen their exemptions be reduced from 100% down to 40%. Further clarity is given then if an already grandfathered exempt organization moves locations within the City they will continue to receive the 100% exemption. Over the years the number of 40% exempt properties has grown as more organization apply and qualify for exemption.

- 64 Properties receiving 100% exemption
- 4 Properties receiving 75% exemption
- 27 Properties receiving 40% exemption

Current Policy Deviations

Over the years staff have had to make judgment calls on some of the processes and requirements of the policy to meet the overall desire of the PTE program. The following are the common challenges that result in deviations from the policy.

- Audit – The policy requires applicants to submit audited financial statements, this is a challenge as many small organizations don't perform annual audits.
- Late applications – The policy does not have a specific application deadline however the PTE application states the deadline is May 15th. Often staff is finding they are spending time chasing down information from prior years applications who have yet to submit information for the current year

Local Municipalities

Cumberland provides exemptions to 3 properties with total municipal taxation forgone of \$2,840. Cumberland provides exemptions equivalent to 0.07% of 2023 municipal taxation levy. The table below provides details on Cumberland’s PTE for 2023

Property Tax Exemptions 2023

The following properties in the Village of Cumberland were provided permissive property tax exemptions by Council in 2023 as authorized by section 98 of the *Community Charter*.

Civic Address	Organization	Value of Exemption
2688 Penrith Avenue	Cumberland United Church Trustees	\$430
2722 Penrith Avenue	Cumberland Community Church	\$459
2680 Dunsmuir Avenue	Historical Society (museum)	\$1951

Comox provides exemptions to 25 properties with total municipal taxation forgone of \$243,656. Comox provides exemptions equivalent to 2.11% of 2023 municipal taxation levy. The Town of Comox does not publish an itemized summary of PTE granted but the City has been advised that they have exemptions approved for not for profit daycares.

Campbell River provides exemptions to 115 properties with total municipal taxation forgone of \$685,781. Campbell River provides exemptions equivalent to 1.7% of 2023 municipal taxation levy. See attachment 3 – City of Campbell River Permissive Tax Exemption Summary 2024 for detail. Note that the City has approved exemptions for not for profit daycares.

Not for Profit Day Care Resolution

The application of PTE for non-profit daycares has been considered on at least two occasions during the history of the City’s PTE policy. Most recently during the May 2013 update, Council direction was to continue the prohibition on PTE for not for profit day-care providers with respect to receiving exemptions.

The following is an excerpt from the current Permissive Property Tax Exemption Policy that has implications for not for profit daycare providers:

5. Extent, Conditions, and Penalties

- a) The following activities and circumstances will be not be considered as eligible for exemption by Council. Exemptions will exclude the portion of land/improvements where the following circumstances exist:
 - land/improvements used by the private sector and/or organizations not meeting Council’s exemption criteria
 - land/improvements used for commercial or for-profit activities by the not-for-profit organization

As outlined above, the PTE policy is directed by Council and based on motions, it is clear that there is an appetite to revisit this direction from 2013. Analysis and recommendations to Council from staff are typically based on policy direction and over the past five years significant work with respect to support for the provision of Day Care services in the valley has been done both at the regional and municipal level. Input to Council from not for profit service providers has indicated that a PTE will assist in the delivery of this community service and the recently adopted Official Community Plan identifies the need to support child care services. Given the existing policy support and requests in the community, staff are of the

opinion that there is sufficient background should Council wish to direct that Section 5. a) be amended to clarify that not for profit daycares are not subject to this prohibition for consideration during the 2025 PTE.

Deferred PTE Consideration Resolution

With respect to the resolution to consider a grant for the application that was not approved in 2024, staff have received notification from the property owner that this organization no longer occupies leased space and consideration of a PTE is no longer required for *Folio 1960.006*.

FINANCIAL IMPLICATIONS:

Until Council provides alternative direction to the current PTE policy there is limited financial impact as total PTE’s as it is estimated that approvals will follow prior years. Should Council give direction to consider not for profit daycares for 2025 PTE staff can provide an estimate of the financial implications during the deliberations for PTE approval which is consistent with the policy and bylaw consideration process.

ADMINISTRATIVE IMPLICATIONS:

Administrative implications will be dependent on Council direction. Any change to PTE’s should be phased to reduce the shock to various not for profit organizations.

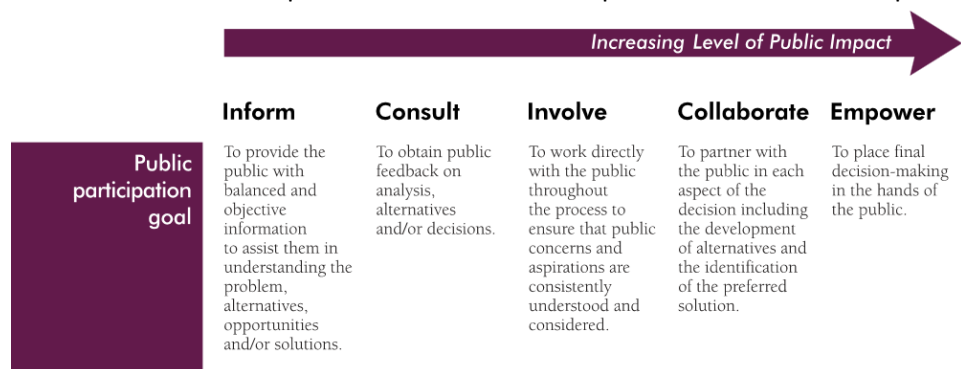
STRATEGIC PRIORITIES REFERENCE:

This initiative addresses the following strategic priorities:

- Financial Sustainability - Ensure capacity to accommodate big change

PUBLIC ENGAGEMENT:

Staff would inform the public based on the IAP2 Spectrum of Public Participation:



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OPTIONS:

1. THAT the permissive tax policy options, as directed by Council at the October 25, 2023 Council meeting, be considered for application in the 2026 tax year;
and,
THAT Council direct staff to expand the scope of the review and options analysis from permissive taxes to grant in aid, and report back on permissive tax and granting policy options;
and,
THAT permissive tax and grant in aid options be guided by the policy priorities and cardinal

directions of the Official Community Plan (OCP).

AND FURTHER THAT Council consider an amendment to Permissive Property Tax Exemption Policy to indicate that Section 5.a) does not apply to not for profit child care centres in accordance with the practice of other local municipalities effective for the 2025 PTE consideration.

2. THAT the permissive tax policy options, as directed by Council at the October 25, 2023 Council meeting, be considered for application in the 2026 tax year; and
THAT Council direct staff to expand the scope of the review and options analysis from permissive taxes to grant in aid, and report back on permissive tax and granting policy options; and
THAT permissive tax and grant in aid options be guided by the policy priorities and cardinal directions of the Official Community Plan (OCP).
3. THAT Council provide alternative direction to staff.

ATTACHMENTS:

- 1 – Permissive Property Tax Exemption Policy 1960.00.01
- 2 – City of Courtenay Estimated Permissive Tax Exemption Summary 2024
- 3 – City of Campbell River Permissive Tax Exemption Summary 2024

Prepared by: Adam Langenmaier BBA, CPA, CA Director of Finance
Concurrence: Geoff Garbutt, M.Pl., MCIP, RPP, City Manager (CAO)

Section 5 - Finance	Policy #	1960.00.01
Subject: Permissive Property Tax Exemption	Revision #	2

SCOPE:

A permissive tax exemption is a means for Council to support not-for-profit organizations within the community which further Council’s objectives of enhancing quality of life (economic, social, cultural) and delivering services economically. A permissive tax exemption is strictly at the discretion of the City of Courtenay Council. After careful consideration of all applications Council may approve a full, a partial, or no tax exemption. The tax exemption may vary for the different applicants. This policy guides identification of organizations meeting Council’s objectives.

POLICY

1. Overall Amount

A projected amount of revenue to be foregone by Permissive Tax Exemptions will be set by Council annually during the development of the Financial Plan. This amount will be used to calculate the following year’s tax exemption for approved organizations based on the current year’s assessment and tax rates. The actual amount of the exemption may vary according to the following year’s assessment and tax rates.

The cumulative estimated value of the exemptions may not exceed 2% of the total tax levy in the previous year. The bylaw for exemptions for any given year must be adopted and submitted before any assessment or tax rate information is available for that year. The 2% amount will therefore be calculated based on the previous year’s assessment and tax rate information.

2. Process

Council will consider applications for permissive tax exemptions annually. Reminder letters to re-apply will be mailed annually or as the term of the exemption expires to current tax exemption recipients. In addition, application packages will be available at any time from the Municipal Office or on our website at www.courtenay.ca.

Applications must be submitted to the Director of Financial Services, using the prescribed application form. The Director will review the applications for completeness, and arrange contact with applicants for additional information as necessary.

Application submissions must include:

- Copies of audited financial statements for last three (3) years for first time applicants, and for the last year for current tax exemption recipients.

AUTHORIZATION: Council R16.06/2017	DATE: August 21,2017
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Section 5 - Finance	Policy #	1960.00.01
Subject: Permissive Property Tax Exemption	Revision #	2

- Copy of state of title certificate or lease agreement, as applicable.
- In the case of a lease agreement for premises rather than ownership, documents are required which indicate that the applicant will benefit by the exemption. Documents should demonstrate that the lease is currently, or will, on approval of the exemption, be reduced by the amount of the exemption, or that other considerations will be provided by the landlord equivalent to the value of the exemption.
- Description of programs/services/benefits delivered from the subject lands/improvements including participant numbers, volunteer hours, benefiting groups/individuals/special needs populations, fees charged for participation
- Description of any 3rd party use of the subject land/improvements including user group names, fees charged conditions of use.
- Information as to the extent to which the activities of the applicant are regional or local (within the City of Courtenay) in nature.
- Financial information on how the tax exemption amount is put back into the community through charitable means or reduced fees paid by the general population of the City of Courtenay.
- Confirmation that the organization’s activities do not compete with any other duly licensed business in the Municipality.

The Director of Financial Services will present a summary report of the applications, relative to the eligibility criteria, to Council and arrange for delegations to Council by applicants as necessary.

3. Criteria

- a) Subject Property must be one of:
 - Land and/or improvements owned by the applicant
 - Land and/or improvements leased under an agreement
 - Land and/or improvements ancillary to a statutory exemption under section 220 of the *Community Charter (Statutory Exemptions)*

- b) Nature of Organization must meet the requirements of *Division 7* of the *Community Charter (Permissive Exemptions)* which includes:
 - Non-profit organization
 - Charitable/philanthropic organization
 - Athletic or Service Club/Association
 - Care facility/licensed private hospital

AUTHORIZATION: Council R16.06/2017	DATE: August 21,2017
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Section 5 - Finance	Policy #	1960.00.01
Subject: Permissive Property Tax Exemption	Revision #	2

- Partner of the municipality by agreement under s. 225 (Partnering, heritage, riparian and other special exemption authority) of the *Community Charter*
- Other local authority
- Organization eligible under *Section 220 of the Community Charter* statutory exemption (e.g. place of public worship, cemetery, library, Indian land, senior’s homes, hospital, etc.)

c) The applicant organization’s use of the land/improvements must benefit the community in one or more of the following ways:

- provides recreational facilities for public use
- provides recreation programs to the public
- provides programs to and/or facilities used by youth, seniors or other special needs groups
- preserves heritage important to the community character
- preserves an environmentally, ecologically significant area of the community
- offers cultural or educational programs to the public which promote community spirit, cohesiveness and/or tolerance
- offers services to the public in formal partnership with the municipality
- [other]

d) All accounts for fees and charges levied by the City of Courtenay to the applicant must be current.

4. Duration of Exemption

Eligible organizations may be considered for tax exemptions exceeding one year (to a maximum of 10 years) where it is demonstrated that the services/benefits they offer to the community are of a duration exceeding one year (i.e. for the period of the tax exemption).

5. Extent, Conditions, and Penalties

a) The following activities and circumstances will be not be considered as eligible for exemption by Council. Exemptions will exclude the portion of land/improvements where the following circumstances exist:

- land/improvements used by the private sector and/or organizations not meeting Council’s exemption criteria
- land/improvements used for commercial or for-profit activities by the not-for-profit organization

AUTHORIZATION: Council R16.06/2017	DATE: August 21,2017
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Section 5 - Finance	Policy #	1960.00.01
Subject: Permissive Property Tax Exemption	Revision #	2

- the activities of the organization are not confined to the City of Courtenay. Council has designated a maximum exemption of 40% for regional service organizations. This policy will not be applied retroactively, and regional service organizations that have previously been approved by bylaw will be grandfathered into the exemption bylaw at those prescribed percentages.
 - **prescribed grandfathered permissive exemption percentages will not change in the event an organization re-locates within the municipality. However, a reduction of the exemption shall apply if the nature of the services provided by the organization changes at its new location.**
 - The applicant already receives grant-in-aid from the municipality and/or other sources
- b) Council may impose conditions on the exempted land/improvements with the applicant organization, including but not limited to:
- registration of a covenant restricting use of the property
 - an agreement committing the organization to continue a specific service/program
 - an agreement committing the organization to have field/facilities open for public use for certain times or a total amount of time
 - an agreement committing the organization to offer use of the field/facility to certain groups free of charge or at reduced rates
 - an agreement committing the organization to immediately disclose any substantial increase in the organization’s revenue or anticipated revenue or any change in the status of the property
 - [other]
- c) Council may impose **penalties** on an exempted organization for knowing breach of conditions of exemption, including but not limited to:
- revoking exemption with notice
 - disqualifying any future application for exemption for specific time period
 - requiring repayment of monies equal to the foregone tax revenue
 - [other]

AUTHORIZATION: Council R16.06/2017	DATE: August 21,2017
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City of Courtenay Estimated Permissive Tax Exemption Summary 2024

2024 Tax Estimation

Registered Owner	Occupant / Lessee	Civic Address	Use of Property	% 2024 Tax Estimation			
				Exempt	City	Other	Total
1124430 BC LTD	Community Justice Centre of the Comox Valley	A & C 450 - 8th St	Resolution services & restorative justice. Referrals from RCMP, CFB Comox, SD 71, VIHA, MCFD, businesses, etc. Leasing 21.68% of the space	40%	\$ 1,410	\$ 887	\$ 2,298
670431 BC LTD	The Canadian Red Cross Society	2683 Moray Avenue, Units 10-12	Health Equipment Loan Program (HELP): short term loans of basic and advanced medical equipment. Office of the Red Cross Comox Valley Emergency Management Team. Leasing 25% of the space	40%	\$ 2,006	\$ 1,262	\$ 3,268
Agar, L. Gray, A.	Comox Valley Project Watershed Society	2356 A Rosewall Cres	Environmental Society focused on sensitive habitat stewardship.	40%	\$ 1,158	\$ 728	\$ 1,886
Agar, L. Gray, M. Gray, A.	Y.A.N.A Fund Society	102, 2456 Rosewall Cres	Support to families who need to travel outside of our community to access medical care for a child under 19 or for a pregnant mother.	40%	\$ 978	\$ 615	\$ 1,594
Alano Club of Courtenay		543 - 6th St	Community facility assisting those recovering from addictions	* 100%	\$ 4,096	\$ 2,576	\$ 6,671
Anglican Synod Diocese Of B.C.		579 - 5th Street	Church	100%	\$ 1,184	\$ 1,254	\$ 2,438
Anglican Synod Diocese Of B.C.		4634 Island Hwy	Church	100%	\$ 461	\$ 489	\$ 950
Anglican Synod Diocese Of B.C.		1514 Dingwall Road	Church cemetery	100%	\$ 676	\$ 716	\$ 1,392
Bishop Of Victoria - Catholic Church		1599 Tunner Drive	Church	100%	\$ 1,285	\$ 1,362	\$ 2,647
Central Evangelical Free Church		765 McPhee Ave	Church	100%	\$ 3,605	\$ 2,267	\$ 5,871
City of Courtenay	Comox Valley Art	580 Duncan Ave	Art Gallery	100%	\$ 27,761	\$ 17,458	\$ 45,219
City of Courtenay	Comox Valley Canoe Racing Club	2040 Cliffe Avenue	Club boat & equipment storage.	* 100%	\$ 586	\$ 369	\$ 955
City of Courtenay	Comox Valley Rowing Club	2040 Cliffe Avenue	Club boat & equipment storage	* 100%	\$ 597	\$ 375	\$ 972
City of Courtenay	Courtenay & District Museum	207 - 4th St	Museum	100%	\$ 13,645	\$ 8,581	\$ 22,226
City of Courtenay	Courtenay & District Museum	207 - 4th St	Museum	100%	\$ 1,578	\$ 1,673	\$ 3,251
City of Courtenay	Courtenay Airpark Society	100 - 20th St	Private and Commercial aircraft operations and storage	100%	\$ 44,101	\$ 27,735	\$ 71,836
City of Courtenay	Courtenay Marina Society	2040 Cliffe Ave	Operation of a public marina including moorage docks and boat ramp	* 100%	\$ 1,736	\$ 1,092	\$ 2,829
City of Courtenay	Courtenay Marina Society	2040 Cliffe Ave	Operation of a public marina including moorage docks and boat ramp	* 100%	\$ 493	\$ 523	\$ 1,016
City of Courtenay	Courtenay Marina Society	2040 Cliffe Ave	Operation of a public marina including moorage docks and boat ramp	* 100%	\$ 13,624	\$ 8,568	\$ 22,192
City of Courtenay	Courtenay Marina Society	2040 Cliffe Ave	Operation of a public marina including moorage docks and boat ramp	* 100%	\$ 80	\$ 85	\$ 166
City of Courtenay	Sid Williams Theatre	442 Cliffe Avenue	Community theatre	100%	\$ 25,128	\$ 15,802	\$ 40,930
City of Courtenay	Comox Valley Recovery Centre Society	641 Menzies Ave	Residential addiction recovery facility	* 100%	\$ 2,580	\$ 2,442	\$ 5,022
City of Courtenay and Nature Trust of BC	City Of Courtenay And Nature Trust Of Bc	559 3rd Street	McPhee Meadows	100%	\$ 2,172	\$ 2,055	\$ 4,227
Comox Valley Child Development Association		237 - 3rd St	Office to serve children with special needs	* 100%	\$ 16,268	\$ 10,230	\$ 26,498
Comox Valley Child Development Association		267 - 3rd Street	1/3 child play area, 2/3 accessible parking space for families	40%	\$ 334	\$ 316	\$ 649
Comox Valley Family Services Association		1415 Cliffe Ave	Child, youth & family community and victim services	* 100%	\$ 7,899	\$ 4,967	\$ 12,866
Comox Valley Food Bank Society		1255 McPhee Ave	Food bank	40%	\$ 2,328	\$ 1,464	\$ 3,792
Comox Valley Kiwanis Village Society		1061 8th Street	Housing for low-income seniors	* 75%	\$ 2,627	\$ 2,486	\$ 5,113
Comox Valley Kiwanis Village Society		1051 8th Street	Housing for low-income seniors	* 75%	\$ 7,178	\$ 6,793	\$ 13,971
Comox Valley Kiwanis Village Society		635 Pidcock Ave	Housing for low-income seniors	* 75%	\$ 4,801	\$ 4,543	\$ 9,344
Comox Valley Regional District	Comox Valley Curling Club	4835 Headquarters Rd	Curling Club Recreation facility	* 100%	\$ 10,500	\$ 6,603	\$ 17,104
Comox Valley Transition Society			Fourplex - rental housing at rental rates geared to income. Target client group is single women and women with children.	40%	\$ 1,279	\$ 1,210	\$ 2,489
Comox Valley Transition Society		625 England Ave	Community offices. Secret Venture Holdings Ltd owned by	40%	\$ 3,096	\$ 1,947	\$ 5,043
Comox Valley Transition Society			Amethyst House, Residential stabilization and supportive recovery program for women.	** 100%	\$ 1,307	\$ 1,237	\$ 2,544
Courtenay & District Historical Society In Trust		2564 Cumberland Rd	Heritage Property	* 100%	\$ 2,671	\$ 2,528	\$ 5,199

City of Courtenay Estimated Permissive Tax Exemption Summary 2024

Registered Owner	Occupant / Leasee	Civic Address	Use of Property	%			Total
				Exempt	City	Other	
Courtenay Elks' Lodge No. 60 of the Benevolent and Protective Order of Elks Canada Inc.		231 6th Street	Facility to promote and support community. Raises funds for several children and community charities	40%	\$ 2,485	\$ 1,562	\$ 4,047
Courtenay Elks' Lodge No. 60 of the Benevolent and Protective Order of Elks Canada Inc.		231 6th Street	Facility to promote and support community. Raises funds for several children and community charities	40%	\$ 307	\$ 325	\$ 632
Courtenay Fellowship Baptist Church		2963 Lake Trail Rd	Church	100%	\$ 1,390	\$ 1,473	\$ 2,862
Dawn to Dawn Action on Homelessness Society		13-1520 Piercy	Affordable/Supportive Housing	40%	\$ 309	\$ 292	\$ 601
Dawn to Dawn Action on Homelessness Society		#102 1015 Cumberland Road	Affordable/Supportive Housing	40%	\$ 190	\$ 179	\$ 369
Dawn to Dawn Action on Homelessness Society		#311 1015 Cumberland Road	Affordable/Supportive Housing	40%	\$ 223	\$ 211	\$ 435
Dawn to Dawn Action on Homelessness Society		520 5th Street	Gukwās sa Wagalus / Rainbow House	40%	\$ 857	\$ 811	\$ 1,667
Delia Von Schilling	AVI Health & Community Services	355 6th Street	AIDS Vancouver Island, a non-profit providing harm reduction services, services for people living with HIV and/or Hepatitis C	40%	\$ 1,137	\$ 715	\$ 1,852
Elim Gospel Hall		566 - 5th Street	Church	100%	\$ 728	\$ 772	\$ 1,500
Elim Gospel Hall		576 - 5th Street	Church parking	100%	\$ 943	\$ 999	\$ 1,942
Eureka Support Society		280-4th St	Community facility for adults with mental illness	100%	\$ 3,678	\$ 2,313	\$ 5,991
Foursquare Gospel Church Of Canada		1640 Burgess Road	Church	100%	\$ 3,684	\$ 3,905	\$ 7,589
Glacier View Lodge Society		2470 Back Road	Seniors long-term care	* 100%	\$ 4,491	\$ 4,250	\$ 8,740
Glacier View Lodge Society		2470 Back Road	Seniors long-term care	* 100%	\$ 4,482	\$ 4,241	\$ 8,722
Glacier View Lodge Society		2450 Back Road	Seniors long-term care	* 100%	\$ 22,296	\$ 21,098	\$ 43,394
Goldfinch Small Home Design Ltd.	Boys and Girls Club of Central Vancouver Island	877 5th Street	Services to youth, children, and families with diverse needs. Leasing 27% of property	* 100%	\$ 890	\$ 560	\$ 1,450
Grace Baptist Church		467 - 4th Street	Church	100%	\$ 258	\$ 273	\$ 531
Habitat for Humanity Vancouver Island North Society		1755 - 13th Street	Re-store (5,000 sf) and Administration (2,000 sf) for both Restore (60%) and H4H (40%). 29% of space for Admin office used at 40% for H4H = 12% net exemption	40%	\$ 442	\$ 278	\$ 720
Island Corridor Foundation			Railway corridor	100%	\$ 1,101	\$ 1,056	\$ 2,156
Island Corridor Foundation			Railway corridor	100%	\$ 235	\$ 226	\$ 461
Island Corridor Foundation			Railway corridor	100%	\$ 194	\$ 186	\$ 379
Island Corridor Foundation			Railway corridor	100%	\$ 7,660	\$ 7,345	\$ 15,005
Island Corridor Foundation		Cumberland Road	Railway corridor	100%	\$ 9,054	\$ 8,683	\$ 17,737
Island Corridor Foundation		Cumberland Road	Railway corridor	100%	\$ 143	\$ 90	\$ 233
Island Corridor Foundation		Menzies Avenue	Railway corridor	100%	\$ 4,472	\$ 2,812	\$ 7,284
Island Corridor Foundation		Cumberland Road	Railway corridor / Train Station	100%	\$ 91	\$ 87	\$ 178
Island Corridor Foundation		Cumberland Road	Railway corridor / Train Station	100%	\$ 6,217	\$ 3,910	\$ 10,126
John Howard Society of North Island		575 10th Street	Services to youth, children, and families with diverse needs	40%	\$ 4,064	\$ 2,556	\$ 6,620
John Howard Society of North Island		994 - 8th Street	Supportive transitional youth housing.	40%	\$ 1,281	\$ 1,212	\$ 2,493
Joshua Hope	Stepping Stones Recovery House for Women Society	1535 Burgess Rd	Women's residential recovery program	40%	\$ 581	\$ 550	\$ 1,131
Kingdom Hall Of Jehovah Witnesses		1581 Dingwall Road	Church	100%	\$ 717	\$ 759	\$ 1,476
L'Arche Comox Valley		534 - 19th Street	Supported group home for adults with developmental disabilities	* 75%	\$ 1,253	\$ 1,185	\$ 2,438
L'Arche Comox Valley		1465 Grieve Avenue	'I Belong Centre' holding L'Arche Office, the Outreach Centre (day programs for adults with disabilities) and 6 semi-independent community living residential suites. Used 100% by the L'Arche community, however day programs are offered to the public.	40%	\$ 901	\$ 852	\$ 1,753
L'Arche Comox Valley		1465 Grieve Avenue	'I Belong Centre' holding L'Arche Office, the Outreach Centre (day programs for adults with disabilities) and 6 semi-independent community living residential suites. Used 100% by the L'Arche community, however day programs are offered to the public.	40%	\$ 3,802	\$ 2,391	\$ 6,193
LDS Church		1901 - 20th Street	Church	100%	\$ 2,329	\$ 2,469	\$ 4,798

City of Courtenay Estimated Permissive Tax Exemption Summary 2024

Registered Owner	Occupant / Leasee	Civic Address	Use of Property	%			Total
				Exempt	City	Other	
Lenco/Norco and Fernco Development LTD	Salvation Army Cornerstone Community and Family Services	Unit 8, 468 29th Street	Emergency services to community members - about 9% of space (excludes thrift store operations)	40%	\$ 2,806	\$ 1,765	\$ 4,571
LUSH Valley Food Action Society		B 2342 Rosewall Cres	Provides food hampers & Hot Meal program to low income households, student meal programs, local fruit picking & distribution, urban agriculture programs, teaching food skills.	40%	\$ 2,015	\$ 1,267	\$ 3,283
Lutheran Church		771 - 17th Street	Church	100%	\$ 992	\$ 1,052	\$ 2,044
M'akola Housing Society		810 Braidwood Road	Affordable / Supportive Housing	100%	\$ 19,974	\$ 18,901	\$ 38,876
Nesting Place Society (formerly Comox Valley Pregnancy Care Centre)		#4 - 204 Island Hwy N	Community resource and peer support centre	* 100%	\$ 1,969	\$ 1,239	\$ 3,208
New Life Apostolic Church		1814 Fitzgerald Avenue	Church	100%	\$ 725	\$ 769	\$ 1,494
Old Church Theatre Society		755 Harmston Ave	Community theatre	* 100%	\$ 9,351	\$ 5,881	\$ 15,232
Project Watershed Society		1901 Comox Road	Kus-kus-sum Property	100%	\$ 12,736	\$ 8,010	\$ 20,746
Project Watershed Society		1901 Comox Road	Kus-kus-sum Property	100%	\$ 8,672	\$ 5,454	\$ 14,126
Project Watershed Society		1901 Comox Road	Kus-kus-sum Property	100%	\$ 11,315	\$ 7,116	\$ 18,431
Project Watershed Society		1901 Comox Road	Kus-kus-sum Property	100%	\$ 13,227	\$ 8,318	\$ 21,546
River Heights Church Society		2201 Robert Lang Drive	Church	100%	\$ 1,468	\$ 1,556	\$ 3,024
Royal Canadian Legion, Courtenay Branch (Pacific) No. 17		367 Cliffe Ave	Facility to support veterans, promote remembrance, act in service of the community	* 100%	\$ 6,875	\$ 4,323	\$ 11,198
Royal Canadian Legion, Courtenay Branch (Pacific) No. 17		367 Cliffe Ave	Facility to support veterans, promote remembrance, act in service of the community	* 100%	\$ 2,727	\$ 2,890	\$ 5,617
Royal Canadian Legion, Courtenay Branch (Pacific) No. 17		101 Island Highway	Cenotaph	* 100%	\$ 163	\$ 173	\$ 336
Salvation Army Canada West		1580,1590 Fitzgerald Ave	Church	100%	\$ 1,143	\$ 1,211	\$ 2,354
Seventh Day Adventist Church		4660 Headquarters	Church	100%	\$ 780	\$ 827	\$ 1,607
St. George's Church		505 - 6th Street	Church	100%	\$ 859	\$ 910	\$ 1,769
The Nature Trust of British Columbia		Sandpiper Drive	Park	100%	\$ 7,423	\$ 7,868	\$ 15,291
The Nature Trust of British Columbia		656 Arden Road	Morrison Nature Park	100%	\$ 3,797	\$ 4,025	\$ 7,822
Upper Island Women of Native Ancestry		956 Grieve Ave	Office; support worker; early childhood development and cultural awareness programs	40%	\$ 842	\$ 797	\$ 1,639
Wachiy Friendship Centre Society		1625 McPhee Avenue	Provides free social programs to urban Aboriginal individuals, Total space: 21,000 sf. Excluding Social enterprise: 2,000 sf. (9,5%) and Daycare: 3,450 sf. (16,4%). Total exempt 26%	40%	\$ 4,482	\$ 2,819	\$ 7,301
Wachiy Friendship Centre Society		1679 McPhee Avenue	Parking lot used by Wachiy Friendship Centre Society members and staff	40%	\$ 6,877	\$ 4,403	\$ 11,280
West Island Capital Corp	City of Courtenay	A1-310 8th Street	Leased to City of Courtenay for IT office space	100%	\$ 3,364	\$ 2,116	\$ 5,480
Youth for Christ Comox Valley		4729 Headquarters Rd	Youth recreation programs, summer camps, retreats, local ministerial meetings Occupy 97.5% of property	40%	\$ 800	\$ 757	\$ 1,557
Total					\$429,494	\$318,309	\$747,804

*Grandfathered at exemption rate
 **Per Council resolution June 26, 2017

2024 Permissive Tax Exemptions

Bylaw #	Name	Civic Address		Approved %	City of Campbell River		
		House	Civic Address Street		Taxes	Other Taxes	Total Taxes
2.01	British Columbia Society for the Prevention of Cruelty to Animals	891	13TH AVE	100%	2,659	1,801	4,459
2.02	Campbell River Alano Club	301	10TH AVE	100%	4,424	2,997	7,421
2.03	Campbell River and District Food Bank Society	1393	MARWALK CRES	100%	5,147	3,486	8,633
2.04	Campbell River and North Island Transition Society	608	ALDER ST	100%	2,525	2,055	4,580
2.05	Campbell River and North Island Transition Society	1116	DOGWOOD ST	100%	1,519	1,029	2,548
2.06	Campbell River Child Care Society	1048	HEMLOCK ST	100%	924	752	1,676
2.07	Campbell River Child Care Society	394	LEISHMAN RD	100%	3,500	2,371	5,871
2.08	Campbell River Dragon Boat Society	1468	IRONWOOD ST	100%	1,410	955	2,364
2.09	Campbell River Eagles Hall	1999	14TH AVE	85%	8,698	5,160	13,859
2.10	Campbell River Family Services Society	427	10TH AVE	100%	10,985	7,441	18,426
2.11	Campbell River Head Injury Support Society	591	9TH AVE	100%	4,785	3,443	8,229
2.12	Campbell River Head Injury Support Society	1710	16TH AVE	100%	5,980	4,867	10,847
2.13	Campbell River Head Injury Support Society	1720	16TH AVE	100%	5,980	4,867	10,847
2.14	Campbell River Hospice Society	440	EVERGREEN RD	100%	8,006	5,423	13,428
2.15	Canadian Red Cross Society	520	2ND AVE	100%	2,335	1,581	3,916
2.16	Communitas Supportive Care Society	1423	16TH AVE	100%	7,133	4,831	11,964
2.17	Communitas Supportive Care Society	59	COLORADO DR	100%	2,122	1,727	3,848
2.18	Communitas Supportive Care Society	150	COLORADO DR	100%	2,032	1,654	3,686
2.19	Communitas Supportive Care Society	2575	MILFORD RD	100%	1,881	1,531	3,413
2.20	Discovery Harbour Authority	2375	SPIT RD	70%	14,912	10,100	25,012
2.21	Discovery Masonic Lodge / Campbell River Shrine Club	2905	ISLAND HWY	70%	1,122	633	1,755
2.22	Governing Council of the Salvation Army	1381	CEDAR ST	100%	7,765	5,260	13,025
2.23	Habitat for Humanity Vancouver Island North Society	1725	WILLOW ST	100%	15,309	10,370	25,679
2.24	Habitat for Humanity Vancouver Island North Society	461	HILCHEY RD	100%	1,698	1,382	3,079
2.25	The John Howard Society of North Island	980	ALDER ST	100%	8,849	5,993	14,842
2.26	The John Howard Society of North Island	140	10TH AVE	100%	18,339	12,422	30,761
2.27	The John Howard Society of North Island	91	DOGWOOD ST	100%	2,534	2,063	4,597
2.28	John Perkins Memorial Housing Society (Royal Canadian Legion)	931	14TH AVE	100%	17,822	14,506	32,328
2.29	Navy League of Canada	931	13TH AVE	100%	5,223	3,217	8,440
2.30	North Island Employment Foundation Society	920	ALDER ST	100%	23,135	15,670	38,804
2.31	North Island Supportive Recovery Society	647	BIRCH ST	100%	1,755	1,428	3,184
2.32	Opportunities Career Services Society	300	ST. ANN'S RD	100%	3,561	2,412	5,974
2.33	Rivercity Inclusion	301	DOGWOOD ST	100%	12,179	8,249	20,429
2.34	Rivercity Inclusion	435	JESMAR PL	100%	2,328	1,895	4,222
2.35	Rivercity Inclusion	1065	GREENWOOD ST	100%	2,208	1,797	4,005
2.36	Rivercity Inclusion	1185	GREENWOOD ST	100%	2,655	1,798	4,453
2.37	Rivercity Inclusion	1153	GREENWOOD ST	100%	6,070	4,111	10,181
2.38	Rivercity Inclusion	1261	SHELLBOURNE BLVD	100%	1,623	1,321	2,943
2.39	Rivercity Inclusion	1841	ISLAND HWY	100%	3,466	2,348	5,814
2.40	Rivercity Inclusion	1851	ISLAND HWY	100%	3,135	2,124	5,259
2.41	Rivercity Inclusion	1180	IRONWOOD ST	100%	2,111	1,430	3,541
2.42	Rivercity Inclusion	1180	IRONWOOD ST	100%	3,416	2,314	5,730
2.43	Royal Canadian Legion	301	11TH AVE	85%	5,273	2,974	8,247
2.44	St. John Ambulance	1433	16TH AVE	100%	6,752	4,573	11,325
2.45	Total Change Ministries	1691	ISLAND HWY	100%	559	378	937
2.46	Upper Island Counselling Services Society	621	11TH AVE	85%	14,429	9,773	24,201
2.47	Vancouver Island Mental Health Society	1330	DOGWOOD ST	100%	10,203	6,911	17,114
2.48	Willow Point Lions Club Society	2165	ISLAND HWY S	85%	4,247	2,876	7,123
2.49	Willow Point Supportive Living Society	142	LARWOOD RD	100%	20,301	16,523	36,824
2.50	Willow Point Supportive Living Society	142	LARWOOD RD	100%	1,075	875	1,950
2.51	Willow Point Supportive Living Society	142	LARWOOD RD	100%	1,030	839	1,869
2.52	Willow Point Supportive Living Society	142	LARWOOD RD	100%	1,030	839	1,869
2.53	Willow Point Supportive Living Society	142	LARWOOD RD	100%	1,030	839	1,869
2.54	Willow Point Supportive Living Society	142	LARWOOD RD	100%	1,075	875	1,950
2.55	Willow Point Supportive Living Society	142	LARWOOD RD	100%	1,075	875	1,950
2.56	Willow Point Supportive Living Society	142	LARWOOD RD	100%	1,030	839	1,869
2.57	Willow Point Supportive Living Society	142	LARWOOD RD	100%	1,075	875	1,950
2.58	Willow Point Supportive Living Society	142	LARWOOD RD	100%	5,247	3,554	8,801
2.59	Willow Point Supportive Living Society	142	LARWOOD RD	100%	1,075	875	1,950
3.01	Association Francophone de Campbell River	891	13TH AVE	100%	2,659	1,801	4,459
3.02	Campbell River Curling Club	260	CEDAR ST	85%	17,652	11,956	29,608
3.03	Campbell River Eagles Water Ski Club		MCIVOR LAKE PARK	85%	579	346	924
3.04	Campbell River Fish and Wildlife Association	2641	CAMPBELL RIVER RD	85%	3,862	2,352	6,215
3.05	Campbell River Gun Club Society	2600	QUINSAM RD	100%	2,029	1,179	3,207
3.06	Campbell River Gymnastics Association	1394	MARWALK CRES	100%	5,909	4,002	9,911
3.07	Campbell River Tyee Judo Club	450	MERECROFT RD	100%	1,081	695	1,775
3.08	Campbell River Minor Hockey Association	225	DOGWOOD ST S	100%	217	147	363
3.09	Campbell River Motocross Association	7021	GOLD RIVER HWY	85%	5,816	3,939	9,755
3.10	Campbell River Skating Club	225	DOGWOOD ST S	100%	82	56	138
3.11	Campbell River Wado Karate Club	450	MERECROFT RD	85%	958	617	1,575
3.12	Campbell River Youth Soccer Association	450	MERECROFT RD	100%	770	484	1,254
3.13	Nature Trust of BC	3200	WILLOW CREEK RD	100%	7,511	4,237	11,748
3.14	RiverCity Players Society	1080	HEMLOCK ST	100%	5,177	3,506	8,683
3.15	Tyee Club of British Columbia	2951	SPIT RD	100%	551	449	1,000
3.16	Tyee Club of British Columbia	3000	SPIT RD	100%	1,099	744	1,843
3.16	Tyee Club of British Columbia (continued)	3000	SPIT RD	100%	2,042	1,383	3,424
4.01	Campbell River & District Museum and Archives Society (Museum)	470	ISLAND HWY	100%	70,718	47,900	118,617
4.02	Campbell River & District Museum and Archives Society (Haig-Brown)	2250	CAMPBELL RIVER RD	100%	10,177	6,155	16,332
4.03	Campbell River Arts Council	2131	ISLAND HWY S	100%	8,215	5,176	13,391
4.04	Campbellton Neighbourhood Association (Community Garden)	1721	15TH AVE	100%	2,557	1,732	4,289
4.05	Campbellton Neighbourhood Association (Community Garden)	1741	15TH AVE	100%	2,553	1,729	4,283
4.06	Campbell River Public Art Gallery	1235	SHOPPERS ROW	100%	5,849	3,962	9,810
4.07	Campbell River Search and Rescue Society	261	LARWOOD RD	100%	11,126	7,536	18,662
4.08	Coastal Animal Control Services of B.C. Ltd.	385	DOGWOOD ST S	100%	-	-	-
4.09	Comox Valley Regional District	6300	ARGONAUT RD	100%	10,892	-	10,892
4.09	Comox Valley Regional District (continued)	6700	ARGONAUT RD	100%	9,175	-	9,175
4.10	Discovery Coast Greenways Land Trust	2251	CAMPBELL RIVER RD	100%	16,611	9,398	26,009
4.11	Greenways Land Trust (Laughing Willow Community Garden)	1909	LAWSON GROVE	100%	2,450	1,384	3,834

2024 Permissive Tax Exemptions

4.12	Greenways Land Trust (Mountain View Community Garden)	600 CHARSTATE DR	100%	4,947	2,792	7,739
4.13	Eagles Found Bicycle Program	900 ALDER ST	100%	536	363	899
4.14	Campbell River Maritime Heritage Society	621 ISLAND HWY	100%	36,468	24,701	61,168
4.15	Nature Conservancy of Canada	2485 ISLAND HWY	100%	13,203	8,943	22,145
4.16	Nature Trust of BC	1201 HOMEWOOD RD	100%	8,651	4,880	13,531
4.17	Nature Trust of BC	HOMEWOOD RD	100%	6,743	3,804	10,547
4.18	North Island 911 Corporation	DOGWOOD ST S	100%	2,581	1,685	4,266
4.19	Tidemark Theatre Society	1220 SHOPPERS ROW	100%	16,343	11,069	27,412
4.20	Volunteer Campbell River	900 ALDER ST	100%	536	363	899
5.01	Anglican Synod Diocese of BC (St. Peter's Anglican Church)	228 DOGWOOD ST S	100%	9,070	5,116	14,185
5.02	Bethany Evangelical Lutheran Church	201 BIRCH ST	100%	2,798	1,578	4,376
5.03	Bishop of Victoria (St. Patrick's Parish)	34 ALDER ST S	100%	13,943	7,865	21,807
5.04	Campbell River Church of Christ	226 HILCHEY RD	100%	3,790	2,138	5,927
5.05	Campbell River Church of the Way	451 7TH AVE	100%	1,974	1,114	3,088
5.06	Campbell River Vineyard Christian Fellowship	2215 CAMPBELL RIVER RD	100%	2,496	1,408	3,904
5.07	Church of Jesus Christ of Latter-Day Saints in Canada	460 GOODWIN RD	70%	3,669	2,070	5,739
5.08	Foursquare Gospel Church of Canada	403 5TH AVE	100%	1,984	1,119	3,103
5.09	Foursquare Gospel Church of Canada	422 COLWYN ST	100%	3,003	1,694	4,697
5.10	Foursquare Gospel Church of Canada	415 5TH AVE	100%	801	652	1,452
5.11	Governing Council of Salvation Army in Canada	291 MCLEAN ST	90%	3,196	1,803	4,999
5.12	Guru Nanak Sikh Society	735 PINECREST RD	70%	3,095	1,746	4,840
5.13	Pentecostal Assemblies of Canada (Christian Life Fellowship)	445 MERECROFT RD	100%	4,710	2,657	7,367
5.14	Seventh Day Adventist Church	300 THULIN ST	100%	2,910	1,642	4,552
5.15	Trinity Presbyterian Church	145 SIMMS RD	100%	3,838	2,165	6,004
5.16	Trustees of Rockland Congregation of Jehovah's Witnesses	1935 EVERGREEN RD	70%	2,470	1,393	3,864
5.17	Campbell River United Church	415 PINECREST RD	100%	3,948	2,227	6,175
Grand Total				685,781	445,952	1,131,733



The Corporation of the City of Courtenay

Staff Report

To: Council

File No.: 5455-01

From: City Manager (CAO)

Date: 12 June 2024

Subject: Downtown Courtenay Parking Study

PURPOSE: To inform Council of the results of the 2023 Downtown Courtenay Parking Study by McElhanney, which includes an update to the 2017 Parking Study.

BACKGROUND:

McElhanney updated a parking review in April 2023 which compared results with a previous parking study conducted in 2017 and analysis of an expanded study area using new drone technology. This digital data gathering was instrumental in creating a web-based analysis tool that the City can use now and in the future at no additional charge, to answer a myriad of questions related to parking.

The purpose of the study was to collect accurate and reliable parking data for the downtown core area that would allow for future analysis and provide insight into parking behaviours and/or future needs. Results indicated times of peak parking and the occupancy of each area throughout the day for weekdays and weekends. The report also answered some key questions about on-street and off-street parking lots as well as the effectiveness of timed parking areas.

EXECUTIVE SUMMARY:

The Downtown Parking Study is a baseline or snapshot analysis relative to where parking stands today, both from a capacity and availability perspective but also from a user ship and an overall functionality standpoint. These works aim to serve as a guiding or baseline document for all future works and or land use planning and master planning functions relative to the future state of parking, including; Official Community Plan Implementation (Densification), Downtown Revitalisation, and the updating of the Downtown Courtenay Playbook.

Overall, parking is healthy and vibrant and the study are show significant spare capacity outside of the immediate core of the downtown. This capacity is well within walking distance, ensuring that people can safely and comfortably walk to the downtown from all sides of downtown.

The expanded survey area of the 2023 parking assessment allowed for not only the ability to compare the new data with the 2017 original study area but also to answer many other questions related to parking.

Comparison with 2017

Public Parking

- The peak parking area shifted from Section 1 (6th Street and 8th Street) of the downtown area to Section 3 (5th Street) in 2023.
- Section 2, comprising the eastern area of downtown north of the 5th Street bridge had modest occupancy during the week in 2017. This increased to de facto capacity in 2023.
- Not only is parking showing similar occupancy during the weekend, which was not the case in 2017 when weekend occupancy was low, but the occupancy during the weekend peaked an hour earlier than in 2017.

Private Parking

- Section 3 (5th Street) also had the highest private parking occupancy,
- The peak parking in private lots has increased to levels that are similar to public parking areas

Parking has become well used in the downtown core, no matter whether it's a weekday or weekend, and across both the public street parking and private parking lots. Outside of the 2017 downtown core area, there are still several streets and parking lots that have low to moderate occupancy, showing that there is capacity, albeit slightly farther than the immediate core area, but still within a 5-10 minute (400-800m) walk.

Site Specific Questions

There were several very specific questions asked by City staff that the report attempted to answer. For most cases, the review was for a frontage, like 550 5th Street, to determine whether parking restrictions should be implemented or more generally if current restrictions were meeting the emerging or trended needs of the community.

In each case, the parking was found to have a high turnover, with durations of 30 minutes or less being the norm. Although time restrictions are not required, given this amount of healthy activity, the signing of a restriction similar to other areas of the core would serve to formalise the parking expectations.

These site-specific questions often reviewed a small section of roadway, on one side, for the weekday, weekend, or both periods.

Of particular interest was that of 6th Street, as it will serve to connect to the 6th Street Bridge for active transportation modes. A question asking about the viability of modifying the angled parking to parallel parking on the street approaching Cliffe Avenue was considered. A holistic approach to that reduction of parking spaces would have to be balanced with the desire to provide greater flexibility for multiple modes of mobility as the function of the roadway shifts to active modes.

Overall Assessments

The overall and expanded survey area used for 2023 was reviewed to gain insight into the capacity of downtown parking as a whole.

Although the public parking occupancy for the area is 71%, which may involve increased circulation of vehicles looking for a parking space, there was considerable unmet capacity just beyond the 2017 core area. The occupancy for private parking lots was calculated at 76%, but similar to public areas, several private lots had significant spare capacity.

Occupancy beyond 80% can be considered near or at capacity since it is very hard to get above that number without significant circulation of vehicles.

This may be why 4th and 5th Street have heavy traffic volumes during their highest peak (the weekday lunch period) with occupancies over 90%.

Given the spare capacity outside the immediate core of downtown, a parking structure was not recommended at present, however, a trigger was used to start the review process. A trigger for this consideration could be 80% of all public and private parking spaces are occupied in the 2023 assessment area that are outside of the 2017 study area.

Private Parking Lot Assessments

Seven private parking lots that range from 19 spots to 107 spots were assessed for their characteristics and the report provided some insights regarding their use in the context of their location.

Related Context – SSMUH Zoning

A city review of the new zoning initiative by the province is being conducted on the impact of the Small-Scale Multi-Use Housing. This has direct implications for parking, especially surrounding the downtown area and commentary on how it could be managed as well as what tools the city staff could use for the development of the small lots with significant parking needs was discussed.

Residential Parking Permits are one way to manage the reduction of on-site parking requirements from two parking spots/dwelling units to one space/ dwelling unit. The highest range of parking would come from 4-plexes being built on 300m² lots. The future is not set, and discussions with Council and staff will have to work through these issues to balance the needs of housing stock, and the needs of other related policies like the Urban Tree canopy, liability of streets without creating the feel of a parking lot, and how to provide a workable multi-modal transportation system that lessens the desire for residents to own more than one car.

DISCUSSION:

In 2016 the City of Courtenay requested McElhanney to conduct a parking review within a defined commercial centre of the downtown core, as illustrated by Figure 1 on the next page. This review was intended to summarize the relationship between private and public parking, the time of peak utilisation, and identify the relative areas of highest occupancy. This study is noted throughout the report as the “2017 Parking Study” and used as a comparative matrix to the data collected in 2023.

This study used human labour for data collection recorded in 30-minute intervals throughout the day, between 9:30am and 5:30pm. The four sections defined by the study area map are equivalent to the area that could be walked by each individual in 30 minutes.

2017 Public Parking Data Highlights:

The percentages shown in **Figure 1** (Public Parking) and **Figure 2** (Private Parking) correspond to each section of the downtown core area indicating the peak occupancy. The weekday is shown in white, and the weekend is shown in orange. They occur roughly between Noon and 1:00pm. See the Appendix for full details.

The pie chart indicates the relative occupancy percentage for weekdays versus weekends, noting that if they are similar values, the pie would have equal parts.

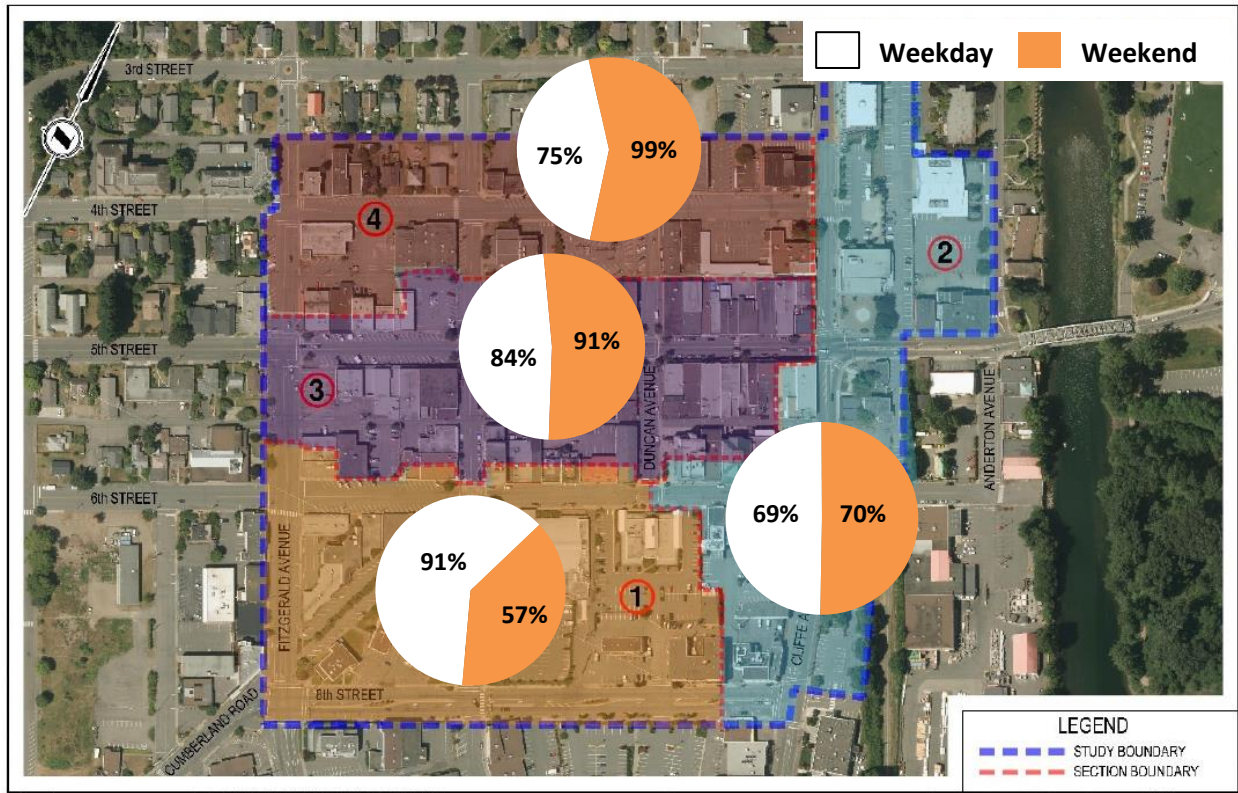


Figure 1: Public Parking Peak Occupancy, 2017

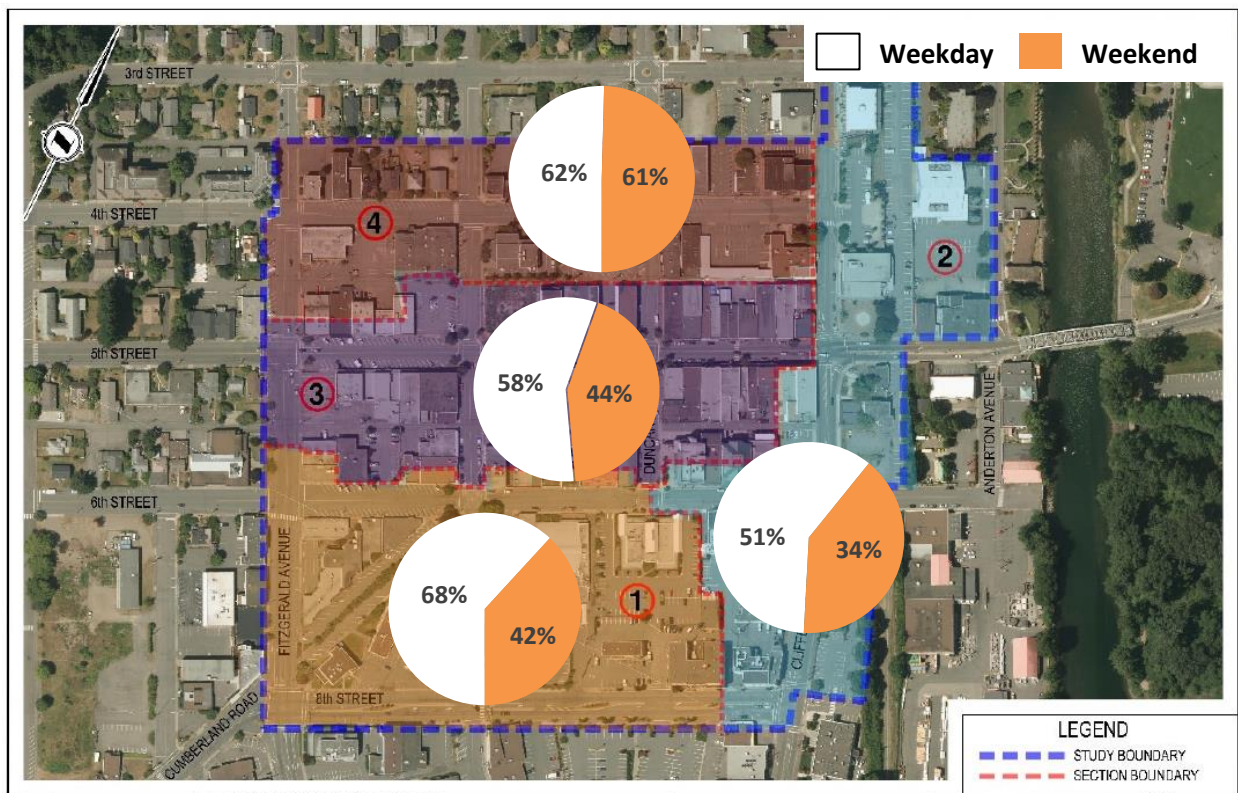


Figure 2: Private Parking Peak Occupancy, 2017

2023 Parking Study

McElhanney was again retained to update the 2017 parking assessment with the same objectives, however, the study area was expanded, and the methodology was updated with new technology to allow for future analysis. This new methodology would also allow for insight into parking behaviours.

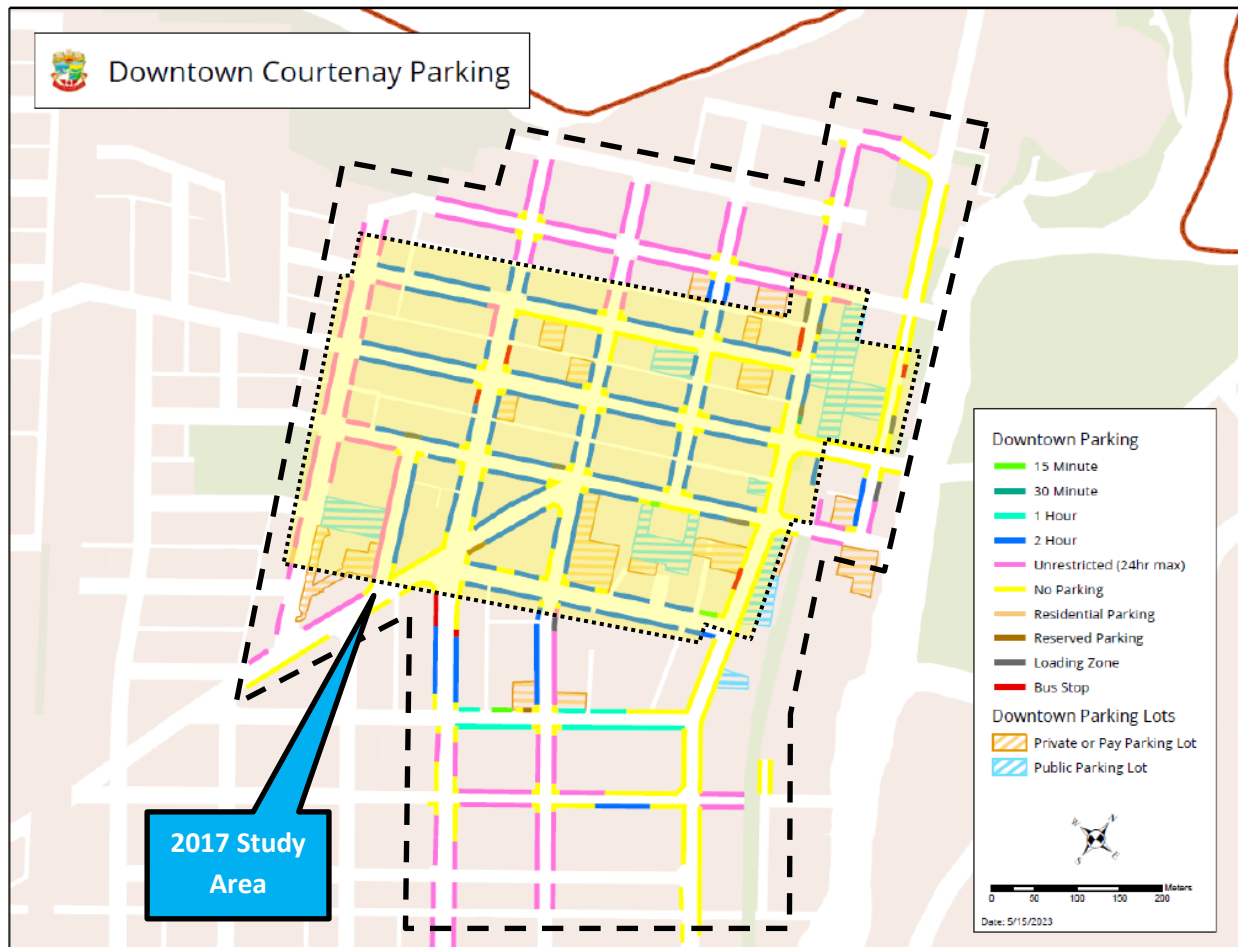


Figure 2: 2023 Expanded Parking Study Area

While the area had expanded, a comparison between the new and previous data was still possible by conforming the data and area to the same as 2017 with the four sections.

Public Parking Data Highlights, 2023

The percentages shown in **Figure 3** (Public Parking) and **Figure 4** (Private Parking) correspond to each section of the downtown core area, and indicate the peak occupancy. The weekday is shown in white, and the weekend, shown in orange. They continue to occur roughly between noon and 1:00pm while the weekend peaks an hour earlier than 2017; at noon. See the Appendix for full details.

The pie chart indicates the relative occupancy percentage for weekdays versus weekends, noting that if they are similar values, the pie would have equal parts.

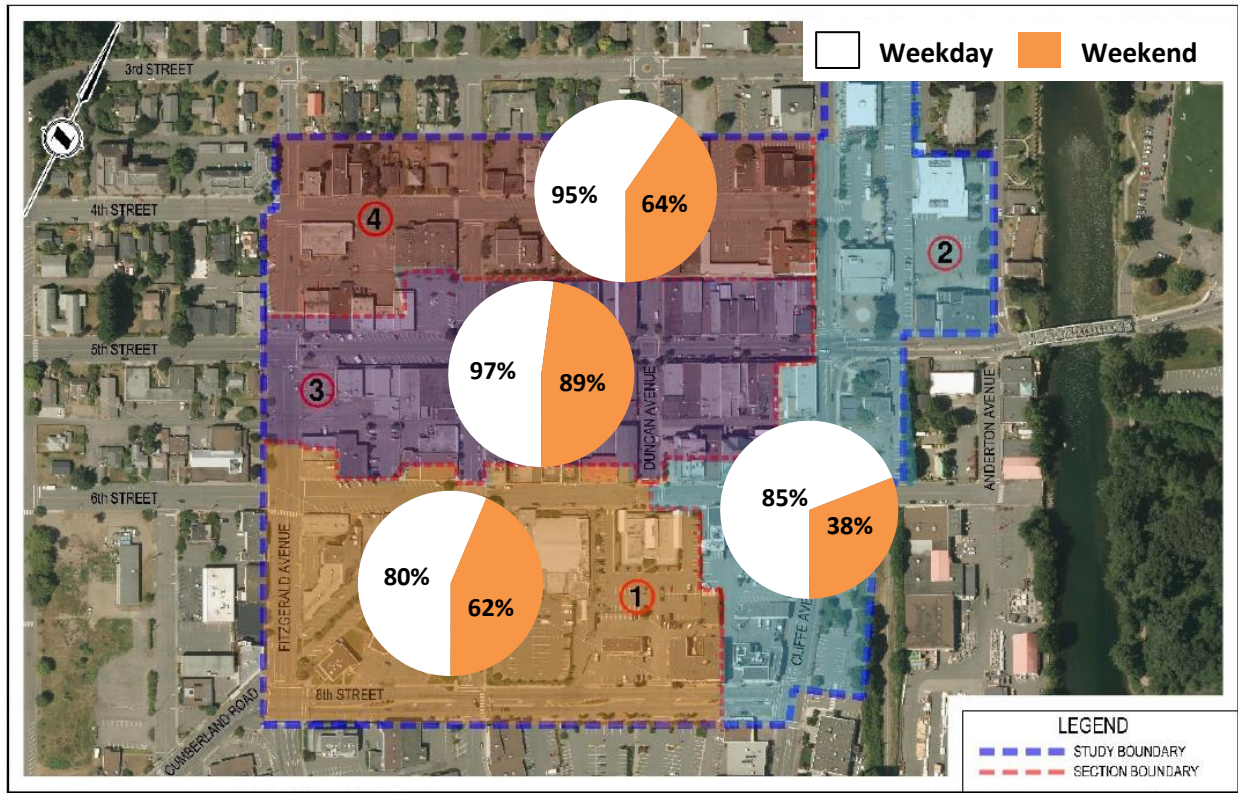


Figure 3: Public Parking Peak Occupancy, 2023

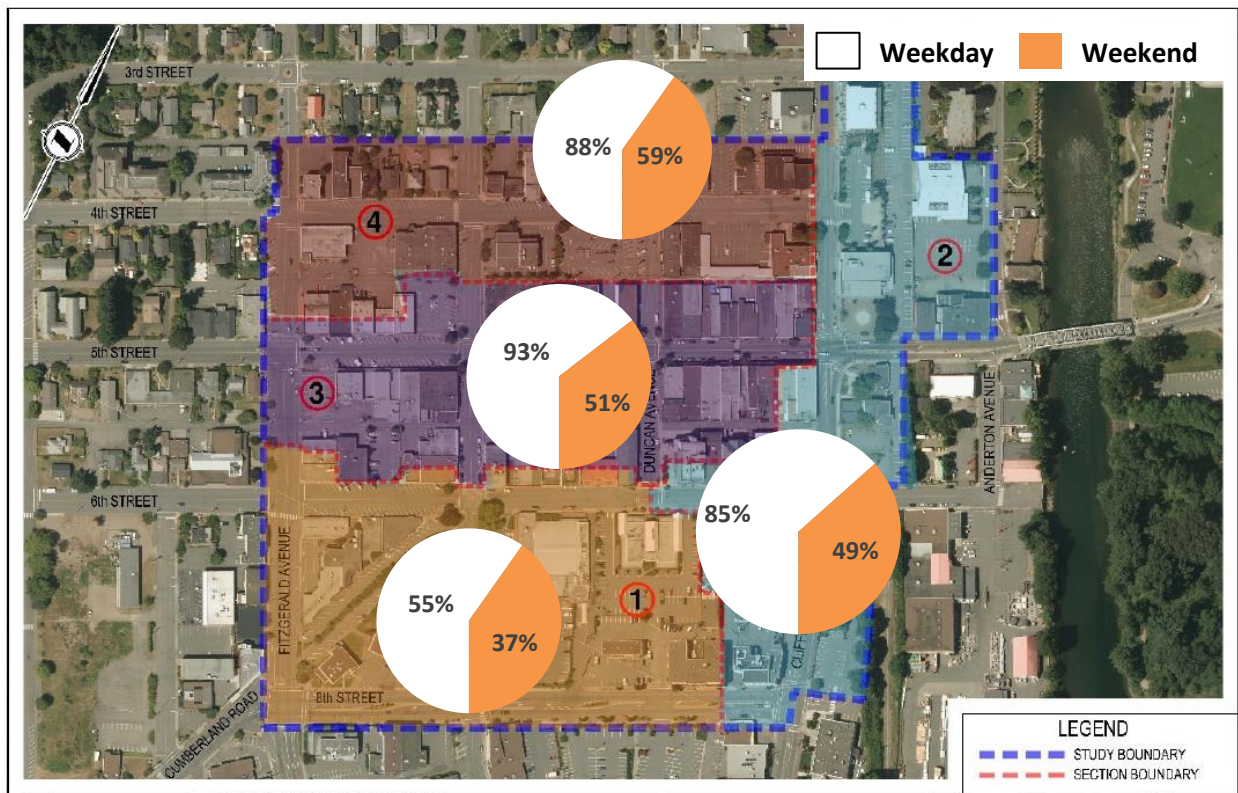


Figure 4: Private Parking Peak Occupancy, 2023

Comparison to the 2017 Study

Public Parking

1. Section 3 and 4 (5th and 4th Street) had the highest occupancy (97% and 95% respectively) on the weekday, [[was Section 1 in 2017](#)]
2. Section 2 weekday occupancy jumps to from 50% to 80% during 1:00pm - 2:00pm [[was steady and low in 2017](#)]
3. Weekend occupancy peaks at 12:00pm [[one hour sooner than 2017](#)]

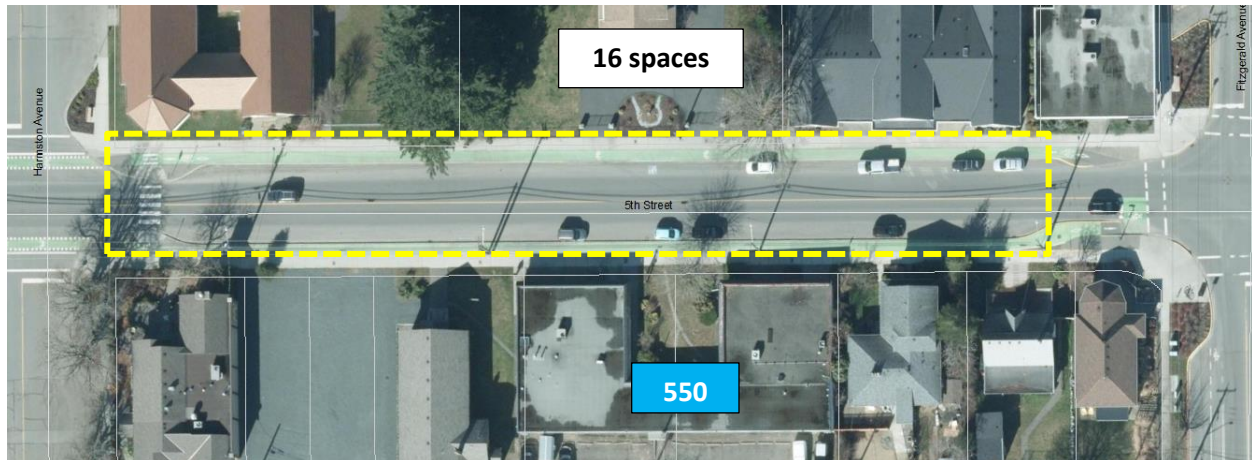
Private Parking

1. Section3 (5th Street) had the highest occupancy (93%) on the weekday, [[was section 1 in 2017](#)]
2. Peak occupancy now the same as public parking [[was substantially lower in 2017](#)]

SITE-SPECIFIC QUESTIONS

While the 2023 report reviewed the expanded area and compared the data with the same area used in 2017, it also looked at several public and private locations in order to answer some parking-related questions. Below is a summary of each public parking area, with the highlights of the private parking lots mentioned in the report:

A. 550 5th Street Frontage

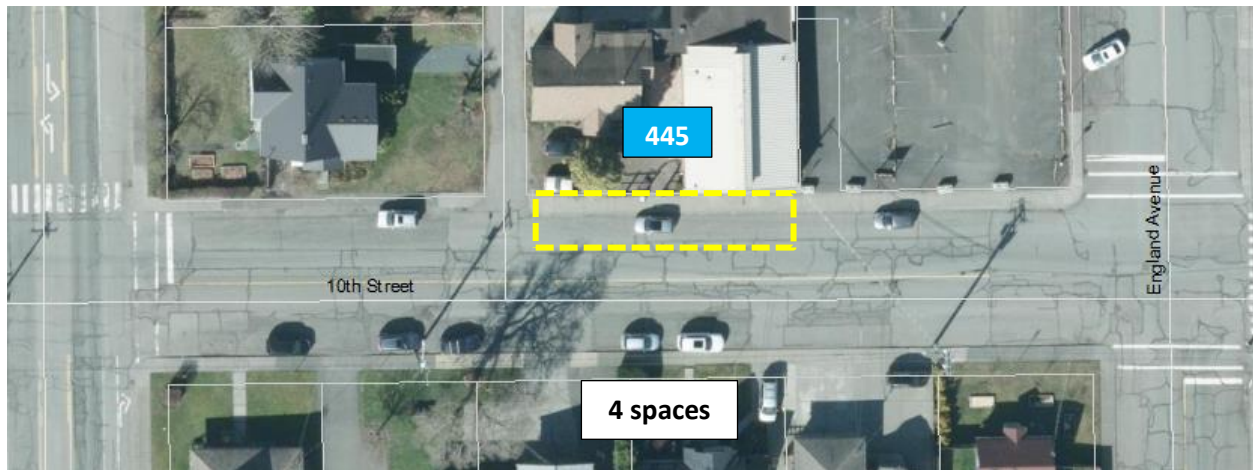


Question 1: Are the timed parking restrictions at 550 5th Street appropriate?

Of the 16 available parallel spaces, occupancy reached between 60-80% on a weekday, and 60% on a weekend.

Answer: Implementation of a time restriction, while not required, would formalise the existing parking behaviour.

B. 445 10th Street Frontage



Question 2: Are the timed parking restrictions at 445 10th Street appropriate?

Of the 4 available parallel spaces, occupancy reached 100% twice (12:00pm and 5:30pm) on a weekday, and 50% on a weekend.

Answer: Implementation of a time restriction that better suits the needs of the current occupant is recommended. The existing 15minute restriction was previously set up for the bakery that no longer exists. This change can be met through the Operational Services Department as part of their core services.

C. 5th Street Corridor

Angled Parking (Fitzgerald Ave to England Ave) North Side Only - Weekday



Question 3: What is parking like on the 5th Street corridor, between Fitzgerald Avenue and England Avenue on a weekday?

This area is within section 3, which had the highest occupancy (97%) for 2023. Being densely populated with small commercial shops it has 24 angled parking spaces with a two-hour time restriction and one accessible parking space. During the weekday, 142 parking movements occurred with the majority of vehicles (89 of 142) parking for 30 minutes or less.

The single accessible parking spot was occupied by two vehicles for a total of one hour.

Answer: This parking behaviour is considered healthy and vibrant with significant spare capacity for accessible vehicles.

Parallel Parking (Duncan Ave to Cliffe Ave) – North Side Only – Weekend



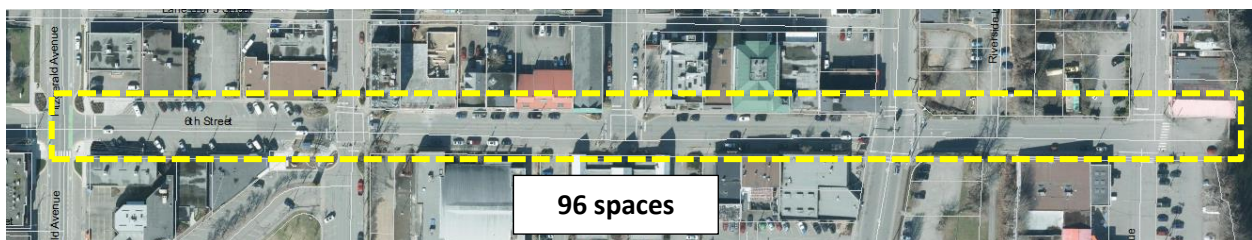
Question 4: What is parking like on the 5th Street corridor, between Duncan Avenue and Cliffe Avenue during a weekend?

5th Street east of Duncan Avenue has parallel parking. The nine parking spaces monitored had a total of 49 parking movements. The occupancy was never below six vehicles (67%) and most vehicles (22 of 49) stayed for 30 minutes or less.

One vehicle stayed for 6.5 hours which is an indication that not all drivers are obeying the time restriction.

Result: enforcement of the time restriction could assist compliance with driver behaviour; however, the vast majority of drivers are compliant, making the parking behaviour relatively healthy and vibrant.

D. 6th Street Corridor



Question 5: What is the parking demand along 6th Street?

The angled parking area (one block north and south) has 17 parking spaces on the north side and 23 on the south, comprising 42% of the total parking (40 of 96). The remainder of 6th Street (three blocks north and south) has 56 parallel parking spaces.

For the entire corridor, weekday occupancy peaks at 12:00pm (~83%) and is steady between 10:00am and 2:00pm, whereas on a weekend the peak occupancy is at 12:30pm (~67%).

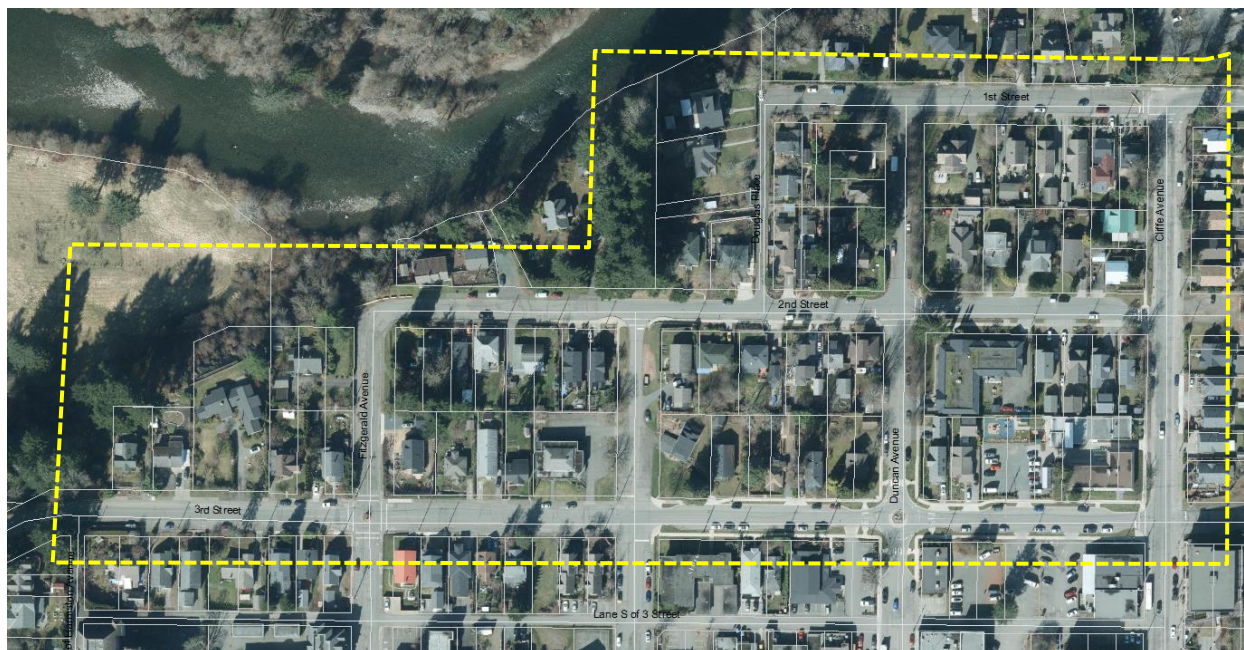
Answer: This street is considered at capacity. Occupancy greater than 80% generally indicates de facto capacity, as spaces become available, it takes time for other vehicles to spot it, and sometimes circle the block in order to park in it.

Question 6: Can the angled parking be converted to parallel parking given the current demand?

Answer: Conversion of angled parking to parallel parking would need to be reviewed in the context of the ability of the surrounding area to shoulder an increase in parking demand, as parallel parking will remove some parking capacity. The trade-off would make 6th Street more viable for Active Transportation modes, which would enhance the corridor's attraction for multiple modes of travel, especially given the 6th Street Active Transportation Bridge project that recently was given the green light to proceed.

There are significant sections of parallel parking that may be able to be converted to angle parking on 5th Street, for example. An assessment of this area to determine the feasibility would confirm this possibility for additional capacity in the future.

E. Old Orchard Area



Question 7: Is there overflow parking from the commercial core into this residential neighbourhood?

This area comprises 207 parking spaces and has a relatively consistent weekday occupancy starting at or before 10:00am until 2:00pm. The peak occupancy (137 of 207) was at 1:00pm (66%).

After 2:00pm, the weekday occupancy drops to approximately 28%, at the end of the work day (5:30pm). An assumption could be made that parking related to the commercial centre is occurring prior to 10:00am when the analysis started, and they gradually left the area after 2:00pm.

Weekend occupancy (50 of 207) was found to be less than half (24%) of that of the weekday at all times. This may indicate the number of residential-related parking on the streets.

Answer: Almost half of the parked vehicles could be attributed to longer-term external parking; typical of employee parking. 42% of the parking within this area could be attributed to the commercial centre ([137-50]87 of 207) while 58% could be attributed to the residents (50 of 207). Feedback from the neighbourhood could help assess if this is the case.

F. Overall Questions

These questions are directed at the expanded parking area of the 2023 assessment.

Question 8: Is the overall parking supply sufficient and what is the turnover rate?

Public Parking occupancy is 71% at 1pm, which is considered near capacity, however, there are several streets surrounding the downtown core that have occupancy rates of 56% or less. This may indicate that these streets are considered too far for many of the drivers to walk to their destination.

Although the overall turnover rate was not given in the report, all indications from the downtown parking areas suggest that turnover is quick and most vehicles that are parked in time-restricted areas (2hr parking limit) do so for 30 minutes.

Private Parking occupancy is 76% at 1pm, which is considered near capacity, however, five areas with private lots had occupancy rates of 50% or less. This may suggest that private lots are catering to those customers only, or the private lots are not well known to the general public.

Although an overall turnover rate was not given, the indications from several of the private lots show longer stays than public street parking, which may indicate employee parking. Management strategies in private lots could constrain employee parking to a specific area of the lot, or require employees to park elsewhere during times of heavy demand like Christmas. Understanding how each private lot operates would require interviews with each lot owner.

G. Parking Structure

Although the core area of downtown, generally identified as the 2017 study limits, are considered at capacity for public and private parking, there is a substantial amount of parking surrounding this area to supply the existing demand that is within walking distance to the core.

It may not be used because the turnover rate in the core is high, so drivers seeking a parking space can generally find one in the area they want, with some wait time. This may translate into a significant amount of circulating traffic seeking parking spots as the occupancy reaches the peak period (noon to 1pm).

A parking structure is not required at present, but should be considered when the surrounding streets and parking lots around the core reach near capacity. A trigger for this consideration could be 80% of all public and private parking spaces are occupied in the 2023 assessment area that are outside of the 2017 study area (ie. All the surrounding parking that has a low occupancy rate at present).

A parking structure could decrease the circulating traffic significantly, based on several factors, like:

- if the occupancy of the structure is available to the public,
- if the cost for parking was priced appropriately,
- if the location was within the demand area, and
- if the structure was clean, vibrant and had 'eyes on' from several perspectives for a feeling of safety.

Question 9: Is it possible to provide insight into the City's current bike parking supply?

Answer: Yes. Although not explored in the overall study area within the report, the web-based tool created by McElhanney can analyse this. One example given states the Vancouver Island Regional Library - Courtenay bike parking had 22 bicycles use the facility across the four survey days.

Another example shows the bike facility outside the Art gallery was used by seven bicycles on a typical weekday with an average stay of one hour. City staff have access to this tool for ongoing assessments, pending access to the tool.

Question 10: How well-used are the accessible parking spots?

Answer: Although not explored for the overall area of the 2023 report, this can be analysed with the web-based tool. One example given is the south side accessible parking on 5th Street by Fitzgerald Avenue. It was used by at least 11 vehicles across the four survey days.

Another example indicated the north side accessible parking space on 5th Street at England Avenue showed two vehicles used it on a typical weekday. These examples seem to indicate a significant spare capacity that can be confirmed through an area-wide assessment by City staff, pending access to the tool.

Private (Off-street) Parking Assessments:

A summary of the key findings from the Assessment of private parking lots is given below.

A. Shopper's Drug Mart – Weekday (107 spots)

This lot has 62% occupancy at 9:30am which remains for most of the day, showing approximately 50% of the parking is long-term, assumed to be employee parking.

B. 4th Street/Fitzgerald Ave Surface Parking Lot – Weekday (19 spots)

This location shows a steady 60% occupancy until 2:30pm, and by 5:30pm is almost empty. Most vehicles are parked long-term, indicating employee parking.

C. 4th Street/England Ave Surface Parking Lot

Although not explored in the overall study area within the report, the web-based tool created by McElhanney can analyse this once staff are given access to the tool.

Public (Off-street) Parking Assessments:

Some additional sites have been identified that were not part of the study, but are included here to indicate sites that would ensure the assessment is comprehensive. Once city staff gain access to the analytical tool created by McElhanney, they can be assessed and included in any future assessment or operational modifications.

D. Vancouver Island Regional Library – Courtenay (76 spots)

The parking was found to be quite variable on the two-weekday study dates, ranging from 55% occupancy to 93% occupancy at 10:00am. In each case the parking demand diminished significantly by 2:30pm.

Weekend occupancy peaked at 32%.

E. 4th Street/Duncan Ave Surface Parking Lot – Weekday (47 spots)

Occupancy rose significantly after 10:30am to 100%, which decreased only slightly by 3:00pm (80% occupancy). This parking lot is heavily used.

F. Lower Filberg Surface Parking Lot – Weekday (40 spots)

Serving the Filberg Centre, the peak occupancy at this location is 60% between 11:00am and 2:30pm.

G. Upper Filberg Surface Parking Lot – Weekday (80 spots)

Serving the Sid Williams Theatre and surrounding amenities, this parking lot peaks between 12:30pm to 2:30pm at 70% occupancy with a significantly lower occupancy before and after.

H. Sid Williams Theatre/Native Sons Hall Surface Parking Lot – (30 spots)

Serving the theatre, the Native Sons Hall, and local amenities, the lot is well used with occupancy varying throughout the day between 60-90% the entire study period.

Weekend use may be influenced by events as the parking lot varied between 18 and almost no vehicles.

I. 721 Grant Avenue (gravel parking lot)

Although not explored in the overall study area within the report, the web-based tool created by McElhanney can analyse this once staff are given access to the tool.

J. 645/655 Cliffe Avenue (opposite City Hall) Parking Lot

Although not explored in the overall study area within the report, the web-based tool created by McElhanney can analyse this once staff are given access to the tool.

RELATED CONTEXT

R-SSMUH Zoning

By June 30th the province will be initiating the ability for developers to increase the housing stock on parcels of land identified as eligible for Small-Scale Multi-Unit Housing (SSMUH). The eligible properties within the City boundary are illustrated in **Figure 5** below.

The impact on public parking will be felt as the Off-street (Private) Parking requirement will be reduced from two spaces per dwelling unit to one space per dwelling unit. This reduced requirement may not immediately reduce the number of vehicles owners or renters will have when occupying the smaller units, and may increase the demand for public (on-street) parking outside of their unit.

In concert with this reduction in required on-site parking, is the ability to create four dwelling units on any of the lots, including the smallest at 300m². These small lots, shown in Figure 5 as the orange colour, indicate all parcels between 300m² to 699m², which surround the downtown core.

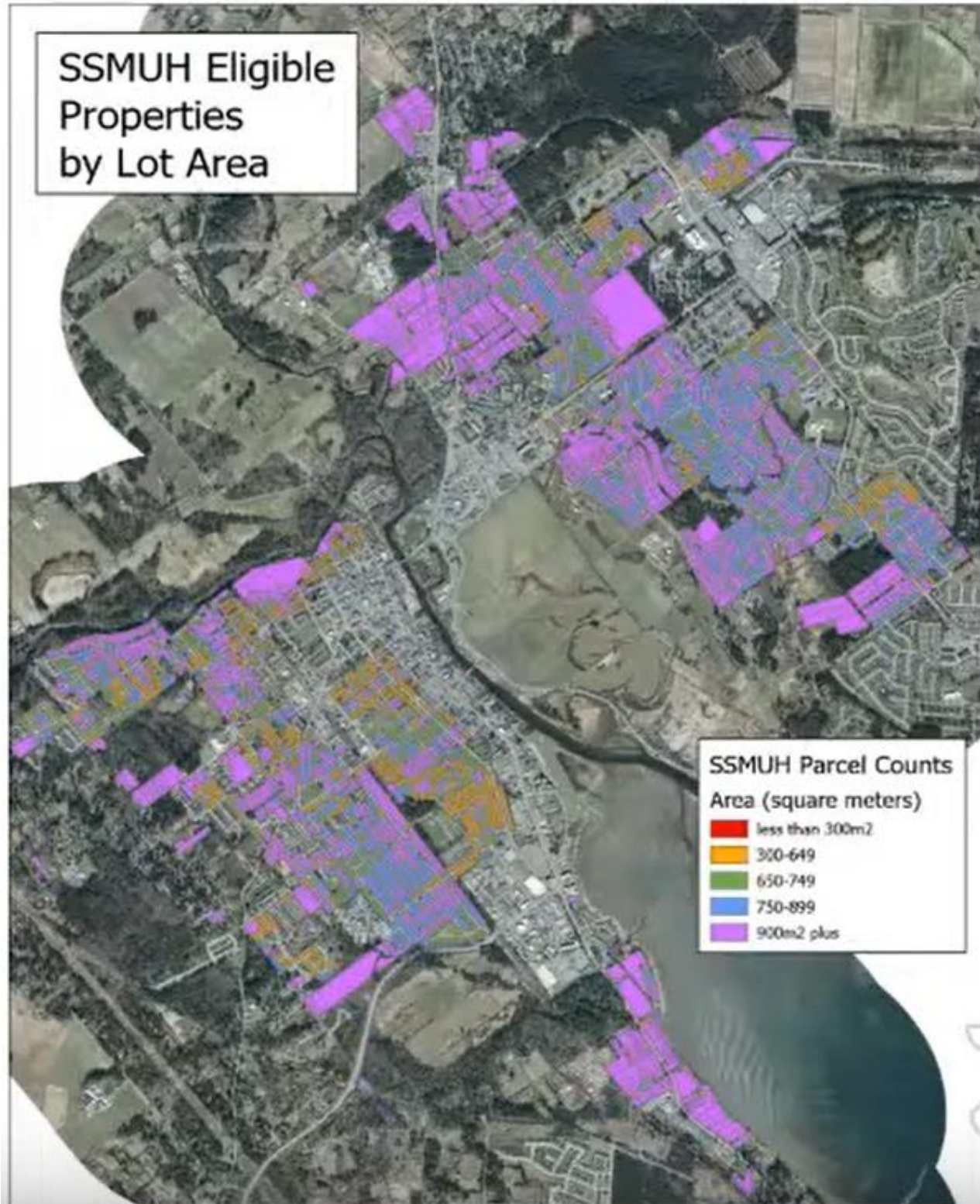


Figure 5: Eligible SSMUH Properties (Image reproduced)

A 'Walkability Radius' is also being pursued, as indicated by **Figure 6**, whereby the parking requirement can be further reduced for housing units on a SSMUH. The darker centre circle illustrates a 5-minute walk to the downtown core and the lighter outer circle illustrates a 10-minute walk to the downtown core.



Figure 6: Area of Walkability Radius Parking Reductions

Walkability Radius

The SSMUH zone could allow for a reduction of one on-site parking vehicle space if the lot was within the Walkability Radius, should Council desire this characteristic. The implication, and general desire from a transportation planning perspective, is that the proximity to the downtown has many more alternative means of mobility that the public can use to get to and from many primary and secondary shopping destinations that do not require a personal vehicle. Over time, people may choose not to own a second vehicle, and the demand for parking can be reduced in this area.

Residential Parking Permits

One management tool under consideration is the use of permit parking for the streets in front of the SSMUH zones where required. Generally, the single parking space required on-site would be mirrored for the street, so one space on the street would also be eligible for a residential parking permit tied to the dwelling unit.

Next Steps

The future is not yet set, and discussions with Council and staff will have to work through these issues to balance the needs of housing stock, and the needs of other related policies like the Urban Tree canopy, liability of streets without creating the feel of a parking lot, and how to provide a workable multi-modal transportation system that lessens the desire for residents to own more than one car.

CONCLUSIONS AND NEXT STEPS:

The parking during the weekdays and weekends is healthy and vibrant and has significant spare capacity outside of the immediate core of the downtown, but well within walking distance. Ensuring that people can safely and comfortably walk to the downtown from all sides of the downtown should be a focus of future active transportation projects that support the core's economic development.

Parking is related to many aspects of mobility, customer satisfaction, inclusivity, and how we accommodate personal vehicles on public rights-of-way. In many areas, the parking exhibited by the public is very healthy, albeit likely has circulation issues in areas exhibiting higher than 80% occupancy.

How we ultimately decide to accommodate parked vehicles, whether by angled or parallel parking, or with public parking structures like parking lots, will determine how our streets are used, and reflect the hierarchy of mobility priority.

With greater alternative modes, comes a greater demand for space within the right-of-way, but this may ultimately reduce the demand for personal vehicles, and therefore parking. However, with a potential rise in housing stock, parking spaces on the street will likely stay very similar to the capacity assessed within this report, and demands may rise to trigger the review of a parking structure at some future point.

Next Steps:

As these works aim to serve as a baseline for all future works including land use planning and or master planning functions relative to the future state of parking, including; Official Community Plan Implementation (Densification), Downtown Revitalisation, and the updating of the Downtown Courtenay Playbook. The City will also need to consider the impacts relative to several inflow works, with these works serving to benefit from these analytics both in terms of future design guidelines and or strategic guidance, while considering the future needs of parking as redevelopment continues to occur;

- **6TH Street Active Transportation Options Analysis;** as it transitions from a vehicle-based roadway to one that supports all active modes should manage parking that reflects the new demands and connections the 6th Street Active Transportation Bridge brings. Having a larger streetscape that is flexible to the City's needs for events and active mobility as it gets closer to Cliffe Avenue. How this happens will require a holistic look at all the surrounding parking and area needs.
- **CVRD Downtown Transit Exchange;** movement and redevelopment of the new Transit Exchange will have an offsetting impact on parking as it stands today. That said, mode shift and parking alternatives such as, micro-mobility and bike parking can be considered complementing offsets.
- **SSMUH Zoning;** coordination with these new requirements will need to consider collaborative and transparent parking procedures for parking management for area of the city in the near future as impact on the development of the housing stock will inevitably impact parking availability.

- **Traffic Regulation Bylaw;** modification of the Bylaw will be required to reflect the new needs of parking permits, including the enforcement or removal of the parking restrictions, which could be completed concurrently with the edits required for the speed limit reduction project, or as it is needed in the near future.

POLICY ANALYSIS:

The Traffic Regulation Bylaw can be modified to remove all of the parking restrictions noted within it that are documented in a tabular format. The parking restrictions do not require inclusion in the bylaw for enforcement and would offer the Operational Services Department the freedom to shift or augment current parking restrictions to best meet the emerging and or trending needs of the community.

Any decision to create a residential parking permit process may require inclusion into the bylaw with enforcement processes and fine rates linked from the Fees & Charges Bylaw.

FINANCIAL IMPLICATIONS:

There are no financial implications associated with this report.

ADMINISTRATIVE IMPLICATIONS:

The addition or adjustment of parking signs, restrictions, and modifications to the angle of parking are all part of Operational Services core duties. That said, currently the Traffic Regulation Bylaw No. 1926 was last updated in 1996 and as such, it is recommended that said bylaw be subjected to a thorough review and updating process.

Staff or consultant capacity may be required to facilitate residential parking permit process, as deemed desirable, to handle SSMUH development procedures.

A fulsome review of both bike and accessible parking spaces would assist the Downtown Core Business Investment Committee and City staff to ensure there is a consistent spatial distribution of these parking spaces throughout the core.

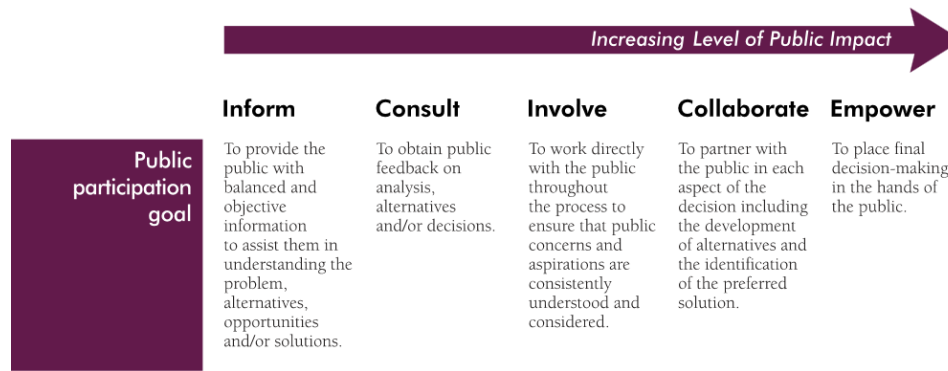
STRATEGIC PRIORITIES REFERENCE:

This initiative addresses the following strategic priorities:

- Streets and Transportation - Review City's approach to parking standards downtown: Parking restrictions and ticketing
- Streets and Transportation - Plan and implement bike parking and secure storage

PUBLIC ENGAGEMENT:

Staff would inform the public based on the IAP2 Spectrum of Public Participation:



RECOMMENDATION: THAT Council receive the “Downtown Courtenay Parking Study” for information and that the study be used as a baseline data set to support future active transportation design standards, regulatory standards, parking enforcement standards, and the implementation of the Official Community Plan.

ATTACHMENTS:

- 1 – 20240404 Courtenay Downtown Parking Study Report – Final.pdf
- 2 – Parking Study Data Analysis.docx
- 3 – 2017-01_Jan-12 47416-17 Parking Summary Memo FINAL.pdf


Prepared by: Paul Butterfield, Transportation Technologist
Reviewed by: Kyle Shaw, Director of Operational Services
Concurrence: Geoff Garbutt, M.PI., MCIP, RPP, City Manager (CAO)



Layer

Detected Cars 0

Counts


50

Cars detected using the selected filters. Zoom or pan the map to update count.

Downtown Courtenay Parking Study

April 4, 2024

Submitted to: City of Courtenay
Prepared by McElhanney

Contact

Matthew Browning P.Eng.,
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Our file: 2121-01086-00



Your Challenge. Our Passion.

April 4, 2023

City of Courtenay
1000 Piercy Avenue
Courtenay, BC, V9N 3E6

Attention: Michael Wright, Manager of Public Works Service | Operational Services

Downtown Courtenay Parking Study

As requested by the City of Courtenay, we are pleased to provide this report outlining the purpose, objectives, methodology, analysis and findings of the Downtown Courtenay Parking Study.

We understand that the City wishes to have an updated parking study that provides new insights over and above those delivered as part of the previous iteration of the downtown parking study (2017) and as such we have undertaken this study utilizing an innovative approach that maximizes the value of the analysis for the City.

Through conversations with the City, we have undertaken aerial drone imagery of the downtown core of the City to generate orthomosaic maps of parking behaviours across all-day surveys for weekday and weekend periods. This provides the City with a reliable, insightful and easily-communicable set of data that can be used to support ongoing planning and development efforts throughout the downtown core.

Thank you for your enthusiastic and collaborative support on this cutting-edge approach to transportation planning.

Sincerely,
McElhanney Ltd.

Prepared by:

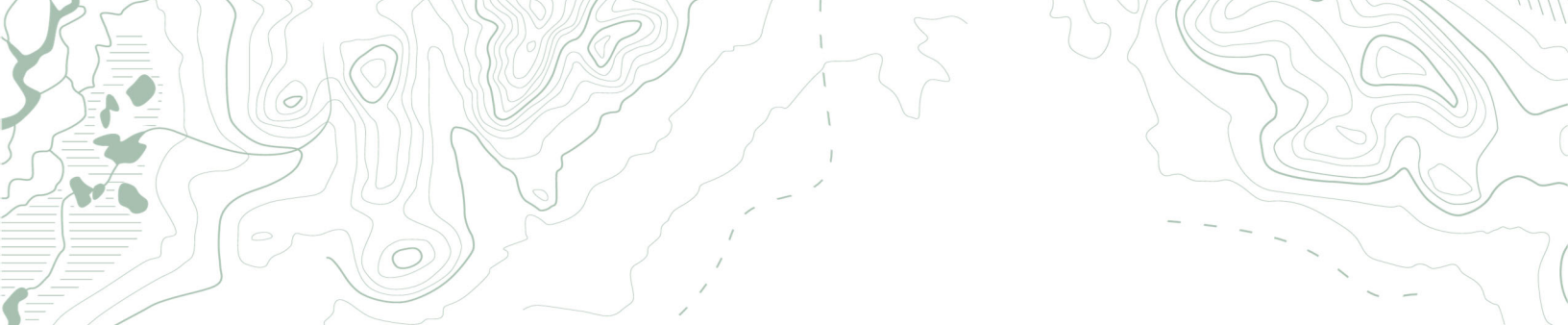


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1. Introduction

The City of Courtenay has retained McElhanney to undertake an update of the 2017 Downtown Courtenay Parking Assessment study. The following sections outline the purpose, objectives, methodology and limitations of the previous study, as well as the methodology and outcomes of the current study.

2. Study Purpose and Objectives

The purpose of the study was to collect accurate and reliable parking data for the downtown core area of the City of Courtenay that would allow for future analysis and provide insight into parking behaviours. This allows the City to better understand the relationship between public and private parking utilization, time of peak demand, areas of high (or low) utilization and overall parking behaviours.

The key identified objectives were:

- Focus on the downtown core but provide opportunity to expand the study area to support future planning activities.
- Complete an update to the previous downtown parking study to allow for a comparison of utilization and parking supply trends.
- Capture two weekdays and two weekends to provide a range of data for both periods that reflect different usage patterns and parking behaviours.
- Capture data across the entire day to provide insight into peak periods and utilization changes throughout the day.
- Differentiate between on-street parking and off-street parking lots

Through the study process a series of key questions were determined that the City has an interest in addressing, namely:

- Are the timed parking restrictions at 550 5th Street and 445 10th Street appropriate?
- What is the parking demand along 6th Street?
- Is the overall parking supply sufficient and what is the turnover rate?
- Is it possible to provide insight into the City's current bike parking supply?
- How well-used are the City's accessible parking spots?

3. Previous Study

3.1. SUMMARY OF PREVIOUS STUDY METHODOLOGY AND FINDINGS

The previous downtown parking study was completed in late 2016 and documented in early 2017. The study limits were broadly similar to those applied for this current work although constituting a smaller downtown core, focussed on the commercial centre (see *Figure 1*).

The 2017 study followed a traditional methodology for capturing parking data with the study area being segmented into four smaller sections to allow field staff to be able to count vehicles within public and private parking lots within a 30-minute increment. Weekday counts were conducted between 9:30am and 5:30pm on a Wednesday and weekend counts between 10:00am and 1:30pm on a Saturday.

The collected data was processed and presented as a series of tables, aggregating the totals for public and private parking across each of the four sections.

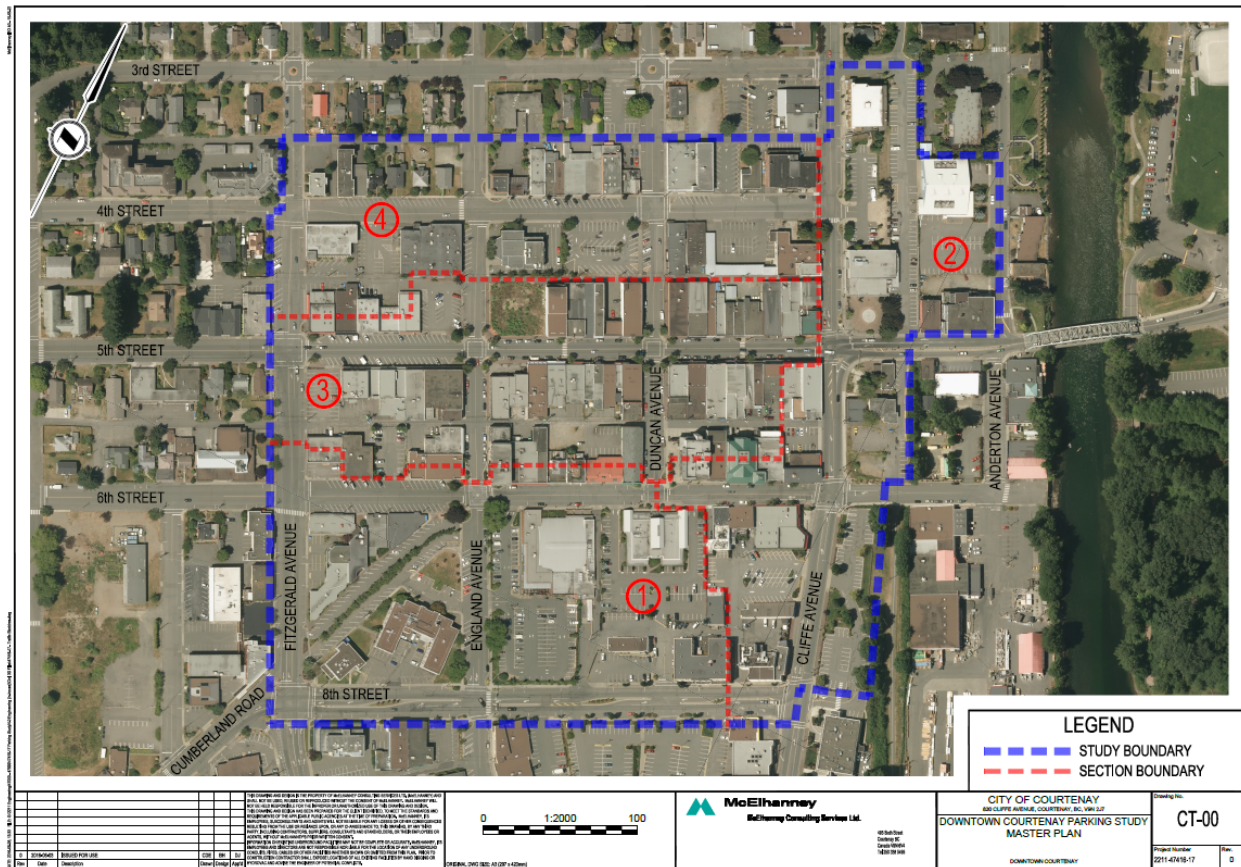


Figure 1 - 2017 Downtown Parking Study Area

Notable from the previous study was that the level of granularity available was minimal, with 'section-level' being the furthest it was possible to drill down spatially into the data. Further, there was no potential to review the fidelity or accuracy of the collected data, leaving some margin for human error at either the

collection, processing, or analysis stage of work. Due diligence was taken, but the methodology did not provide an opportunity for more insightful analysis or investigation.

Broadly, the following observations were noted:

- Total private parking spots = 374
- Total public parking spots = 889
- Private parking was generally more consistently utilized during weekday operations (compared to public facilities)
- Private parking was generally utilized less than 60% during weekday counts
- Weekday private parking counts did not identify a consistent peak of activity
- Weekend private parking counts trended toward higher utilization as the day went on
- Weekday public counts indicated a peak utilization between 10:00am and 2:30pm
- The 5th Street and 6th Street corridors yielded the highest utilization for weekday public parking (84% and 91% respectively)

4. Study Area and Methodology

4.1. AREA MAP AND ZONES

The downtown core area (see [Figure 2](#)) for this study was expanded somewhat from the 2017 study, with the inclusion of some adjacent residential areas and the commercial areas to the south of the traditional downtown core.



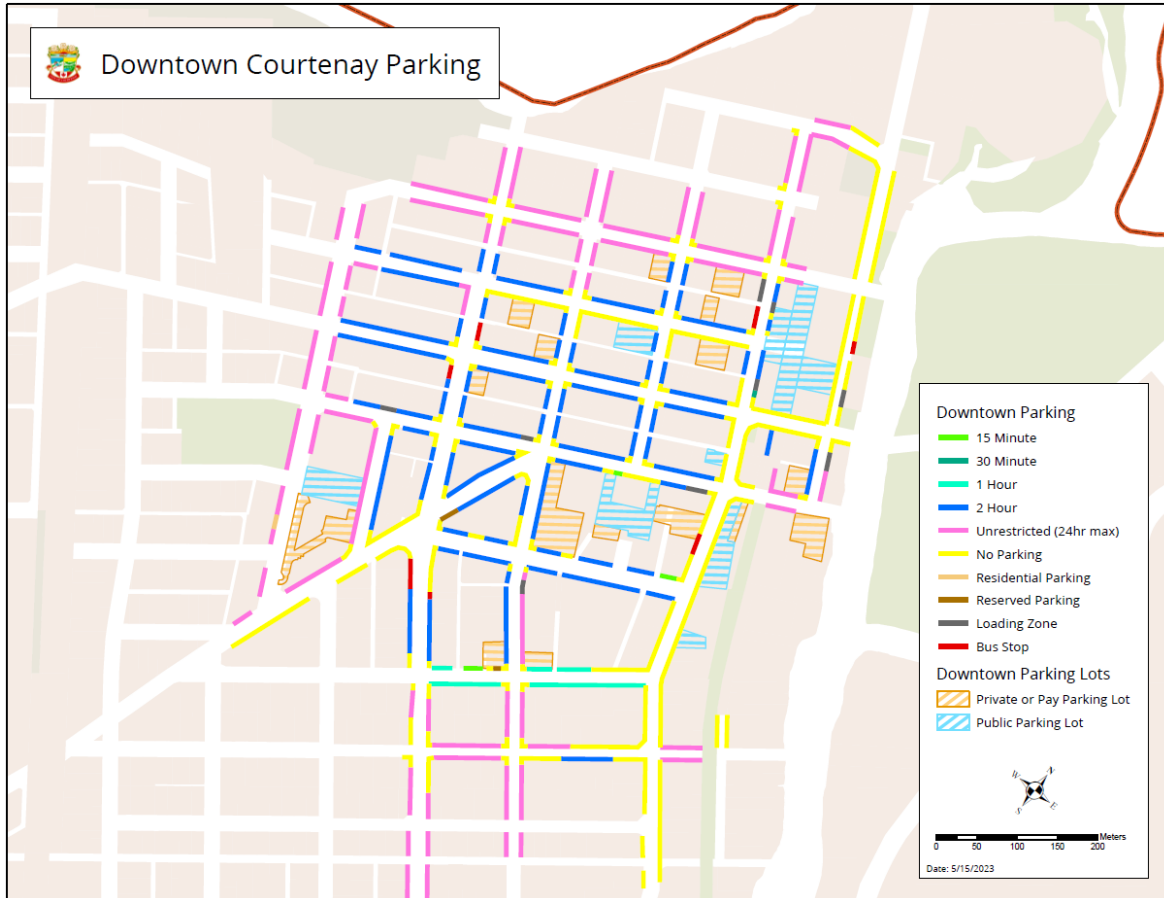


Figure 2 - Initial Extents of Parking Study Area with existing parking restrictions

Through discussion with City staff, the extents of the study area were confirmed as being:

- Northern extent: 1st Street between Anderton Avenue and Cliffe Avenue, 2nd Street between Cliffe Avenue and Fitzgerald Avenue
- Western extent: Harmston Avenue between 3rd Street and Cumberland Road, Fitzgerald Avenue between Cumberland Road and 13th Street
- Southern extent: 13th Street between Fitzgerald Avenue and Beckensell Avenue
- Eastern extent: Anderton Avenue between 1st Street and 6th Street, Beckensell Avenue between 6th Street and 13th Street

4.2. DRONE-BASED DATA CAPTURE METHODOLOGY

In response to the review of the previous study, which found that the parking data was informative, but not as insightful as needed, it was proposed that a new approach to capturing the parking data be pursued.

With the advances in drone technology in recent years and the availability of GIS software that is capable of undertaking vehicle identification processes, the methodology for this study was proposed as follows:



- Two weekday captures (9:30am to 18:00pm) on either Tues/Wed/Thur, avoiding farmer’s markets, events, or inclement weather
- Two weekend captures (10:30am to 13:30pm) on Saturdays avoiding events and bad weather
- Each capture event to involve a single drone operator piloting a drone mission to capture orthographic photography of the study area within a 30-minute timeframe
- Orthographic images processed and stitched to form a mosaic representing a snapshot of the study area during that 30-minute time period
- Compilation of all mosaic images into a complete GIS database and build a toolkit to enable parking analysis with the imagery
- Generate a dashboard such that further analysis can be undertaken as the need may arise in the future

Further information on the study methodology, including study dates, timing, data processing and dashboard development are found in [Appendix A](#).

5. Summary of Data Collected

5.1. OVERALL PARKING DATA SUMMARY

A total inventory of the cars that parked in downtown Courtenay bounded by the study area defined in [Section 4.1](#) is summarized in [Table 1](#). The data was collected for 30-minute time intervals. That is, if any vehicle occupied the parking spot within the 30-minute time interval it was recorded as occupied by only one vehicle. Weekday parking data was collected between 10AM – 6PM and weekend parking data between 10:00am – 2:00pm.

The study area includes residential and commercial neighbourhoods with a large number of private parking, off-street parking, and curbside on-street parking. Loading and delivery parking was omitted from the count, as were private, residential driveway vehicles. It should also be noted that the parking data is an aggregate value and does not provide parking duration of a single vehicle, only if an available space is occupied during the survey period or not.

Table 1: Parking Data Summary

Day Type	Time	Total Occupied Stalls		Off-Street Utilization (based on 1794 available stalls)	On-Street Utilization (based on 1192 available stalls)
		Off-Street	On-Street		
Average Weekday	10:00 AM	1279	717	66%	60%
	10:30 AM	1350	776	70%	65%
	11:00 AM	1401	808	73%	68%
	11:30 AM	1411	800	73%	67%
	12:00 PM	1413	840	73%	70%
	12:30 PM	1402	836	73%	70%



	1:00 PM	1458	852	76%	71%
	1:30 PM	1376	808	71%	68%
	2:00 PM	1311	830	68%	70%
	2:30 PM	1265	762	66%	64%
	3:00 PM	1234	704	64%	59%
	3:30 PM	1179	665	61%	56%
	4:00 PM	1088	613	56%	51%
	4:30 PM	910	513	47%	43%
	5:00 PM	746	430	39%	36%
	5:30 PM	700	386	36%	32%
Average Weekend	10:00 AM	551	364	31%	31%
	10:30 AM	575	400	32%	34%
	11:00 AM	605	430	34%	36%
	11:30 AM	627	438	35%	37%
	12:00 PM	657	456	37%	38%
	12:30 PM	690	459	38%	39%
	1:00 PM	699	438	39%	37%
	1:30 PM	693	432	39%	36%

Overall study area parking observations

At 1pm on a weekday, the busiest time on-average, approximately 76% of off-street parking is occupied. This may appear to be low, based on experiences with finding parking, however the following should be noted:

- 501 4th Street lot – 10/21 (50% occupied)
- 391 4th Street lot – 4/14 (30% occupied)
- 280-232 3rd Street lots – 32/60 (50% occupied)
- Cliffe Ave lots (east side from 10th Street to 12th Street) – 24/90 (25% occupied)
- 1175 Cliffe Ave lot – 14/56 (25% occupied)

There are many smaller, usually-commercial lots that are available throughout the broader downtown area that bring the overall utilization percentage down. The downtown core lots are typically more utilized during the same period.

Similarly, the busiest on-street parking utilization (71%, also at 1pm) may appear low, but there are significant stretches of curb outside the downtown core that are under-utilized, including:

- 2nd Street – 15/50
- 8th Street – 20/38
- 12th Street – 25/44



- Fitzgerald Ave – 39/84
- Harmston Ave – 39/94

Weekend parking utilization was observed to be significantly lower than weekday, with less variability across the time-period surveyed.

Based on the above analysis, it appears that there is sufficient supply within the broader downtown area to accommodate demand, although it may require drivers to walk a modest distance to their destination once parked. Many of the commercial parking lots on the fringe of the downtown do not become busy until later in the afternoon/evening, and so there are opportunities for parking to be supplied there, provided owner-cooperation is received (many businesses are signed 'for customers only').

At this time, there does not appear to be sufficient demand to warrant an additional parking structure within the downtown to provide substantially more parking supply.

5.2. PARKING UTILIZATION

Parking utilization refers to the degree to which parking spaces or facilities are being used or occupied within a given area or over a specific period (or both) as a measure of how effectively parking resources are being utilized by vehicles. Weekday and weekend parking utilization has been calculated and expressed as an occupancy rate; the total number of vehicles parked relative to the parking spaces available.

The utilization rate was calculated for each of the four sections of the downtown core to provide a like-for-like comparison with the 2017 study (extents provided in [Figure 3](#) for reference):



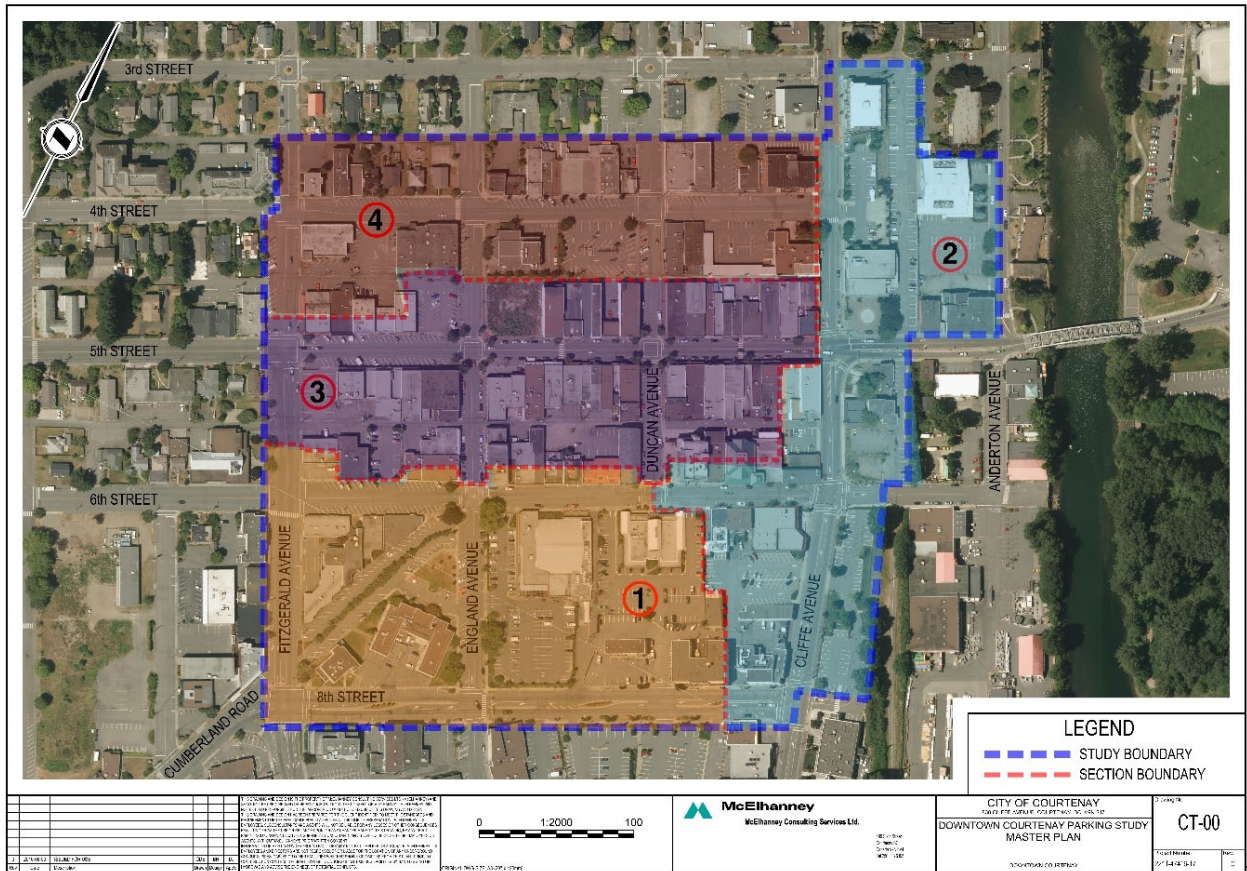


Figure 3: Downtown Parking Section Extents (from 2017 Study)

Figure 4 and Figure 5 show the average weekday on-street and off-street parking utilization, respectively, by time of day and section of the downtown. For both on-street and off-street parking, the utilization peaks at 1:00pm and gradually decreases as the day progresses. On a typical weekday, Section 3 has the highest and Section 2 the lowest off-street and on-street parking utilization.

Sections 1 (85%), 3 (93%), and 4 (88%) have substantially higher utilization than Section 2 (55%) for off-street parking due to lower parking supply while having high demand.



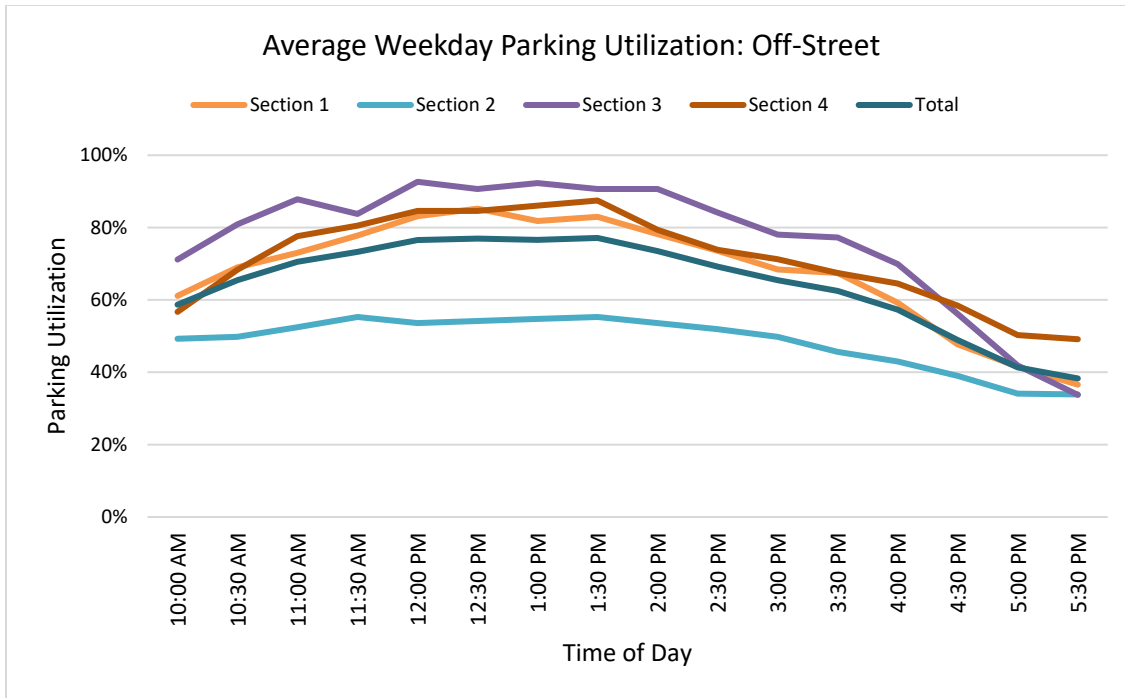


Figure 4: Average Weekday Off-Street Parking Utilization

On-street parking is more variable throughout the day and with an earlier peak that diminishes substantially in the afternoon. This could indicate that the off-street lots are occupied by all-day employees, whereas on-street parking is at least partly driven by discretionary trips (shopping, errands, drop-offs, etc.). Section 3 again has the highest utilization (97%), followed by Section 4 (95%), Section 1 (85%) and Section 2 (80%).

It is notable that the overall parking utilization for off-street parking never exceeds 80% of the available supply, whereas on-street parking is over 80% for around 3hrs during the middle of the day with Section 3 operating at close to 100% from 11am to 12:30pm.

Off-Street Parking Count					
Off-Street					
	Section 1	Section 2	Section 3	Section 4	Total
Time/Date					
9/21/2023					
10:00 AM	145	125	85	87	446
10:30 AM	176	125	93	104	504



11:00 AM	178	129	99	117	530
11:30 AM	189	146	87	129	560
12:00 PM	219	149	111	131	619
12:30 PM	215	156	108	134	621
1:00 PM	206	154	107	136	610
1:30 PM	214	150	106	130	608
2:00 PM	193	149	109	120	581
2:30 PM	182	147	106	116	559
3:00 PM	176	138	97	114	531
3:30 PM	170	124	95	98	489
4:00 PM	152	118	84	93	448
4:30 PM	119	107	70	89	388
5:00 PM	102	99	55	79	336
5:30 PM	86	94	37	85	307
9/27/2023					
10:00 AM	174	135	90	108	516
10:30 AM	184	138	106	131	569
11:00 AM	203	148	117	150	627
11:30 AM	217	146	119	148	642
12:00 PM	215	134	117	160	636
12:30 PM	230	130	115	157	641
1:00 PM	221	135	120	160	646
1:30 PM	219	142	117	171	657
2:00 PM	215	134	114	153	624
2:30 PM	202	127	101	138	576
3:00 PM	181	125	95	131	542
3:30 PM	182	117	95	134	536
4:00 PM	157	109	88	129	492
4:30 PM	130	99	68	112	414
5:00 PM	115	81	48	94	342
5:30 PM	105	85	46	84	321



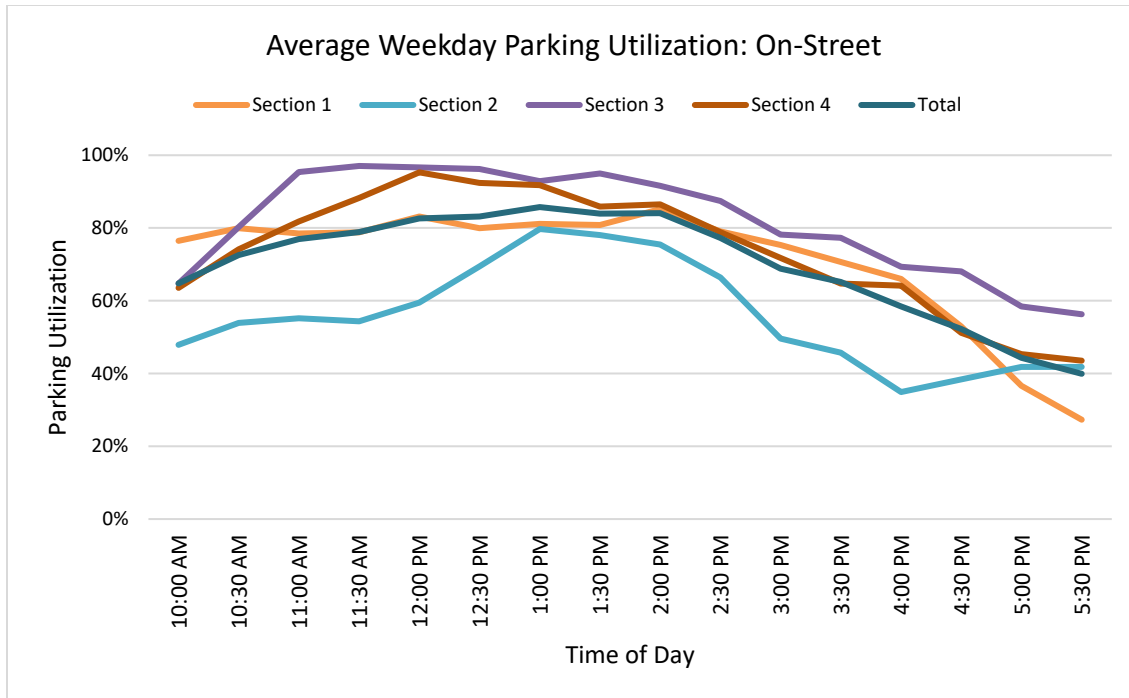


Figure 5: Average Weekday On-Street Parking Utilization

On-Street Parking Count					
	Section 1	Section 2	Section 3	Section 4	Total
Time					
9/21/2023					
10:00 AM	126	43	57	47	274
10:30 AM	124	53	85	51	312
11:00 AM	128	57	108	61	350
11:30 AM	130	61	112	70	372
12:00 PM	140	75	117	79	409
12:30 PM	135	91	113	72	408
1:00 PM	135	95	102	71	400
1:30 PM	139	92	107	61	395
2:00 PM	142	89	102	64	392
2:30 PM	139	82	100	59	375
3:00 PM	126	53	91	55	322
3:30 PM	110	58	93	51	310
4:00 PM	105	42	86	50	279
4:30 PM	79	46	79	49	248
5:00 PM	57	50	63	41	210
5:30 PM	49	51	64	41	202
9/27/2023					
10:00 AM	137	68	97	61	363



10:30 AM	151	72	106	75	402
11:00 AM	142	71	119	78	407
11:30 AM	141	65	120	80	404
12:00 PM	146	63	113	83	404
12:30 PM	140	70	116	87	410
1:00 PM	144	90	120	91	444
1:30 PM	139	89	119	89	431
2:00 PM	151	86	116	83	435
2:30 PM	133	72	108	75	385
3:00 PM	133	62	95	67	355
3:30 PM	133	48	91	59	331
4:00 PM	122	39	79	59	296
4:30 PM	103	43	83	38	266
5:00 PM	69	47	76	36	226
5:30 PM	45	46	70	33	191

Weekend parking utilization by time of day is presented in [Figure 6](#) for off-street parking, and [Figure 7](#) for on-street parking. Similar to weekday utilization, Section 4 has the highest weekend off-street parking utilization (59%) while Section 2 has the lowest (37%). Section 3 has the highest on-street parking utilization (89%) while Section 1 has the lowest (38%). Parking utilization steadily increases from 10:00am – 1:00pm at which it becomes relatively constant. Data was not collected after 2:00pm so it is unknown when utilization decreases.



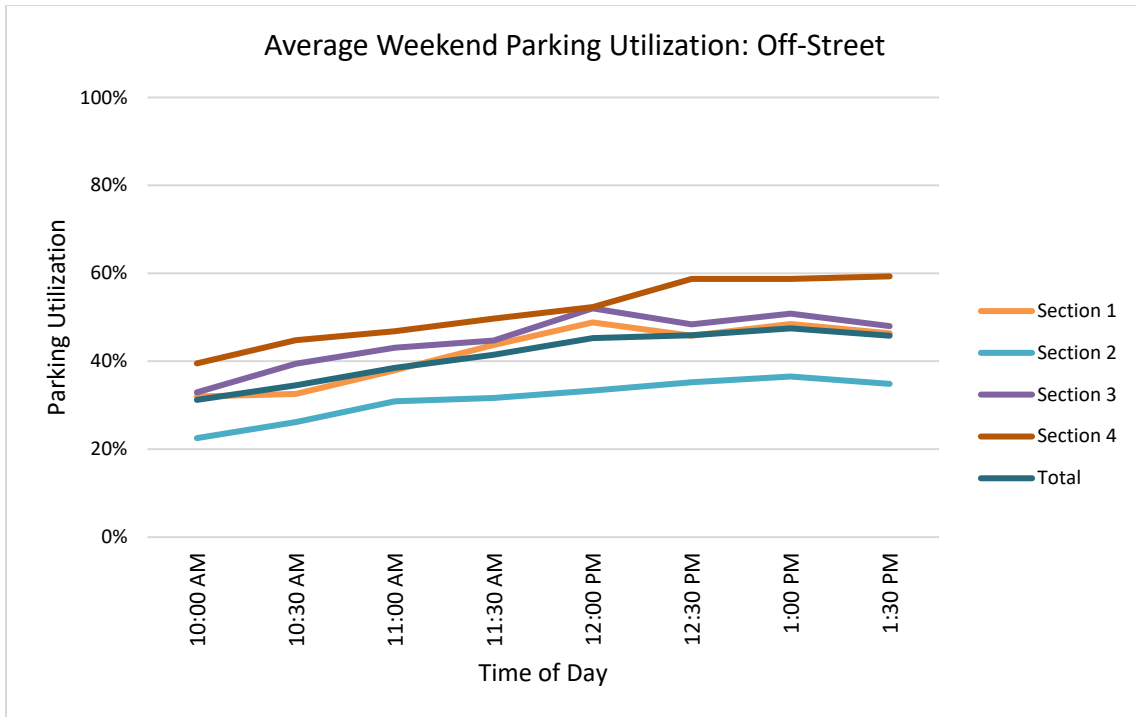


Figure 6: Average Weekend Off-Street Parking Utilization

Off-Street Parking Count						
	Section 1	Section 2	Section 3	Section 4	Total	
Time						
9/30/2023						
10:00 AM	71	43	36	49	202	
10:30 AM	72	61	44	58	238	
11:00 AM	81	73	52	60	266	
11:30 AM	95	77	53	64	290	
12:00 PM	107	83	63	77	331	
12:30 PM	104	86	57	88	337	
1:00 PM	108	81	55	96	341	
1:30 PM	100	74	56	90	321	
10/7/2023						
10:00 AM	96	76	45	87	310	
10:30 AM	98	77	53	96	328	
11:00 AM	117	90	54	101	365	
11:30 AM	133	90	57	107	390	
12:00 PM	148	93	65	103	411	
12:30 PM	135	100	62	114	416	
1:00 PM	145	112	70	106	438	
1:30 PM	142	110	62	114	430	



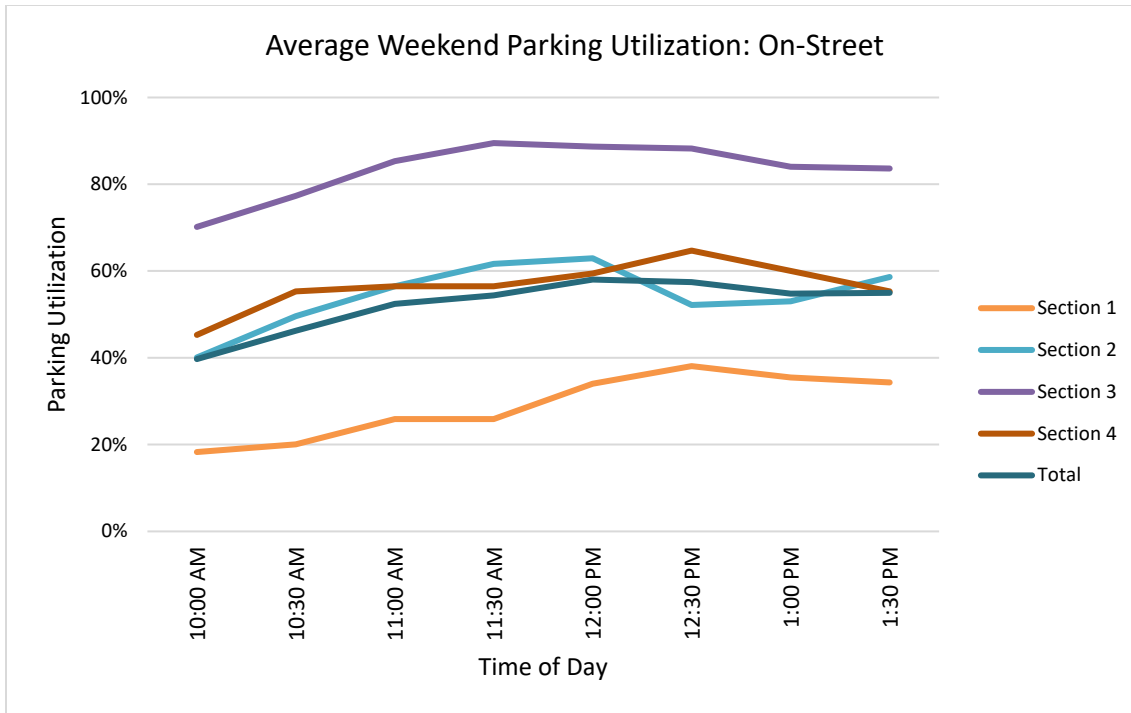


Figure 7: Average weekend On-Street Parking Utilization

On-Street Parking Count					
	Section 1	Section 2	Section 3	Section 4	Total
Time					
9/30/2023					
10:00 AM	22	33	79	25	154
10:30 AM	24	41	94	32	188
11:00 AM	44	48	101	35	227
11:30 AM	43	53	105	33	231
12:00 PM	47	53	100	37	235
12:30 PM	59	34	106	47	243
1:00 PM	52	37	96	45	226
1:30 PM	49	45	103	43	237
10/7/2023					
10:00 AM	41	60	88	52	237
10:30 AM	45	74	90	62	267
11:00 AM	45	83	102	61	289
11:30 AM	46	90	108	63	304
12:00 PM	70	93	111	64	336
12:30 PM	72	87	104	63	322
1:00 PM	70	86	104	57	313
1:30 PM	69	91	96	51	304



5.3. SITE-SPECIFIC OBSERVATIONS

Analysis of the 550 5th Street Frontage

The 550 5th Street frontage is shown in **Figure 8** where vehicle parking is currently permitted without restrictions. The 550 5th Street frontage has a new protected bike lane between the sidewalk and the parking lane. There are approximately 16 parallel parking spaces available on 5th Street between Harmston Avenue to Fitzgerald Avenue.



Figure 8: 550 5th St Frontage

On a weekday, parking utilization peaks between 60 – 80% between 10:00am and 2:00pm. On an average weekend, utilization reaches a maximum of 60% around 10:30am and then drops off for the remainder of the day. **Figure 9** provides a time series distribution of the parking utilization along the entire 550 5th Street frontage corridor.

Looking specifically at the immediate frontage that serves 550 5th Street, approximately room for 10 vehicles, there were 33 unique parking movements between 10am and 5:30pm on September 21 (weekday). The average stay was approximately 1.5hrs with the maximum stay of 3.5hrs observed by two vehicles. Most vehicles (9) parked for half an hour or less, eight vehicles parked for an hour, and four vehicles parked for 3hrs or more.

Based on the observed current behaviour, implementing a time restriction on this section of 5th Street would formalize most pre-existing behaviours, and impact only a minority of current users.

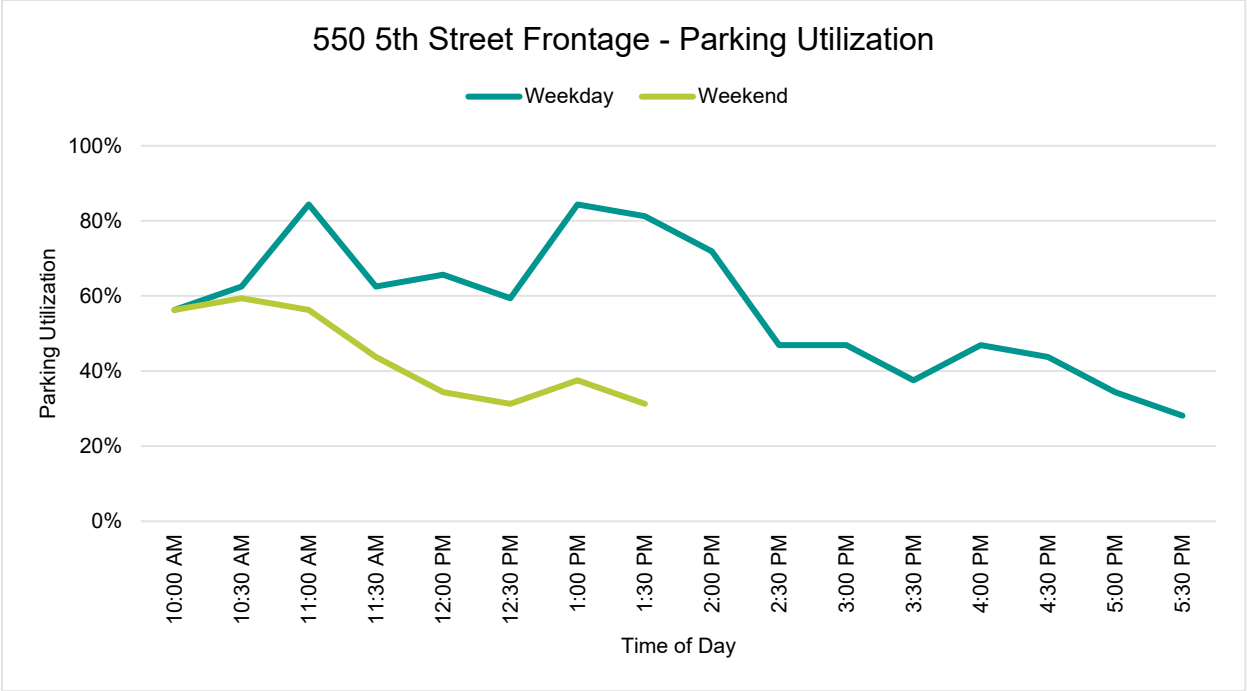


Figure 9: 550 5th St Parking Utilization (between Harmston Avenue and Fitzgerald Ave)

Analysis of the 445 10th Street Frontage

The frontage of 445 10th Street is shown in **Figure 10** which currently has four parallel parking spots available.



Figure 10: 445 10th Street frontage



The maximum parking occupancy between the two weekday and weekend days of parking occupancy collection was analyzed by time of day. **Figure 11** displays the maximum parking utilization of these four spots. There were only two periods on a weekday when parking utilization was at 100%. On a weekend, parking utilization was at 50% (two cars parked out of the four stalls available) at 10:00am, then decreased as the day progressed.

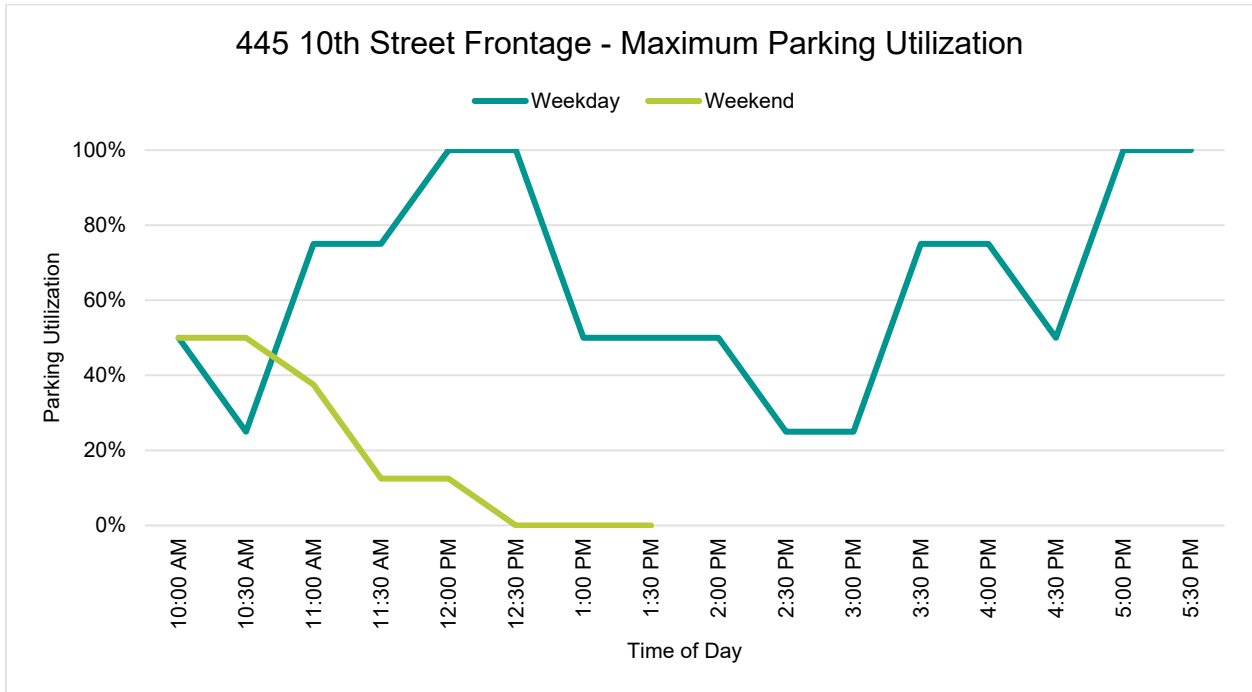


Figure 11: 445 10th St Parking Utilization

Further interrogation of the surveyed data indicates that turnover at the location is relatively high with vehicles stopping, on average for 1.1hrs and not more than 2hrs. In total, an average of 13 individual vehicles utilized the curb between the hours of 10am and 5:30pm, most of which parked for a 1hr duration.

Based on the observed current behaviour, implementing a time restriction on this section of 10th Street would formalize most pre-existing behaviours, and impact only a minority of current users.

Analysis of the 5th Street Corridor

5th Street is spine corridor of downtown Courtenay with a large number of shops and amenities that draw significant activity. The angled parking between Fitzgerald Avenue and England Avenue is known to be an area of high activity and so a specific review of this area was conducted for a typical weekday.

During a typical weekday (10am to 5:30pm) there are an average of 142 individual parking movements across the 24 parking spaces on the north side of 5th Street between Fitzgerald Avenue and England Avenue with an average of 6 different vehicles occupying each space. The average time spent parked is 1 hour and the maximum observed stay was 4.5 hours. The majority of vehicles park for half an hour or



less (89 out of 142 vehicles) with 30 vehicles parking for an hour, 11 parking for 1.5 hours, and very few parking for longer than that. The accessible parking spot at the England Avenue end of the curb was occupied for 1 hour in total, by two different vehicles.



Figure 12: 5th Street Angled Parking

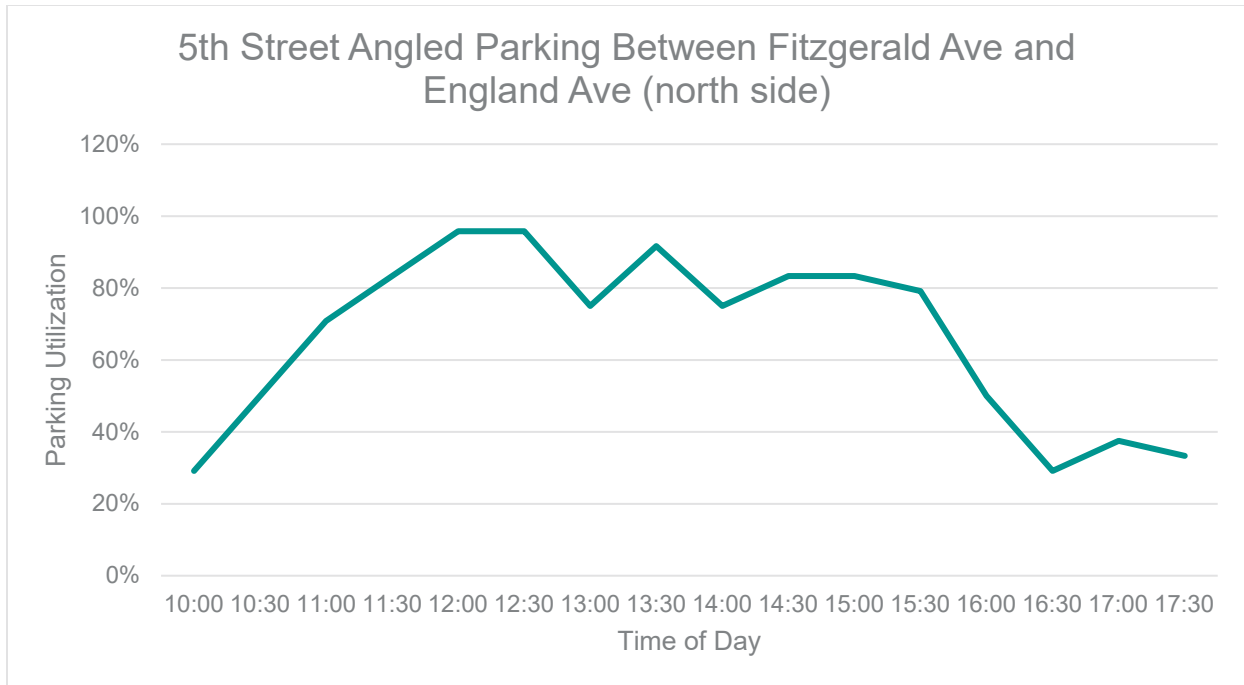


Figure 13: 5th Street Between Fitzgerald Ave and England Ave Parking Utilization

Closer to Cliffe Avenue, 5th Street is served by parallel curb parking, with fewer available parking spots for visitors and customers to park. The 2hr parking restriction is intended to protect this area for short-term stays and provide an opportunity for proximate parking to key retail destinations within the downtown.

An analysis of the weekend parking data indicates that, for the 9 available parking spots on the north side of 5th Street between Duncan Ave and Cliffe Ave, there are an average of 49 individual parking movements within the surveyed time period (10am to 5:30pm). Utilization is high, with never less than 6 spots occupied (and this includes occasions where large vehicles – or bad parking – remove the availability of one or more spaces), and 8-9 spots occupied between 12:30 and 3pm. On average, vehicles park for around 1.2hrs with most vehicles (22 of 49) staying for 30 minutes or less. 16 vehicles stay for an hour or less and longer stays are infrequent. However, it should be noted that one vehicle was observed parking for 6.5hrs on both survey days, indicating that all-day parking is not uncommon within the downtown core area.

Analysis of the 6th Street Corridor

The 6th Street corridor is a major part of the downtown area with several popular restaurants as well as the Vancouver Island Regional Library, the Old Farm Market, and other key destinations. Parking analysis has been completed for 6th Street between Fitzgerald Avenue and the Anderton Avenue; the analysis area is shown in [Figure 14](#). The block between Fitzgerald Avenue and England Avenue is notable for the angled parking stalls that maximize parking supply in this area. Angled parking currently provides space for 17 stalls on the north side of 6th Street, with a gap for access to the off-street parking lot. This is around double what would be expected with parallel curb parking (approximately eight stalls) in the same



space. On the south side of the street are 23 angled parking spaces along a 75m frontage (this would likely accommodate 12 vehicles with parallel curb parking).

There are currently approximately 96 parking spots available on 6th Street from Fitzgerald Street to the Puntledge River. Parking utilization varies by block along 6th Street, with especially high utilization between Fitzgerald Avenue and England Avenue to access the adjacent businesses. **Figure 15** illustrates the parking utilization for the whole corridor by time of day for both a typical weekday and weekend. On a weekday, utilization is at 80% until 2:00pm where it decreases to about 40% by 6:00pm. On a weekend, utilization peaks around 12:30pm at around 60%.



Figure 14 - 6th St Corridor

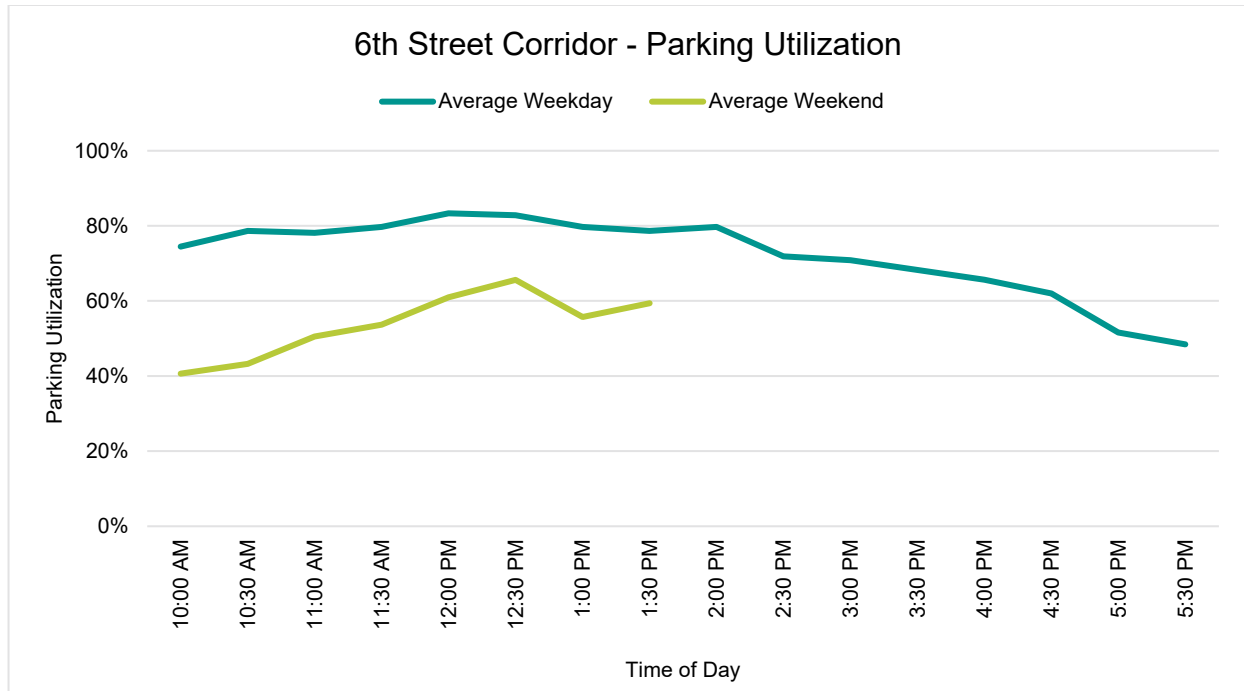


Figure 15: 6th Street Corridor Parking Utilization

Given that parking utilization is never observed to be much more than 80%, the loss of 20 spaces along the corridor (by converting angled parking stalls to parallel) could result in demand exceeding capacity during the busiest times, although the Section 1 utilization more generally is also around 80% and so there would appear to be sufficient capacity in the surrounding area to absorb additional demand when including both on- and off-street parking opportunities. 80% peak period utilization is typically optimal to allow drivers to find a parking spot and minimize parking search behaviour and driver frustration.

Analysis of the Old Orchard Area

Old Orchard is a residential neighbourhood to the north of the downtown area, generally comprised of 1st, 2nd, and 3rd Streets between Harmston Avenue and Cliffe Ave. Community feedback has indicated that there can be some overflow of parking into the residential area which has unrestricted parking access.

Looking specifically at on-street parking within the Old Orchard area, there are approximately 207 parking locations available along 2nd and 3rd Streets and the sections of Harmston Ave, Fitzgerald Ave, England Ave, Duncan Ave, and Cliffe Ave north of 4th Street. Parking utilization is quite consistent from 10am through to around 2pm where it begins to drop off, based on an average of the two weekday survey periods. No more than 137 individual vehicles were observed parking in the neighbourhood during one time period, with an average of around 109 vehicles. Many vehicles appear to stay for the long periods of time, potentially indicating that they have parked prior to the start of the business day by workers within the downtown area. This is somewhat borne out by the fact that weekend parking utilization is far lower with between 47-50 vehicles observed during any time period (less than half of the weekday volume).



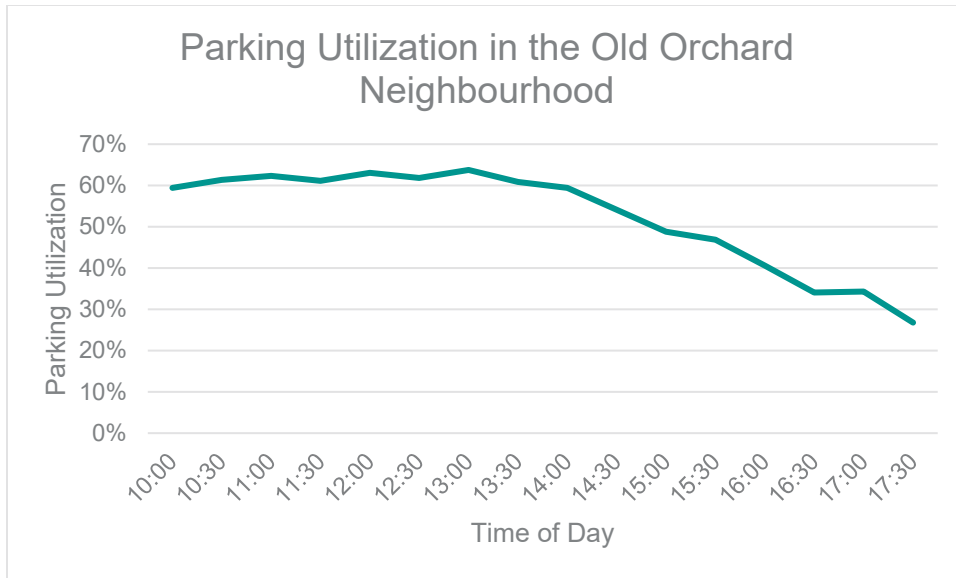


Figure 16: Parking Utilization across the Old Orchard neighbourhood (weekday)

It is interesting to note that the on-street parking north of 4th Street is busy by 10am, whereas the, time-restricted, parking on 4th Street and further into downtown, has significant additional capacity. This would seem to indicate that vehicles are parked early within the Old Orchard neighbourhood for long-stay parking.

Analysis of Off-Street Parking Lots

Shoppers Drug Mart

The Shopper’s Drug Mart, located on 8th Street between England Avenue and Cliffe Avenue is a frequently-visited pharmacy, gift shop, grocery, and general goods store which would be assumed to have a relatively high frequency of turnover across parking spaces. The image shown in [Figure 17](#) reports 67 vehicles occupying the lot at 9:30am, out of 107 total available spaces (62% occupancy) while at 1:30pm on the same day, there are 73 vehicles in the lot (68% occupancy), see [Figure 18](#). What is interesting is that by comparing the images we can see that 31 of the vehicles from the 9:30am images are still there at 1:30pm, indicating a stay of at least 4 hours indicating that almost half of vehicles parked in the lot are unlikely to be customers of the store and are more likely to be employees of this, or a neighbouring business. Most of these 31 vehicles are still in their original location by 3:30pm.



Figure 17 - Shopper's Drug Mart parking lot at 9:30am on Wednesday, September 27 2023



Figure 18 - Shopper's Drug Mart parking lot at 1:30pm on Wednesday, September 27 2023

Vancouver Island Regional Library – Courtenay

The public library has 76 parking spaces, including 4 accessible parking stalls and is accessed by a bi-directional circulation route that also provides through-access from 6th Street through to 8th Street.

On day 1 of the weekday survey there were 42 vehicles parked at 10am on a weekday with only 6 still in place by 2:30pm. However, on the second weekday survey date; of the 71 vehicles parked at 10am, 24 were still in place by 2:30pm, indicating that there may be some variability in how people use the library lot for all-day parking.

Weekend parking utilization is far lower (~25 vehicles parked in the morning peak, on average) but there are still between 6-8 vehicles parked for longer than 3hrs.



Figure 19: Vancouver Island Public Library parking lot

4th Street and Duncan Avenue Surface Parking Lot



The public parking lot at 4th Street and Duncan Avenue contains 47 parking spots. Based on weekday survey data, the utilization of the lot increases rapidly on the approach to midday with utilization at or above 80% between 11:30 and 3pm. Reviewing the individual orthoimages reveals that some vehicles do park for longer than 2 hours (3 vehicles on Sept 21, and 10 on Sept 27).

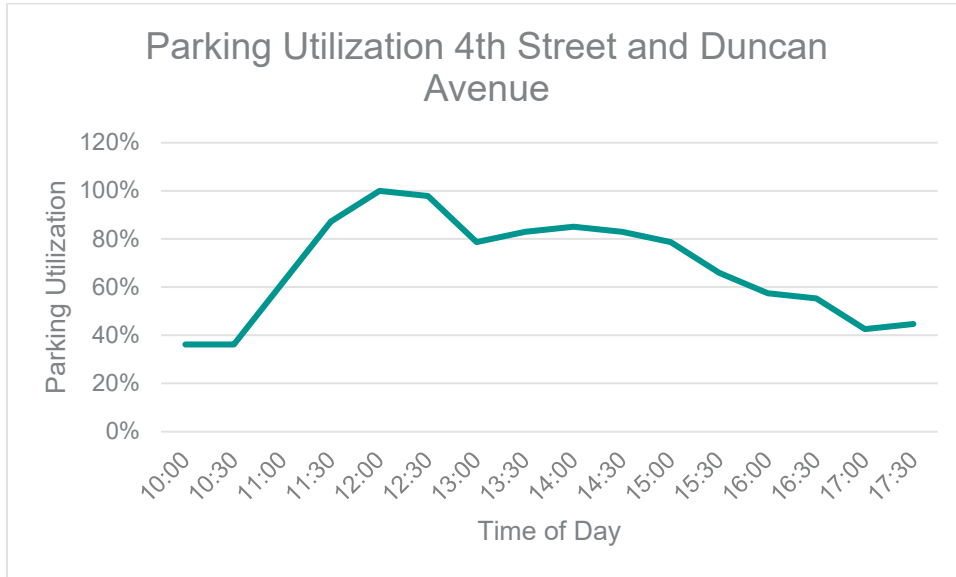


Figure 20: Public Parking Utilization at 4th Street and Duncan Avenue

4th Street and Fitzgerald Avenue Surface Parking Lot

The public parking lot at 4th Street near Fitzgerald Avenue contains 19 parking spots. Based on weekday survey data, the utilization of the lot remains fairly stable at around 60% until the mid-afternoon when it begins to drop to only 2 vehicles by 5:30pm. Reviewing the individual orthoimages reveals that between 6-12 vehicles (out of the maximum of 14 observed parking at one time) park for longer than 4 hours with turnover being low compared to surrounding on-street activities.



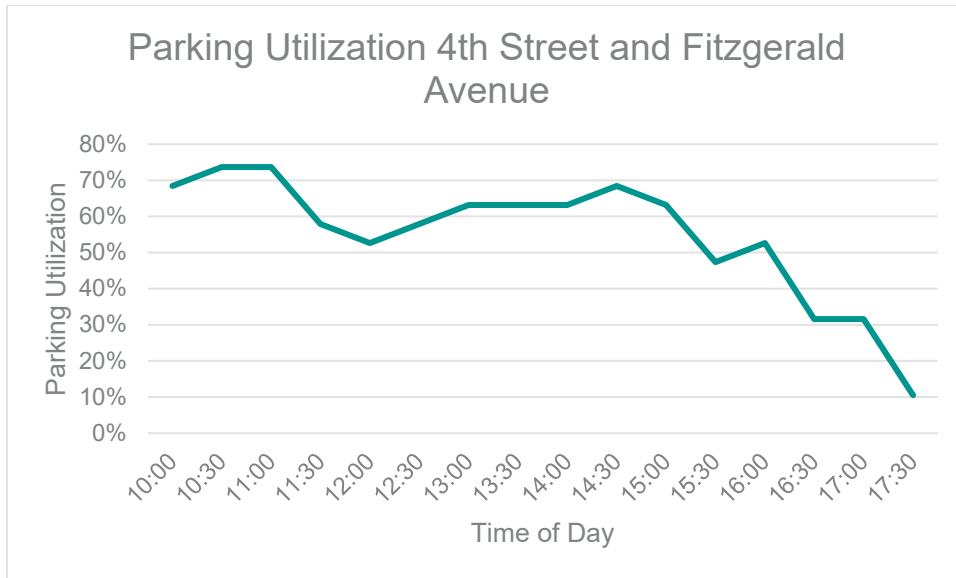


Figure 21: Public Parking Utilization at 4th Street and Fitzgerald Avenue

Upper/Lower Filberg Centre Surface Parking Lot

The lower Filberg parking lot directly serves the Filberg Centre and contains 40 parking stalls. Based on weekday survey data, the utilization of the lot peaks between 11:30am and 2:30pm and reaches maximum capacity around noon, with an average of 26 vehicles present throughout the day. 3-4 vehicles were observed to park for up to 4hrs in duration during weekday operations, however no vehicles parked for longer than this.

There are approximately 80 stalls in the upper parking lot, which also serves the Sid Williams Theatre and other local amenities. The lot is mostly underutilized with less than 50% occupancy until around noon when it increases to around 70% until mid-afternoon (2:30pm). An average of 33 vehicles park in the lot during a typical weekday. Between 10-14 vehicles stay for up to 4hrs on a typical weekday, with only 1 vehicle observed to stay longer than this during the two weekdays surveyed.



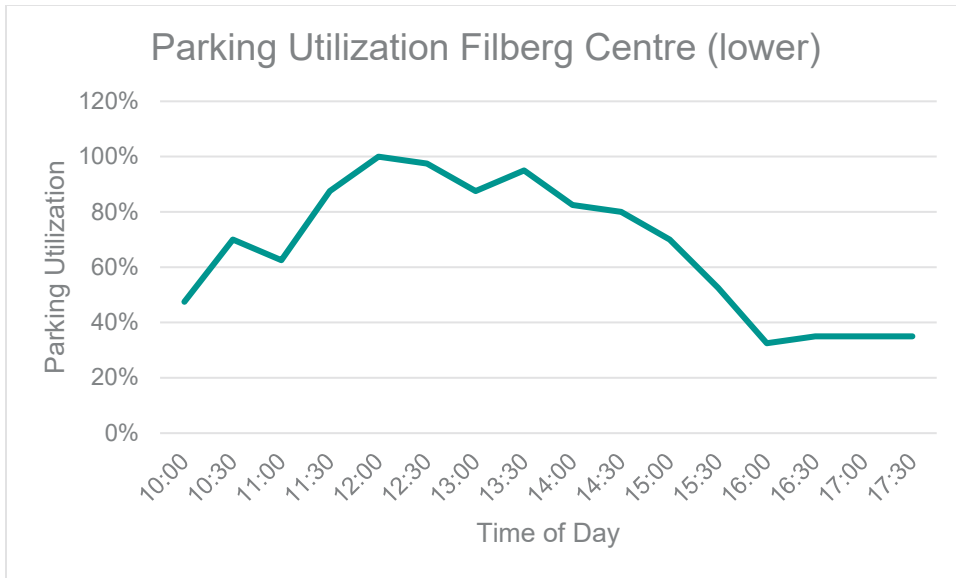


Figure 22: Public Parking at the Filberg Centre (lower)

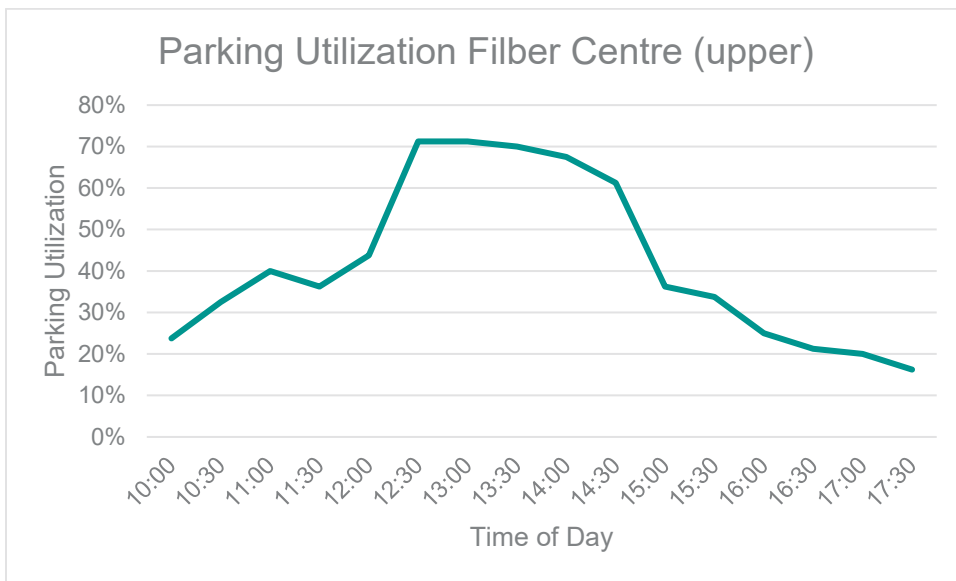


Figure 23: Parking Utilization at Filberg Centre (upper)

Sid Williams Theatre / Native Sons Hall Surface Parking Lot

The surface parking lot at the east end of 4th Street provides adjacent parking for the Sid Williams Theatre, Native Sons Hall, and amenities and commercial activities in the downtown area. There are 33 parking stalls provided. Utilization is quite consistent throughout the day, varying between 60-90% with an average of 24 vehicles parked.



During weekday operations, only 2 vehicles were observed parking for more than 4hrs with most vehicles parking for less than 2hrs, indicating that the lot is being used for relatively short trips/visits. The 2 vehicles identified as parking over 4hrs were there for the entire duration of both survey days, indicating they are regular, likely commuter, parking spots.

Weekend parking appears to be influenced by one-off events, as the first survey day saw 18 vehicles parking from 10am to 1:30pm, whereas the second survey day only saw 1 vehicle park for the whole duration.

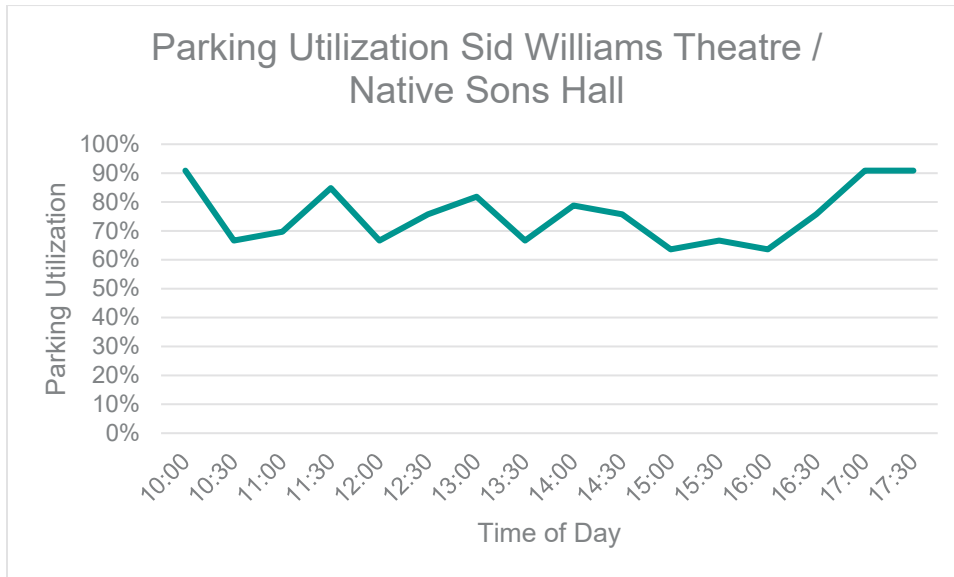


Figure 24: Parking Utilization at Sid Willams Theatre / Native Sons Hall Surface Parking Lot

5.4. GENERAL OBSERVATIONS

Some other general observations to note are:

- Parking utilization peaks mid-day, between 11:00am and 1:30pm.
- Off-street and on-street parking utilization are generally similar.
- Weekday utilization is on average higher than weekend utilization.
- Section 3 and 4 have the highest parking utilization, largely due to lower parking supply than the other sections and a high demand for accessing adjacent businesses.
- Bike parking was not specifically counted, but the data gathered indicates that an average of 7 individual bicycles make use of the 6th Street bike parking facility opposite the library on a typical weekday, with an average duration of stay of approximately 1hr.



6. Tool Development and Future Usage

6.1. WEB-BASED DASHBOARD

The current dashboard is hosted on McElhanney's server and is accessible only by a secure link to maintain security and privacy. Further study and analysis of the data can be undertaken upon request.

Examples of where the data sets collected as part of the drone survey can be interrogated further to provide valuable insights include:

- **Identifying non-conformance with parking restrictions.** Example: 5th Street is designated for 2-hour parking, however on Wednesday, September 27 between 9:30am and 3pm there were at least three vehicles that remained parking on-street for the entire 5.5-hour period.
- **Identifying abandoned vehicles.** Example: 6th Street at Harmston Avenue has 24-hour maximum parking restrictions, however there is an identified vehicle that did not move from the first survey on September 21 to the last on October 7.
- **Identifying the utilization of accessible parking.** Example: the south side accessible parking space at the end of the angled parking on 5th Street by Fitzgerald Avenue was utilized by at least 11 different vehicles across the four surveyed days.
- **Identifying the utilization of bike parking.** Example: the recently-installed bike parking opposite the Comox Valley Regional Library on 6th Street is visible in the drone photography and the utilization of this facility can be determined. Based on the data collected, at least 22 individual bicycles utilized the parking facility across the four survey days.

7. Conclusions and Next Steps

- The available on-street parking supply for the downtown area defined for the study, experiences slightly more than 80% occupancy between 11:30am and 2:30pm on a typical weekday. A generally similar profile is seen for off-street parking, although occupancy never exceeds 80%.
- Weekend parking remains below 50% occupancy for both on- and off-street parking.
- The 5th Street corridor experiences the highest consistent levels of utilization throughout the downtown whereas the areas south of 6th Street and along the Cliffe Avenue corridor are generally under-utilized.
- 5th Street experiences significant turnover, with the area between Fitzgerald Avenue and England Avenue (north side) turning over at an average rate of 0.75 vehicles per hour with 142 unique movements across the 24 spaces in that location.
- Weekday parking demand drops off substantially after 2pm for both on- and off-street parking.



- There are numerous instances across the downtown area where vehicles park in exceedance of the posted parking restriction, resulting in reduced overall supply for short-stay trips.
- Potential bike share solutions: many cities have found space for bike share companies to set up zones for parking and unlocking e-bikes and there are potential locations within the downtown area that would make sense. Typically, parking zones are located in high-activity or destination areas as these are the return-points for bikes in-use, as well as where people expect to be able to find a bike. Given the size and walkability of the downtown area, a single zone location may be sufficient in the downtown, with other zones located in areas such as the Driftwood Mall, Tin Town, North Island College, Vanier Secondary, and/or Comox Exhibition Grounds. Bike share systems have been found to be most effective where either extensive coverage is provided (i.e., dozens of smaller bike parking locations) or else where a limited number of strategic locations are provided. Smaller municipalities with successful bike share systems, such as Whistler, have around 10 locations for parking zones.
- Aside from Cumberland Road, there are a total of 137 angled parking stalls within the downtown area (51 on 4th Street, 46 on 5th Street, and 40 on 6th Street. Angled parking is particularly helpful where demand and turnover are high and there is sufficient right-of-way to allow for the safe reverse-out movement into the roadway. Typically, an angled parking space takes up around 3.2-3.5m of linear curb space (depending on the angle), whereas parallel parking requires around 6.5m of linear curb per space. The current angled parking supply occupies around 480m of curb length, which, if converted to parallel parking would provide around 73 parking spaces (a reduction of some 63 stalls overall). Given the significant loss in parking supply, this conversation should only be considered where:
 - Parking demand and/or turnover is low
 - Sidewalk expansion would yield significant benefits for the community
 - Alternative parking supply is available

Some challenges associated with angled parking include:

- Frequent overhanging of vehicles into the sidewalk (typically longer vehicles where the driver is concerned about the rear of the car obstructing the roadway)
- Poor visibility for reversing movement – low speeds on streets with angled parking is helpful, and the frequent stop signs within the downtown area achieve this well
- Based on the analysis, timed parking restrictions at the 550 5th Street and 445 10th Street frontages would be appropriate measures to continue to manage parking supply and demand throughout the downtown area
- Enforcement of compliance with parking restrictions should be focused in problem areas



Appendix A

Study Methodology

Study Dates, Timing, and Conditions

The drone captures were conducted on the following dates:

- Thursday, September 21 – 9:30am – 18:00pm [weather conditions: sunny]
- Wednesday, September 27 – 9:30am – 18:00pm [weather conditions: cloudy with patches of rain]
- Saturday, September 30 – 10:30am – 13:30pm [weather conditions: mainly sunny]
- Saturday, October 10 – 10:30am – 13:30pm [weather conditions: mainly sunny]

On each date there were no observed special events or other disruptions outside of normal City operations and the weather was a mix of scattered clouds with some light precipitation. Based on the conditions, there is ample reason to assume that these dates and times reflect typical operations for the downtown area, particularly because the capture dates were after the summer holidays and reflect “back to school” and more typical commuting conditions.

The study gathered data utilizing the DJI M300 RPAS (Remotely Piloted Aircraft System), equipped with a Zemmuse P1 camera featuring a 35mm focal length lens. As a safety precaution, an AVSS parachute system was affixed to the drone in case of emergencies. The use of this drone requires an Advanced Pilot’s Licence, regulated by Transport Canada. The advanced licence allows the operator to fly a drone over bystanders at a specific altitude in an urban / semi-urban setting. Proper communication between Transport Canada, the City, and local RCMP was required prior to starting the flight



Figure 25: DJI M300 Drone

missions to allow for clear response to members of the public who may report the drone activities. For each capture mission, the drone operator was located at 6th Street and Harmston Avenue, away from active vehicular movements. No issues were reported with the drone missions on any of the capture days, and all flights were completed as planned; this included the capture on the rainy day (September 27) where clear images were still captured due to the protective housing around the camera lens.

Data Processing

Each data capture mission constituted approximately 1,175 individual images (see [Figure 26](#)) which were then stitched together to form a composite mosaic of the study area for that 30-minute period utilizing the Site Scan application within the ArcGIS software platform.

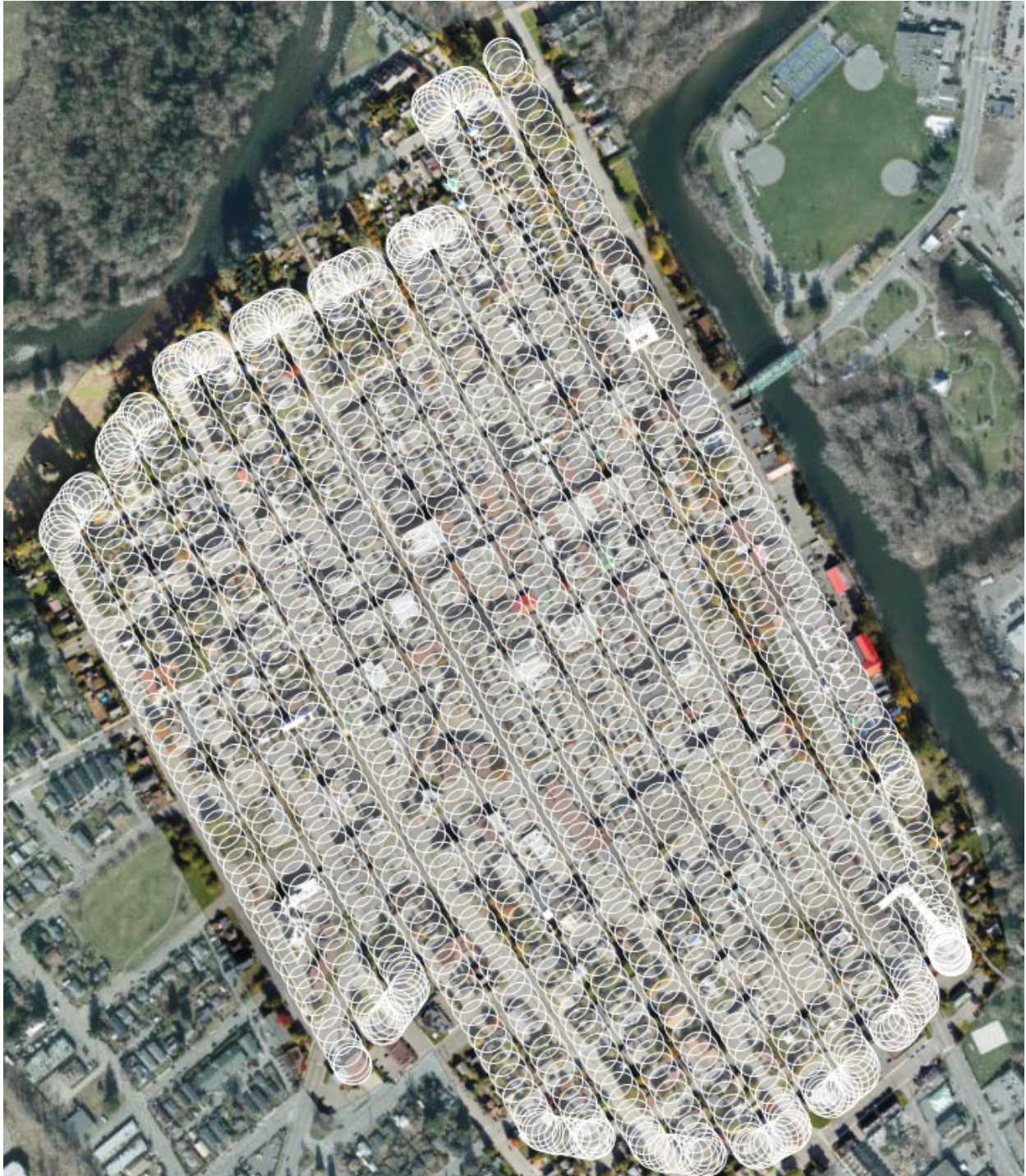


Figure 26 - Drone Mission Image Capture

An example of the imagery composite is provided in **Figure 27** below, with the boundaries of the study area clearly visible via the higher quality imagery covering the downtown area.





Figure 27 - Composite image of the 12:00pm drone flight for October 7, 2023

The high-resolution images provided through the drone photography allow for close-up inspection of parking occupancy and other street-level details for the study period under investigation in a way that was previously very difficult, if not impossible to derive. An example is provided in [Figure 28](#) for the parking lot at the intersection at 4th Street and Duncan Avenue where the lot occupancy at 12:00pm on Saturday, October 7 is observed to be full with 45 out of 46 spaces occupied and one (facing 4th Street, third from the left) either recently occupied or about to be occupied. Also notable is that one vehicle (a silver-grey truck) is occupying an area in the lower left of the image that is not technically a parking space.



Additionally, as the drone photographs include the on-street conditions, the use of the roadway is also visible, indicating the busy operations on the westbound lane of 4th Street west of Duncan Avenue.



Figure 28 - Parking lot at 4th Street and Duncan Avenue on Saturday, October 7 2023

With the data set available from the drone survey of the downtown core, it is possible to investigate period-by-period changes in parking space occupancy to gain insights on frequency of turnover and other operational metrics.

Data Dashboard

Following the processing of the individual images into the usable mosaics that allow for the initial scan and review summarized in Section 4.5, the data files were imported into McElhanney's GIS Portal for further analysis.

Following the import to the GIS portal, a series of analytical tools were developed to provide users with the ability to interrogate and export findings. Primarily, the main feature of the GIS software is the ability to run vehicle identification software that locates vehicles within the image and automatically counts them. Along with this core functionality, the following tools were also developed:



- **Layers** - This allows the user to show or hide ‘detected cars’, ‘roads’, ‘building footprints’, and drone imagery’ depending on what the user would like to see.
- **Parking filter** - This allows the user to differentiate between vehicles that are being driven on the road and vehicles parked in either on-street or off-street parking lots.
- **Date filter** - This allows the user to select the date and time period to review, based on the surveyed data (which can be added to over time).
- **Select** - This function allows the user to create a shape within the image and only count cars within this area. An example would be to draw a polygon around an off-street parking lot to automatically count the number of cars within the lot.
- **Export** - This allows the user to select a time period, location and/or filter and export the results across a range of times or dates to a .csv file which can then be opened in Microsoft Excel for further analysis and reporting.

An example screenshot of the dashboard is provided in **Figure 29** where the number of on-street vehicles at 10:00am on Thursday, September 21, 2023 is derived (717 across the entire study area). A toggle of the Parking Filter button indicates that 1,420 vehicles are parked off-street at the same time, showing the relative parking utilization for both forms of parking.

The value of this dashboard is that it allows for bespoke, detailed analysis of specific areas that are of interest to the City. For example, **Figure 30** shows the on street parking demand on 6th Street between Fitzgerald Avenue and Cliffe Avenue for Wednesday, September 27, 2023 (47 vehicles), and this can be extracted for all time periods on this date to assess the fluctuation in demand over time for this corridor.

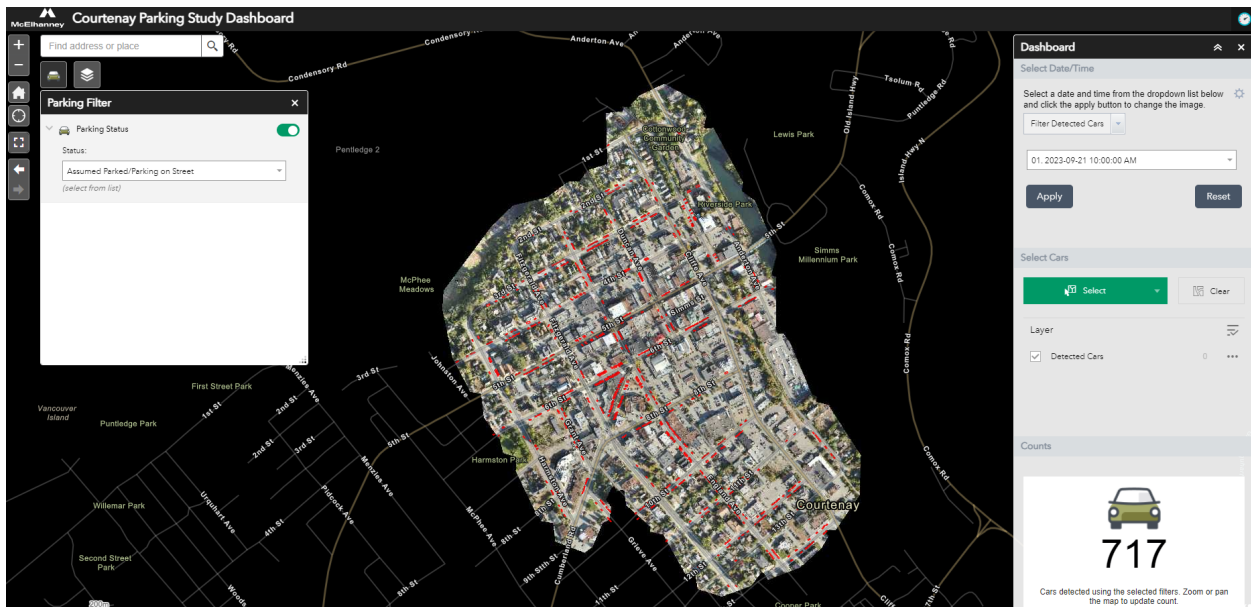


Figure 29 - Example Screenshot of the GIS Dashboard



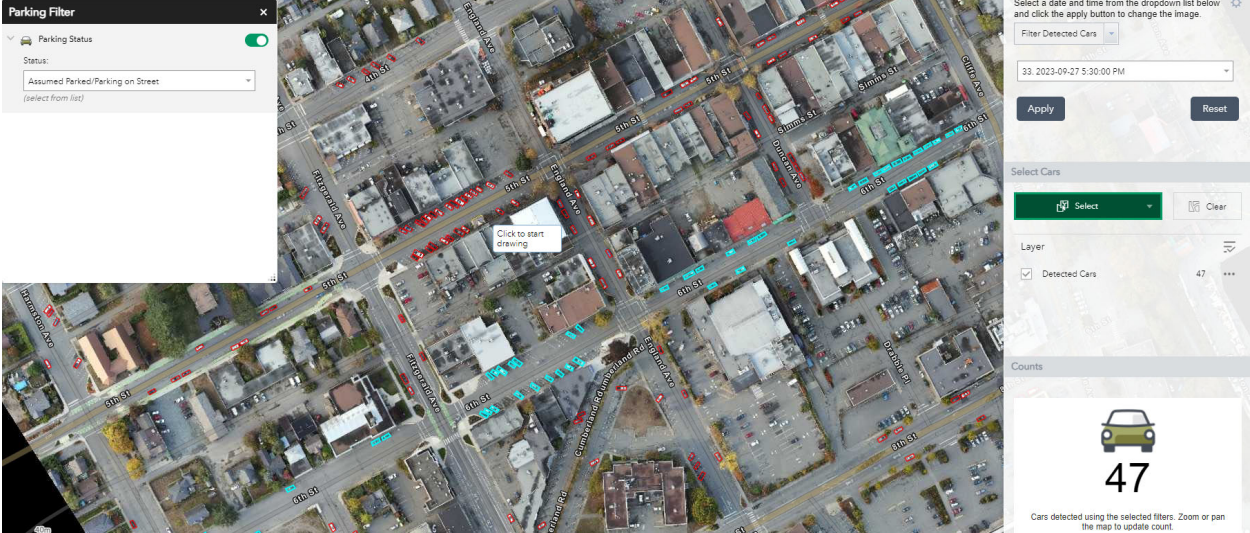


Figure 30 - Example of Isolating On-Street Parking on 6th Street Only



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2017 Parking Study

In 2016 the City of Courtenay requested McElhanney to conduct a parking review within a defined commercial centre of the downtown core, as illustrated by Figure 1. This review was intended to summarize the relationship between private and public parking, the time of peak utilisation, and identify the relative areas of highest occupancy.

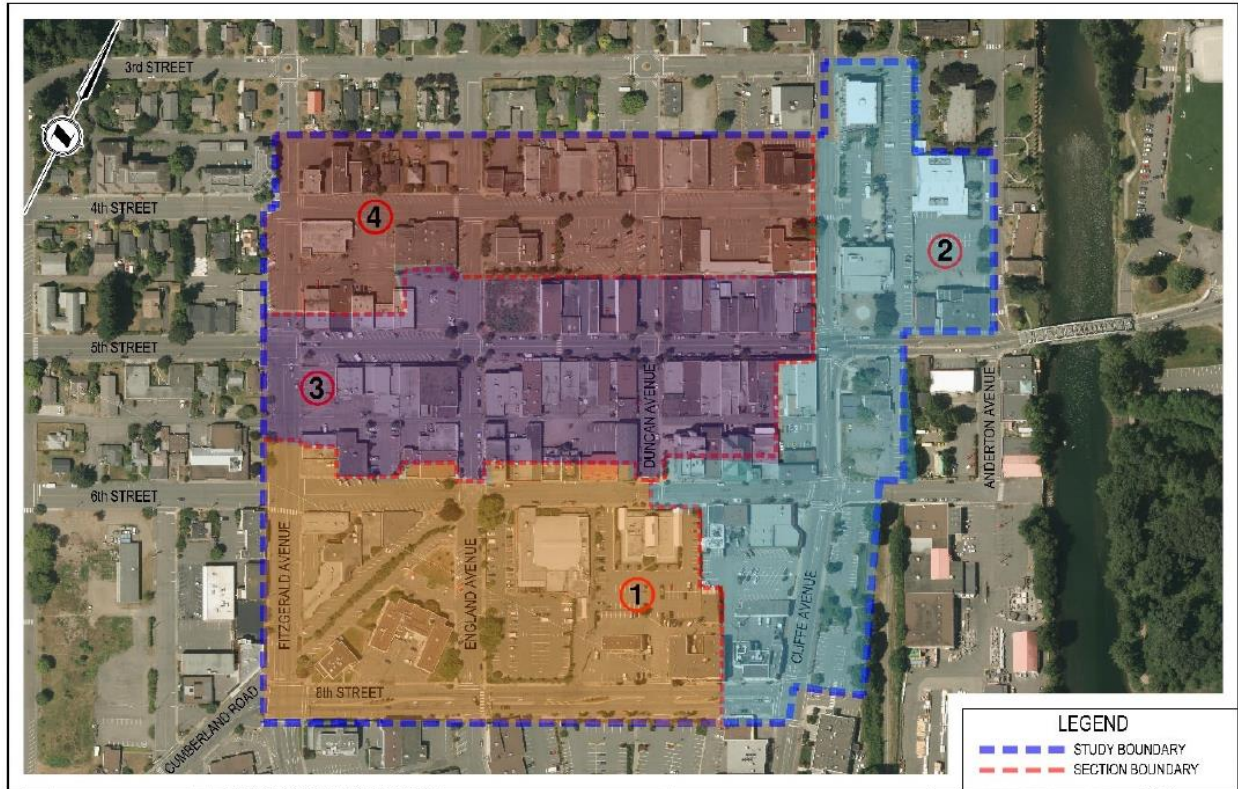


Figure 1: The Downtown Core Divided into 4 Sections

Public Parking Data Highlights, 2017

PUBLIC	Section 1	Section 2	Section 3	Section 4	Total
Number of Stalls	300	299	149	141	889
Weekday					
Peak Occupancy %	91%	69%	84%	75%	77%
Peak Time	11:00am	12:30pm	12:30pm 1:30pm	1:30pm	12:30pm
Weekend					
Peak Occupancy %	57%	70%	91%	99%	73%
Peak Time	1:30pm	1:00pm	1:00pm	1:30pm	1:30pm

Private Parking Data Highlights, 2017

PRIVATE	Section 1	Section 2	Section 3	Section 4	Total
Number of Stalls	154	71	45	104	374
Weekday					
Peak Occupancy %	68%	51%	58%	62%	58%
Peak Time	10:00am 11:30am	12:00pm	12:00pm	1:30pm	12:00pm
Weekend					
Peak Occupancy %	42%	34%	44%	61%	45%
Peak Time	1:30pm	12:00pm	1:30pm	1:30pm	1:30pm

Results

Public Parking

1. Peak parking occurred at noon throughout the core,
2. Section 1 (6th and 8th Street) had the highest occupancy (91%) on the weekday,
3. Section 3 (5th Street) had the highest occupancy (91%) on the weekend,
4. Occupancy was higher on the weekend for sections 3 and 4 (91% and 99% respectively)

Private Parking

1. Peak occupancy was significantly lower (51% - 68%), but peaked at similar times, and
2. Occupancy was lower in all sections on the weekend

Overall

1. The relative pattern of location and time of highest occupancy is the same for public and private parking areas.

2023 Parking Study

McElhanney was retained to update the 2017 parking assessment with the same objectives, however, the study area was expanded, and the methodology updated with new technology to allow for future analysis. This new methodology would also allow for insight into parking behaviours.

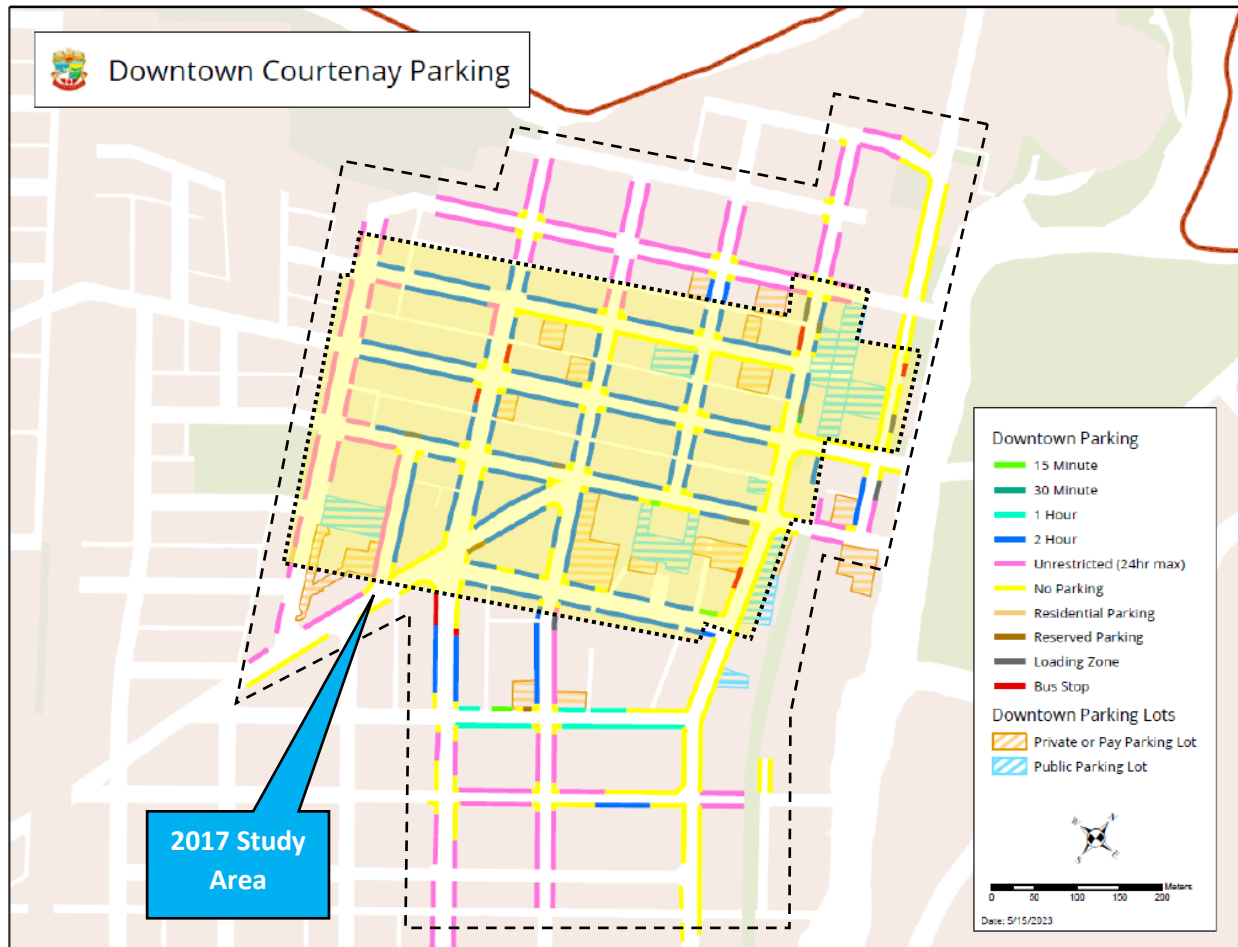


Figure 2: 2023 Expanded Parking Study Area

While the area had expanded, a comparison between the new and previous data was still possible by conforming the data and area to the same as 2017 with the four sections.

Public Parking Data Highlights, 2023

PUBLIC	Section 1	Section 2	Section 3	Section 4	Total
Number of Stalls	300	299	149	141	889
Weekday					
Peak Occupancy %	85%	80%	97%	95%	--
Peak Time	10:30am	1:00pm	11:30pm 1:00pm	1:00pm	--
Weekend					
Peak Occupancy %	38%	~62%	89%	~64%	--
Peak Time	12:30pm	12:00pm	12:00pm	12:00pm	--

~ interpolated from graph

Private Parking Data Highlights, 2023

PRIVATE	Section 1	Section 2	Section 3	Section 4	Total
Number of Stalls	154	71	45	104	374
Weekday					
Peak Occupancy %	85%	55%	93%	88%	--
Peak Time	12:30pm	11:00am	1:00pm	12:00pm 1:00pm	--
Weekend					
Peak Occupancy %	~49%	37%	~51%	59%	--
Peak Time	12:00pm	1:00pm	1:00pm	12:30pm 1:30pm	--

~ interpolated from graph

Results with Comparison to 2017 study

Public Parking

1. Section 3 and 4 (5th and 4th Street) had the highest occupancy (97% and 95% respectively) on the weekday, [new – was Section 1 in 2017]
2. Section 2 weekday occupancy jumps to from 50% to 80% during 1:00pm - 2:00pm [new – was steady and low in 2017]
3. Weekend occupancy peaks at 12:00pm [new – one hour sooner]

Private Parking

1. Section3 (5th Street) had the highest occupancy (93%) on the weekday, [new – was section 1 in 2017]
2. Peak occupancy now the same as public parking [new – was substantially lower in 2017]

Site Specific Questions

550 5th Street Frontage



Question 1: Are the timed parking restrictions at 550 5th Street appropriate?

Of the 16 available parallel spaces, occupancy reached between 60-80% on a weekday, and 60% on a weekend.

Answer: Implementation of a time restriction, while not required, would formalise the existing parking behaviour.

445 10th Street Frontage



Question 2: Are the timed parking restrictions at 445 10th Street appropriate?

Of the 4 available parallel spaces, occupancy reached 100% twice (12:00pm and 5:30pm) on a weekday, and 50% on a weekend.

Answer: Implementation of a time restriction that better suits the needs of the current occupant is recommended. The existing 15minute restriction was previously set up for the bakery that no longer exists. This change can be met through the Operational Services Department as part of their core services.

5th Street Corridor

Angled Parking (Fitzgerald Ave to England Ave) North Side Only - Weekday



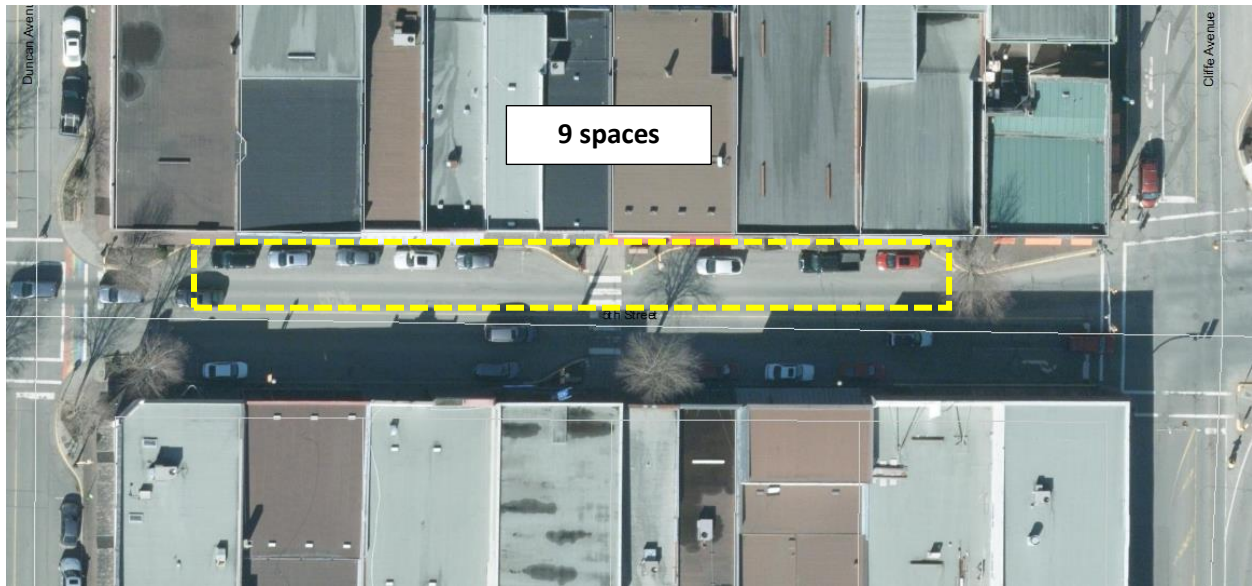
Question 3: What is parking like on the 5th Street corridor, between Fitzgerald Avenue and England Avenue during a weekday?

This area is within section 3, which had the highest occupancy (97%) for 2023. Being densely populated with small commercial shops it has 24 angled parking spaces with a two-hour time restriction and one accessible parking space. During the weekday, 142 parking movements occurred with the majority of vehicles (89 of 142) parking for 30 minutes or less.

The single accessible parking spot was occupied by two vehicles for a total of one hour.

Answer: This parking behaviour is considered healthy and vibrant with significant spare capacity for accessible vehicles.

Parallel Parking (Duncan Ave to Cliffe Ave) – North Side Only – Weekend



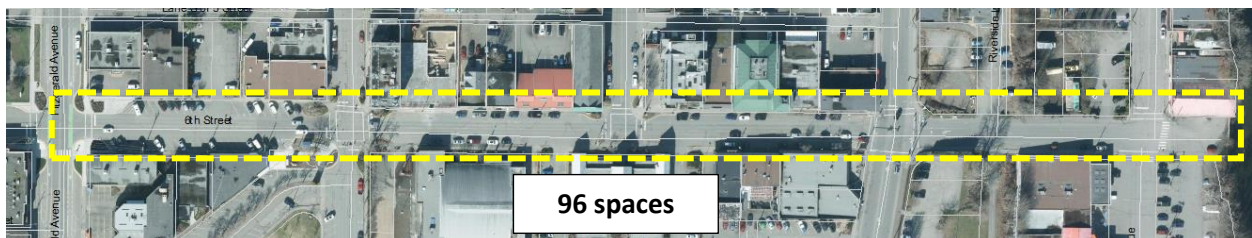
Question 4: What is parking like on the 5th Street corridor, between Duncan Avenue and Cliffe Avenue during a weekend?

5th Street east of Duncan Avenue has parallel parking. The nine parking spaces monitored had a total of 49 parking movements. The occupancy was never below six vehicles (67%) and most vehicles (22 of 49) stayed for 30 minutes or less.

One vehicle stayed for 6.5 hours which is an indication that not all drivers are obeying the time restriction.

Result: enforcement of the time restriction could assist compliance of driver behaviour; however, the vast majority of drivers are compliant, making the parking behaviour relatively healthy and vibrant.

6th Street Corridor



Question 5: What is the parking demand along 6th Street?

The angled parking area (one block north and south) has 17 parking spaces on the north side and 23 on the south, comprising 42% of the total parking (40 of 96). The remainder of 6th Street (three blocks north and south) has 56 parallel parking spaces.

For the entire corridor, weekday occupancy peaks at 12:00pm (~83%) and is steady between 10:00am and 2:00pm, whereas on a weekend the peak occupancy is at 12:30pm (~67%).

Answer: This street is considered at capacity. Occupancy greater than 80% generally indicates de facto capacity, as spaces become available, it takes time for other vehicles to spot it, and sometimes circle the block in order to park in it.

Question 6: Can the angled parking be converted to parallel parking given the current demand?

Answer: Conversion of angled parking to parallel parking would need to be reviewed in the context of the ability of the surrounding area to shoulder an increase in parking demand, as parallel parking will remove some parking capacity. The trade-off would make 6th Street more viable for Active Transportation modes, which would enhance the corridor's attraction for multiple modes of travel, especially given the 6th Street Active Transportation Bridge project that recently was given the green light to proceed.

There are significant sections of parallel parking that may be able to be converted to angle parking on 5th Street, for example. An assessment of this area to determine the feasibility would confirm this possibility for additional capacity in the future.

Old Orchard Area



Question 7: Is there overflow parking from the commercial core into this residential neighbourhood?

This area comprises 207 parking spaces and has a relatively consistent weekday occupancy starting at or before 10:00am until 2:00pm. The peak occupancy (137 of 207) was at 1:00pm (66%).

After 2:00pm, the weekday occupancy drops to approximately 28%, at the end of the work day (5:30pm). An assumption could be made that parking related to the commercial centre is occurring prior to 10:00am when the analysis started, and they gradually left the area after 2:00pm.

Weekend occupancy (50 of 207) was found to be less than half (24%) of that of the weekday at all times. This may indicate the number of residential related parking on the streets.

Answer: Almost half of the parked vehicles could be attributed to longer term external parking; typical of employee parking. 42% of the parking within this area could be attributed to the commercial centre ([137-50]87 of 207) while 58% could be attributed to the residents (50 of 207). Feedback from the neighbourhood could help assess if this is the case.

Overall Questions:

These questions are directed at the expanded parking area of the 2023 assessment.

Question 8: Is the overall parking supply sufficient and what is the turnover rate?

Public Parking occupancy is 71% at 1pm, which is considered near capacity, however there are several streets surrounding the downtown core that have occupancy rates of 56% or less. This may indicate that these streets are considered too far for many of the drivers to walk to their destination.

Although the overall turnover rate was not given in the report, all indications from the downtown parking areas suggest that turnover is quick and most vehicles that are parked in time restricted areas (2hr parking limit) do so for 30 minutes.

Private Parking occupancy is 76% at 1pm, which is considered near capacity, however, five areas with private lots had occupancy rates of 50% or less. This may suggest that private lots are catering to those customers only, or the private lots are not well known to the general public.

Although an overall turnover rate was not given, the indications from several of the private lots show longer stays than public street parking, which may indicate employee parking. Management strategies in private lots could constrain employee parking to a specific area of the lot, or require employees to park elsewhere during times of heavy demand like Christmas. Understanding how each private lot operates would require interviews with each lot owner.

Parking Structure

Although the core area of downtown, generally identified as the 2017 study limits, are considered at capacity for public and private parking, there is a substantial amount of parking surrounding this area to supply the existing demand that is within walking distance to the core.

It may not be used because the turnover rate in the core is high, so drivers seeking a parking space can generally find one in the area they want, with some wait time. This may translate into a significant amount of circulating traffic seeking parking spots as the occupancy reaches the peak period (noon to 1pm).

A parking structure is not required at present, but should be considered when the surrounding streets and parking lots around the core reach near capacity. A trigger for this consideration could be 80% of all public and private parking spaces are occupied in the 2023 assessment area that are outside of the 2017 study area (ie. All the surrounding parking that has a low occupancy rate at present).

A parking structure could decrease the circulating traffic significantly, based on several factors, like:

- if the occupancy of the structure is available to the public,
- if the cost for parking was priced appropriately,
- if the location was within the demand area, and
- if the structure was clean, vibrant and had ‘eyes on’ from several perspectives for a feeling of safety.

Question 9: Is it possible to provide insight into the City’s current bike parking supply?

Answer: Yes. Although not explored in the overall study area within the report, the web-based tool created by McElhanney has the ability to analyse this. One example given states the Vancouver Island Regional Library - Courtenay bike parking had 22 bicycles use the facility across the four survey days.

Another example shows the bike facility outside the Art gallery was used by seven bicycles on a typical weekday with an average stay of one hour. City staff have access to this tool for ongoing assessments, pending access to the tool.

Question 10: How well-used are the accessible parking spots?

Answer: Although not explored for the overall area of the 2023 report, this can be analysed with the web-based tool. One example given is the south side accessible parking on 5th Street by Fitzgerald Avenue. It was used by at least 11 vehicles across the four survey days.

Another example indicated the north side accessible parking space on 5th Street at England Avenue showed two vehicles used it on a typical weekday. These examples seem to indicate a significant spare capacity that can be confirmed through an area-wide assessment by City staff, pending access to the tool.

Private Parking Assessments

A summary of the key findings from the Assessment of private parking lots is given below.

A. Shopper’s Drug Mart – Weekday (107 spots)

This lot has 62% occupancy at 9:30am which remains for most of the day, showing approximately 50% of the parking is long-term, assumed to be employee parking.

B. Vancouver Island Regional Library – Courtenay (76 spots)

The parking was found to be quite variable on the two weekday study dates, ranging from 55% occupancy to 93% occupancy at 10:00am. In each case the parking demand diminished significantly by 2:30pm.

Weekend occupancy peaked at 32%.

C. 4th Street/Duncan Ave Surface Parking Lot – Weekday (47 spots)

Occupancy rose significantly after 10:30am to 100%, which decreased only slightly by 3:00pm (80% occupancy). This parking lot is heavily used.

D. 4th Street/Fitzgerald Ave Surface Parking Lot – Weekday (19 spots)

This location shows a steady 60% occupancy until 2:30pm, and by 5:30pm is almost empty. Most vehicles are parked long term, indicating employee parking.

E. Lower Filberg Surface Parking Lot – Weekday (40 spots)

Serving the Filberg Centre, the peak occupancy at this location is 60% between 11:00am and 2:30pm.

F. Upper Filberg Surface Parking Lot – Weekday (80 spots)

Serving the Sid Williams Theatre and surrounding amenities, this parking lot peaks between 12:30pm to 2:30pm at 70% occupancy with a significantly lower occupancy before and after.

G. Sid Williams Theatre/Native sons Hall Surface Parking Lot – (30 spots)

Serving the theatre, the Native Sons Hall, and local amenities, the lot is well used with occupancy varying throughout the day between 60-90% the entire study time period.

Weekend use may be influenced by events as the parking lot varied between 18 and almost no vehicles.

T E C H N I C A L M E M O R A N D U M

	CITY OF COURTENAY Downtown Courtenay Parking Assessment – Summary of Findings
Date:	January 12, 2017
Our Reference:	2211-47416-17
City File:	

To: City of Courtenay
Attn: Tatsuyuki Setta, Sr. Planner

Prepared By: Bob Hudson, P.Eng.

Reviewed By: Chris Durupt, P.Eng.

The City of Courtenay has requested that McElhanney Consulting Services Ltd. (MCSL) undertake a series of parking counts within a defined downtown “core commercial area” within the City center. We understand the City desires to better understand the relationship between public and private parking utilization, timing of peak utilization, and relative areas of highest utilization. This information is required to better manage existing public parking space, and to inform City planning initiatives, including the Downtown Revitalization processes.

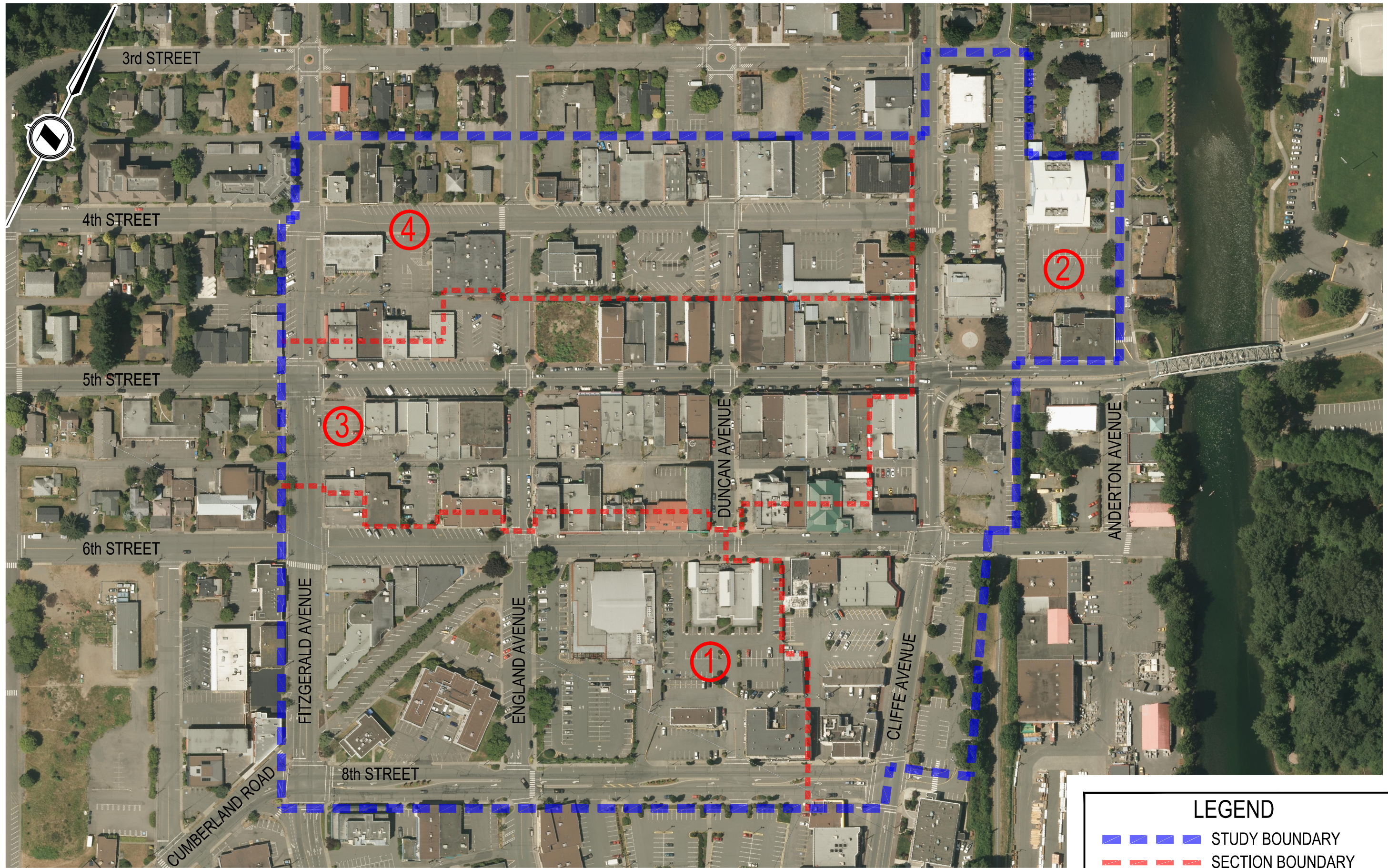
Provided herein is a brief summary of study methodology, data collected, and general observations.

1 STUDY AREA AND METHODOLOGY

Based on direction provided by City staff, the study area was defined as that area generally bound by:

- The west side of Fitzgerald Avenue to the west,
- Mid block (laneway) between 3rd Street and 4th Street to the north,
- The south side of 8th Street to the south, and,
- Mid block between Cliffe Avenue and Anderton Avenue, plus the Filberg centre, to the East.

The physical limits of study proposed were selected based on common perceptions as to what area constitutes “Downtown Courtenay”, and with consideration given to budget limitations. The attached **MCSL drawing CT-00** indicates the limits of the study area.



LEGEND

- - - - STUDY BOUNDARY
- - - - SECTION BOUNDARY

Rev	Date	Description	CDE	BH	DJ
0	2016-06-03	ISSUED FOR USE			

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0 1:2000 100

ORIGINAL DWG SIZE: A3 (297 x 420mm)

McElhanney
McElhanney Consulting Services Ltd.

455 Sixth Street
Courtenay BC
Canada V9N 6V4
Tel 250 338 5495

CITY OF COURTENAY
830 CLIFFE AVENUE, COURTENAY, BC, V9N 2J7

**DOWNTOWN COURTENAY PARKING STUDY
MASTER PLAN**

DOWNTOWN COURTENAY

Drawing No. **CT-00**

Project Number 2211-47416-17 Rev. 0

In order to consistently and accurately record parking usage, and develop meaningful datasets, the following process was used to document parking utilization:

- The study area was divided in four sectors that were suitably sized to allow a technician to cover the entire area, accurately counting the number of vehicles in each public parking lot, private parking lot, lane, roadside, etc., within 30 minutes.
- “Weekday” counts were carried out every half hour between 9:30am and 5:30pm. Wednesday was selected as the preferred weekday for counts, to most accurately reflect “average” parking demands.
- “Weekend” counts were carried out every half hour between 10:00am and 1:30pm on a Saturday.
- Count dates were selected after confirming that no significant local, or regional public functions (concerts, markets, parades, etc.) were planned for, any of which could influence (in a positive, or negative sense), the number of vehicles present within the study area.

2 STUDY TIMING, AND CONDITIONS

Parking counts were undertaken on Wednesday, June 8, 2016, and Saturday, June 11, 2016. Weather conditions were typical for this time of year, sunny with some cloud cover, with temperatures in the high teens, and low twenties. There were no unusual public events scheduled within the Comox Valley on either proposed count day. A summary of local events scheduled for June 8th and June 11th can be found at What’s On Digest at <http://comoxvalley.whatsondigest.com/events/calendar>.

3 SUMMARY OF DATA COLLECTED

Provided overleaf is a summary of all parking utilization data collected.

Date: Wednesday, June 8, 2016

Stalls In Use

Area CT-01 (Section #1)

Total No. of Stalls																			
Private	Public																		
154	300	454																	
		9:00 AM	9:30 AM	10:00 AM	10:30 AM	11:00 AM	11:30 AM	12:00 PM	12:30 PM	1:00 PM	1:30 PM	2:00 PM	2:30 PM	3:00 PM	3:30 PM	4:00 PM	4:30 PM	5:00 PM	5:30 PM
Number of Private Stalls		80	87	105	99	99	105	102	88	93	86	92	94	90	98	84	80	77	47
Private Parking Utilization		52%	56%	68%	64%	64%	68%	66%	57%	60%	56%	60%	61%	58%	64%	55%	52%	50%	31%
Number of Public Stalls		184	249	247	266	274	253	249	250	228	229	257	234	224	212	200	191	130	102
Public Parking Utilization		61%	83%	82%	89%	91%	84%	83%	83%	76%	76%	86%	78%	75%	71%	67%	64%	43%	34%
Total Number of Stalls		264	336	352	365	373	358	351	338	321	315	349	328	314	310	284	271	207	149
Overall Utilization		58%	74%	78%	80%	82%	79%	77%	74%	71%	69%	77%	72%	69%	68%	63%	60%	46%	33%

Area CT-02 (Section #2)

Total No. of Stalls																			
Private	Public																		
71	299	370																	
		9:00 AM	9:30 AM	10:00 AM	10:30 AM	11:00 AM	11:30 AM	12:00 PM	12:30 PM	1:00 PM	1:30 PM	2:00 PM	2:30 PM	3:00 PM	3:30 PM	4:00 PM	4:30 PM	5:00 PM	5:30 PM
Number of Private Stalls		26	24	30	31	34	34	36	35	33	34	30	29	27	25	27	27	19	16
Private Parking Utilization		37%	34%	42%	44%	48%	48%	51%	49%	46%	48%	42%	41%	38%	35%	38%	38%	27%	23%
Number of Public Stalls		129	152	177	178	189	163	184	205	202	183	154	151	134	106	120	117	102	122
Public Parking Utilization		43%	51%	59%	60%	63%	55%	62%	69%	68%	61%	52%	51%	45%	35%	40%	39%	34%	41%
Total Number of Stalls		155	176	207	209	223	197	220	240	235	217	184	180	161	131	147	144	121	138
Overall Utilization		42%	48%	56%	56%	60%	53%	59%	65%	64%	59%	50%	49%	44%	35%	40%	39%	33%	37%

Area CT-03 (Section #3)

Total No. of Stalls																			
Private	Public																		
45	149	194																	
		9:00 AM	9:30 AM	10:00 AM	10:30 AM	11:00 AM	11:30 AM	12:00 PM	12:30 PM	1:00 PM	1:30 PM	2:00 PM	2:30 PM	3:00 PM	3:30 PM	4:00 PM	4:30 PM	5:00 PM	5:30 PM
Number of Private Stalls		9	10	12	22	21	23	26	24	19	19	25	20	21	22	21	16	16	11
Private Parking Utilization		20%	22%	27%	49%	47%	51%	58%	53%	42%	42%	56%	44%	47%	49%	47%	36%	36%	24%
Number of Public Stalls		38	46	73	89	109	110	100	125	117	125	108	114	101	100	77	86	79	69
Public Parking Utilization		26%	31%	49%	60%	73%	74%	67%	84%	79%	84%	72%	77%	68%	67%	52%	58%	53%	46%
Total Number of Stalls		47	56	85	111	130	133	126	149	136	144	133	134	122	122	98	102	95	80
Overall Utilization		24%	29%	44%	57%	67%	69%	65%	77%	70%	74%	69%	69%	63%	63%	51%	53%	49%	41%

Area CT-03 (Section #4)

Total No. of Stalls																			
Private	Public																		
104	141	245																	
		9:00 AM	9:30 AM	10:00 AM	10:30 AM	11:00 AM	11:30 AM	12:00 PM	12:30 PM	1:00 PM	1:30 PM	2:00 PM	2:30 PM	3:00 PM	3:30 PM	4:00 PM	4:30 PM	5:00 PM	5:30 PM
Number of Private Stalls		38	43	50	44	49	52	52	52	58	64	63	61	50	55	60	52	31	25
Private Parking Utilization		37%	41%	48%	42%	47%	50%	50%	50%	56%	62%	61%	59%	48%	53%	58%	50%	30%	24%
Number of Public Stalls		59	76	87	87	91	102	106	101	104	106	97	90	84	78	68	51	49	32
Public Parking Utilization		42%	54%	62%	62%	65%	72%	75%	72%	74%	75%	69%	64%	60%	55%	48%	36%	35%	23%
Total Number of Stalls		97	119	137	131	140	154	158	153	162	170	160	151	134	133	128	103	80	57
Overall Utilization		40%	49%	56%	53%	57%	63%	64%	62%	66%	69%	65%	62%	55%	54%	52%	42%	33%	23%

COMBINED

Total No. of Stalls																				
Private	Public	Total																		
374	889	1263																		
			9:00 AM	9:30 AM	10:00 AM	10:30 AM	11:00 AM	11:30 AM	12:00 PM	12:30 PM	1:00 PM	1:30 PM	2:00 PM	2:30 PM	3:00 PM	3:30 PM	4:00 PM	4:30 PM	5:00 PM	5:30 PM
Number of Private Stalls			153	164	197	196	203	214	216	199	203	203	210	204	188	200	192	175	143	99
Private Parking Utilization			41%	44%	53%	52%	54%	57%	58%	53%	54%	56%	55%	50%	53%	51%	47%	38%	26%	
Number of Public Stalls			410	523	584	620	663	628	639	681	651	643	616	589	543	496	465	445	360	325
Public Parking Utilization			46%	59%	66%	70%	75%	71%	72%	77%	73%	72%	69%	66%	61%	56%	52%	50%	40%	37%
Total Number of Stalls			563	687	781	816	866	842	855	880	854	846	826	793	731	696	657	620	503	424
Overall Utilization			45%	54%	62%	65%	69%	67%	68%	70%	68%	67%	65%	63%	58%	55%	52%	49%	40%	34%

Date: Saturday, June 11, 2016
Stalls In Use

Area CT-01 (Section #1)

Total No. of Stalls	
Private	Public
154	300

454

	10:00 AM	10:30 AM	11:00 AM	11:30 AM	12:00 PM	12:30 PM	1:00 PM	1:30 PM
Number of Private Stalls	36	43	41	54	48	53	56	65
Private Parking Utilization	23%	28%	27%	35%	31%	34%	36%	42%
Number of Public Stalls	100	101	136	133	139	139	143	172
Public Parking Utilization	33%	34%	45%	44%	46%	46%	48%	57%
Total Number of Stalls	136	144	177	187	187	192	199	237
Overall Utilization	30%	32%	39%	41%	41%	42%	44%	52%

Area CT-02 (Section #2)

Total No. of Stalls	
Private	Public
71	299

370

	10:00 AM	10:30 AM	11:00 AM	11:30 AM	12:00 PM	12:30 PM	1:00 PM	1:30 PM
Number of Private Stalls	14	14	22	23	24	18	22	21
Private Parking Utilization	20%	20%	31%	32%	34%	25%	31%	30%
Number of Public Stalls	61	67	94	112	195	193	208	203
Public Parking Utilization	20%	22%	31%	37%	65%	65%	70%	68%
Total Number of Stalls	75	81	116	135	219	211	230	224
Overall Utilization	20%	22%	31%	36%	59%	57%	62%	61%

Area CT-03 (Section #3)

Total No. of Stalls	
Private	Public
45	149

194

	10:00 AM	10:30 AM	11:00 AM	11:30 AM	12:00 PM	12:30 PM	1:00 PM	1:30 PM
Number of Private Stalls	11	11	11	17	15	14	16	20
Private Parking Utilization	24%	24%	24%	38%	33%	31%	36%	44%
Number of Public Stalls	63	78	106	103	126	132	135	132
Public Parking Utilization	42%	52%	71%	69%	85%	89%	91%	89%
Total Number of Stalls	74	89	117	120	141	146	151	152
Overall Utilization	38%	46%	60%	62%	73%	75%	78%	78%

Area CT-03 (Section #4)

Total No. of Stalls	
Private	Public
104	141

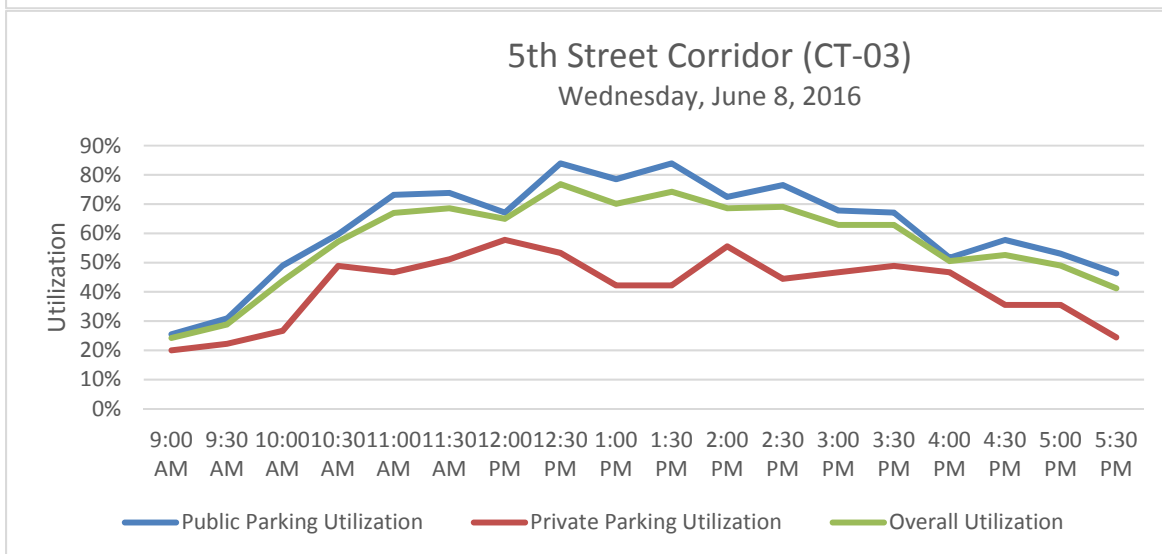
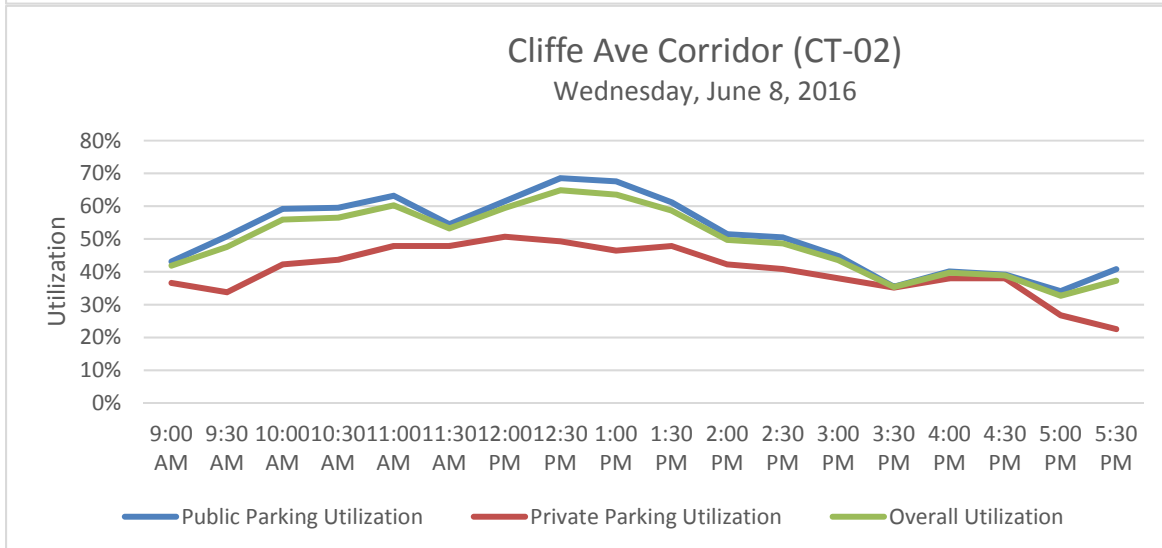
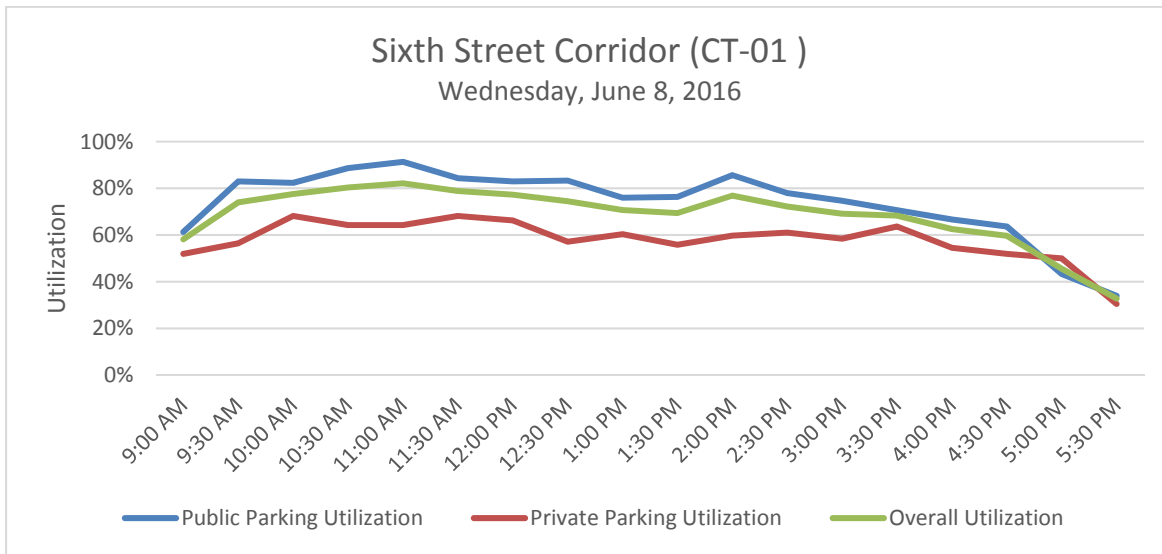
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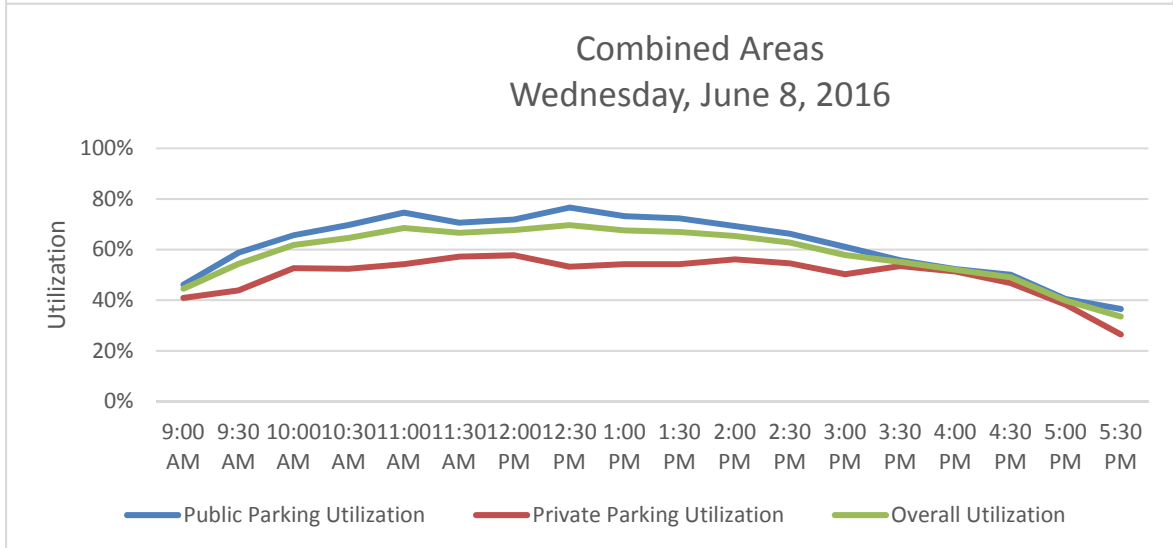
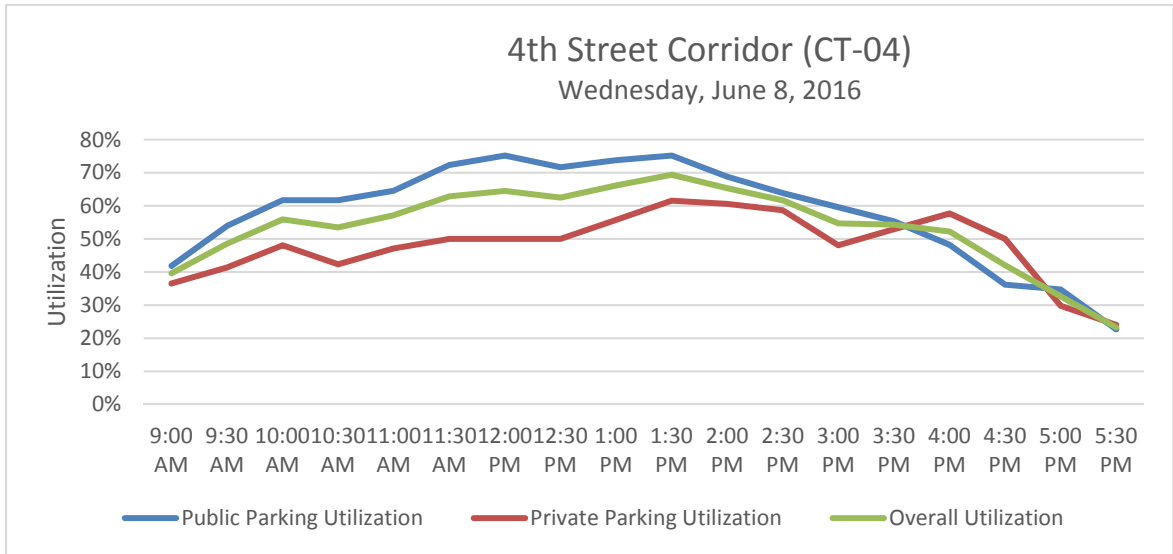
	10:00 AM	10:30 AM	11:00 AM	11:30 AM	12:00 PM	12:30 PM	1:00 PM	1:30 PM
Number of Private Stalls	13	24	28	27	30	40	54	63
Private Parking Utilization	13%	23%	27%	26%	29%	38%	52%	61%
Number of Public Stalls	63	86	104	102	113	122	138	139
Public Parking Utilization	45%	61%	74%	72%	80%	87%	98%	99%
Total Number of Stalls	76	110	132	129	143	162	192	202
Overall Utilization	31%	45%	54%	53%	58%	66%	78%	82%

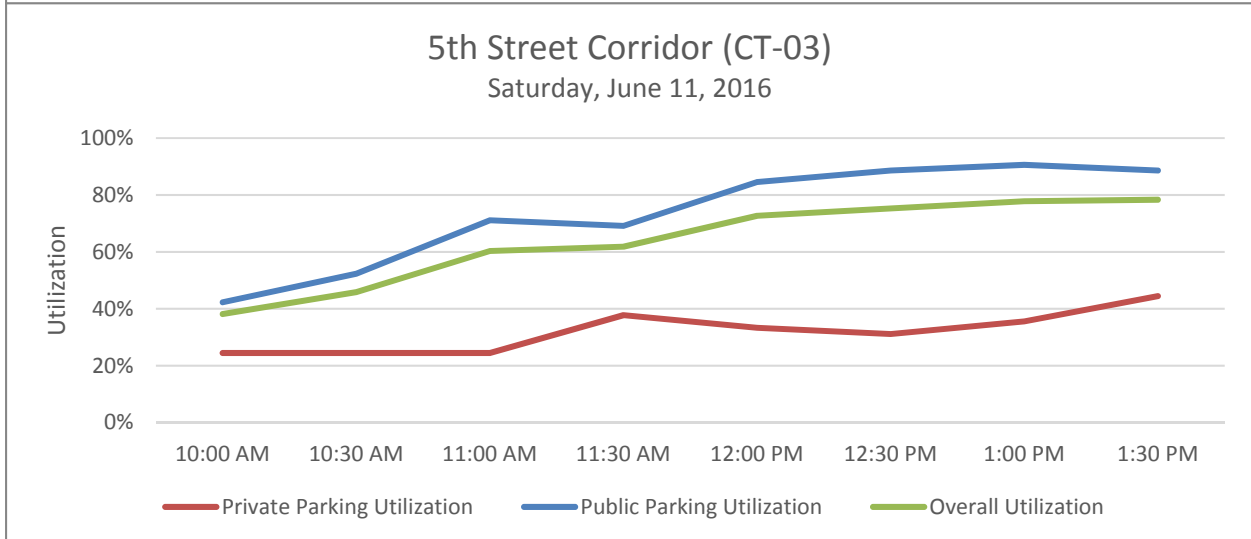
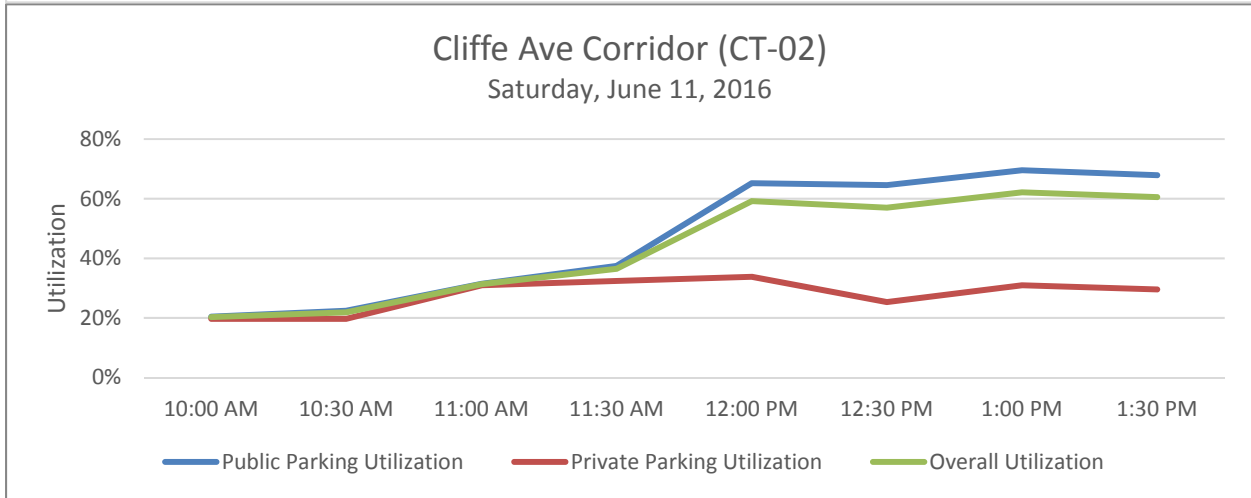
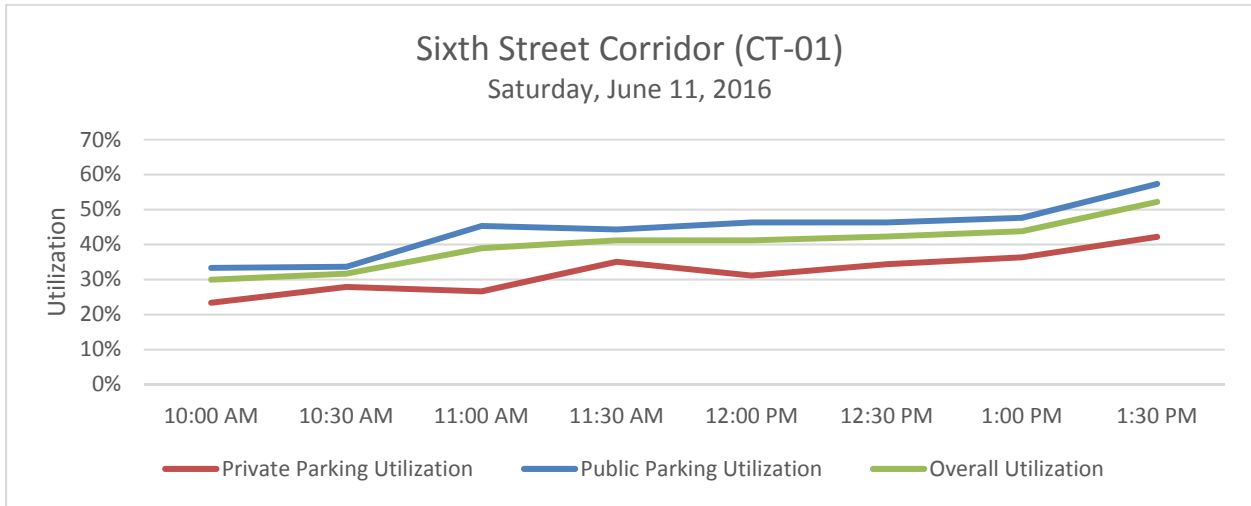
Total No. of Stalls		
Private	Public	Total
374	889	1263

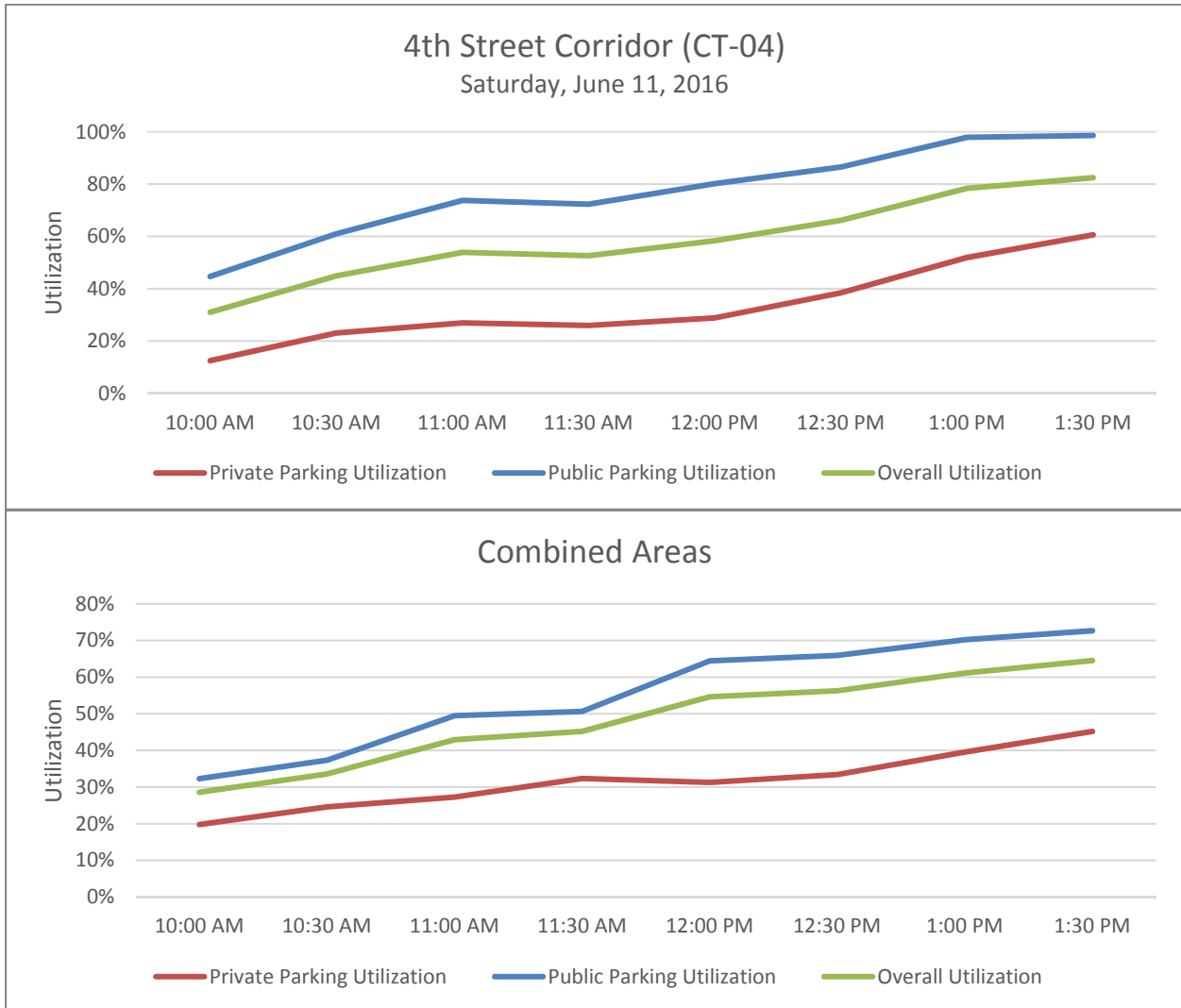
COMBINED

	10:00 AM	10:30 AM	11:00 AM	11:30 AM	12:00 PM	12:30 PM	1:00 PM	1:30 PM
Number of Private Stalls	74	92	102	121	117	125	148	169
Private Parking Utilization	20%	25%	27%	32%	31%	33%	40%	45%
Number of Public Stalls	287	332	440	450	573	586	624	646
Public Parking Utilization	32%	37%	49%	51%	64%	66%	70%	73%
Total Number of Stalls	361	424	542	571	690	711	772	815
Overall Utilization	29%	34%	43%	45%	55%	56%	61%	65%









4 OBSERVATIONS

- Sector 1 – (6th Street to 8th Street, MCSL drawing CT-01) contained 154 private, and 300 public parking spaces.
- Sector 2 – (Cliffe Ave, MCSL drawing CT-02) contained 71 private, and 299 public parking spaces.
- Sector 3 – (5th Street, MCSL drawing CT-03) contained 45 private, and 149 public parking spaces.
- Sector 1 – (4th Street, MCSL drawing CT-04) contained 104 private, and 141 public parking spaces.
- Total Private parking spaces in study area = 374, total public parking spaces in study area = 889, for a combined total of 1263 parking spaces.

- Private parking was generally more consistently utilized during weekday counts (as compared to public parking facilities).
- Private parking was generally utilized at less than 60% during weekday counts.
- Weekday private parking counts did not identify a consistent “peak” utilization period.
- Weekend private parking counts trended towards higher utilization as the day went on, i.e., utilization generally increased as the day went on, with counts concluding before peak utilization was observed.
- Weekday public parking counts indicate a pronounce peak utilization period, generally between 10:00am and 2:30pm.
- The 5th street and 6th street corridors yielded the highest peak weekday public parking utilization at 84% and 91% respectively.
- During peak weekday parking periods (10:00am to 2:30pm), private parking utilization to typically between 40-65%. During this same period, public utilization is typically 10-40% higher.

5 CLOSURE

We trust the information herein is as required at this time. Should the City wish to discuss the findings of this document, we would gladly make ourselves available at your convenience.

Yours truly,

MCELHANNEY CONSULTING SERVICES LTD.



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Enclosures

BH/njg

REVISION HISTORY

Date	Status	Revision	Author
January 12, 2017	Final	Rev. 0	B. Hudson, P.Eng.

LIMITATION

This report has been prepared for the exclusive use of the City of Courtenay. The material in it reflects the best judgement of the Consultant in light of the information available to the Consultant at the time of preparation. As such, McElhanney, its employees, sub-consultants and agents will not be liable for any losses or other consequences resulting from the use or reliance on the report by any third party.

Downtown Courtenay Parking Study

Utilizing Drones and GIS to provide added-value to the gathering of parking data

Matthew Browning, P.Eng

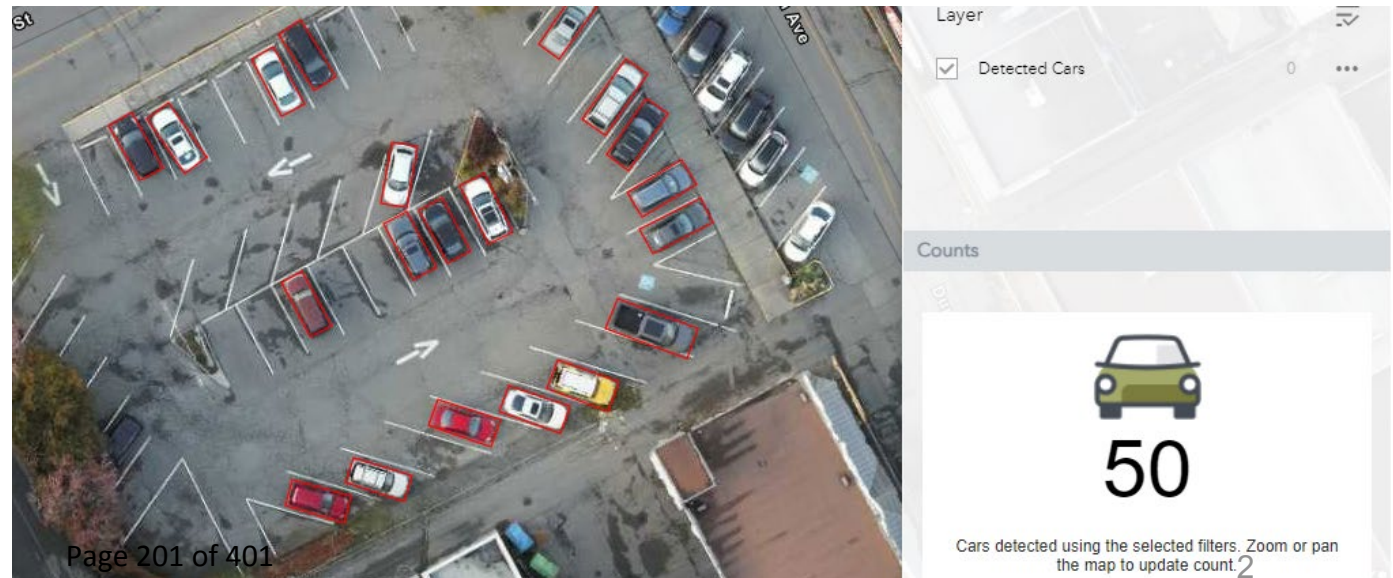
June 12 2024

Page 200 of 401



- Introduction
- Project Overview
 - Traditional Methodology
 - New Approach
 - Findings and Outcomes
- Benefits and Future Uses

Today's Presentation





Introduction

Matthew Browning P.Eng

Matthew Browning, P.Eng

19 years in the transportation industry

- Liverpool
- Toronto
- Vancouver Island

Island projects

- Parksville TMP
- Port Alice AT Plan
- Six Mile Mobility Hub
- Hullo Ferry Terminal
- Courtenay Parking Study



Project Overview

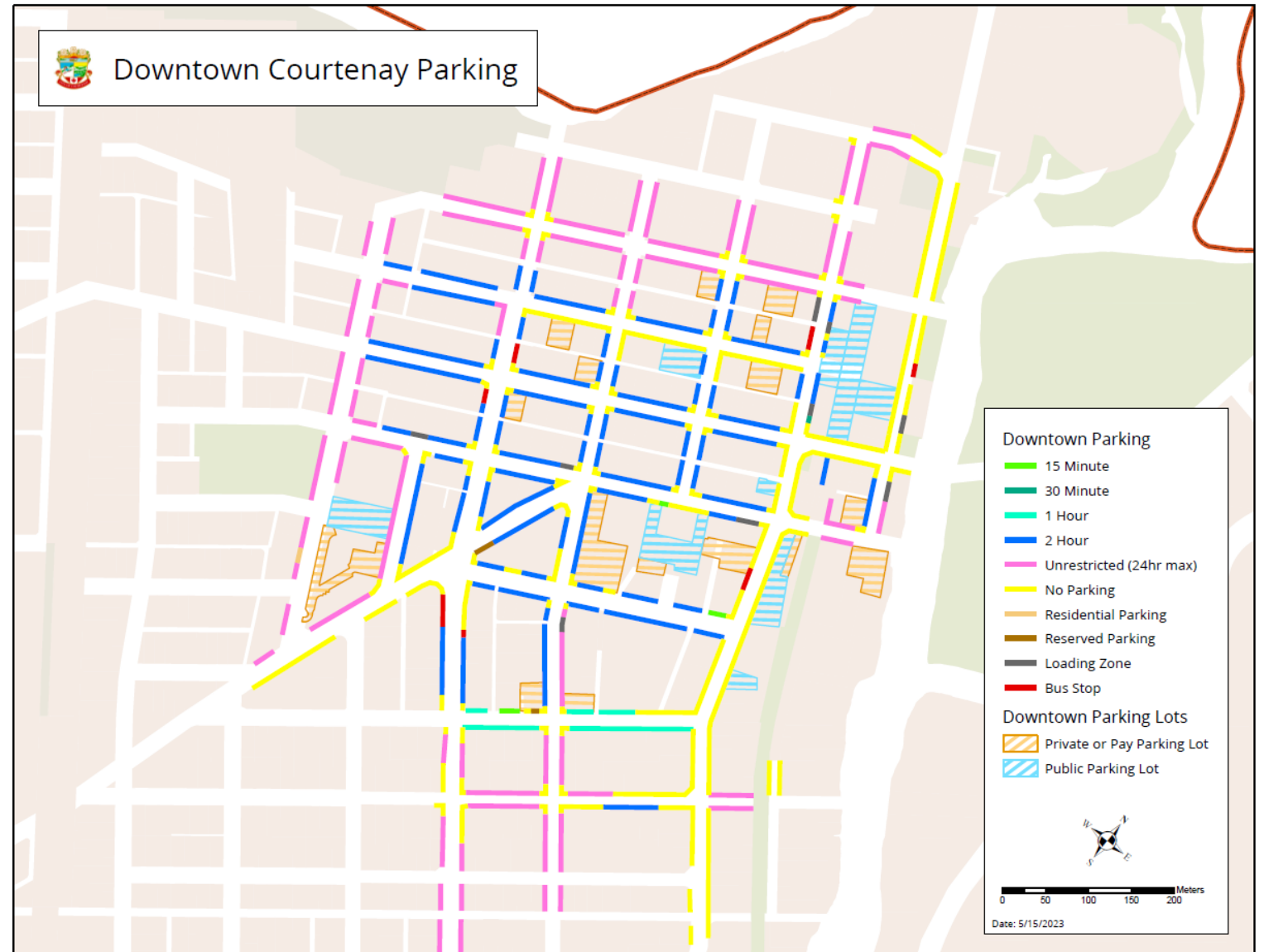
Downtown Courtenay Parking Study

Downtown Courtenay Parking Study

- City of Courtenay staff reached out to us:
 - We previously completed the parking study in 2017
 - We're known to the City for our multi-disciplinary engineering, surveying and planning work
 - Limited work in terms of transportation planning services for the City
- Request to update the parking study with minimal prescriptive language about *how*

Study Area

- 2023 study extents provided (see right)



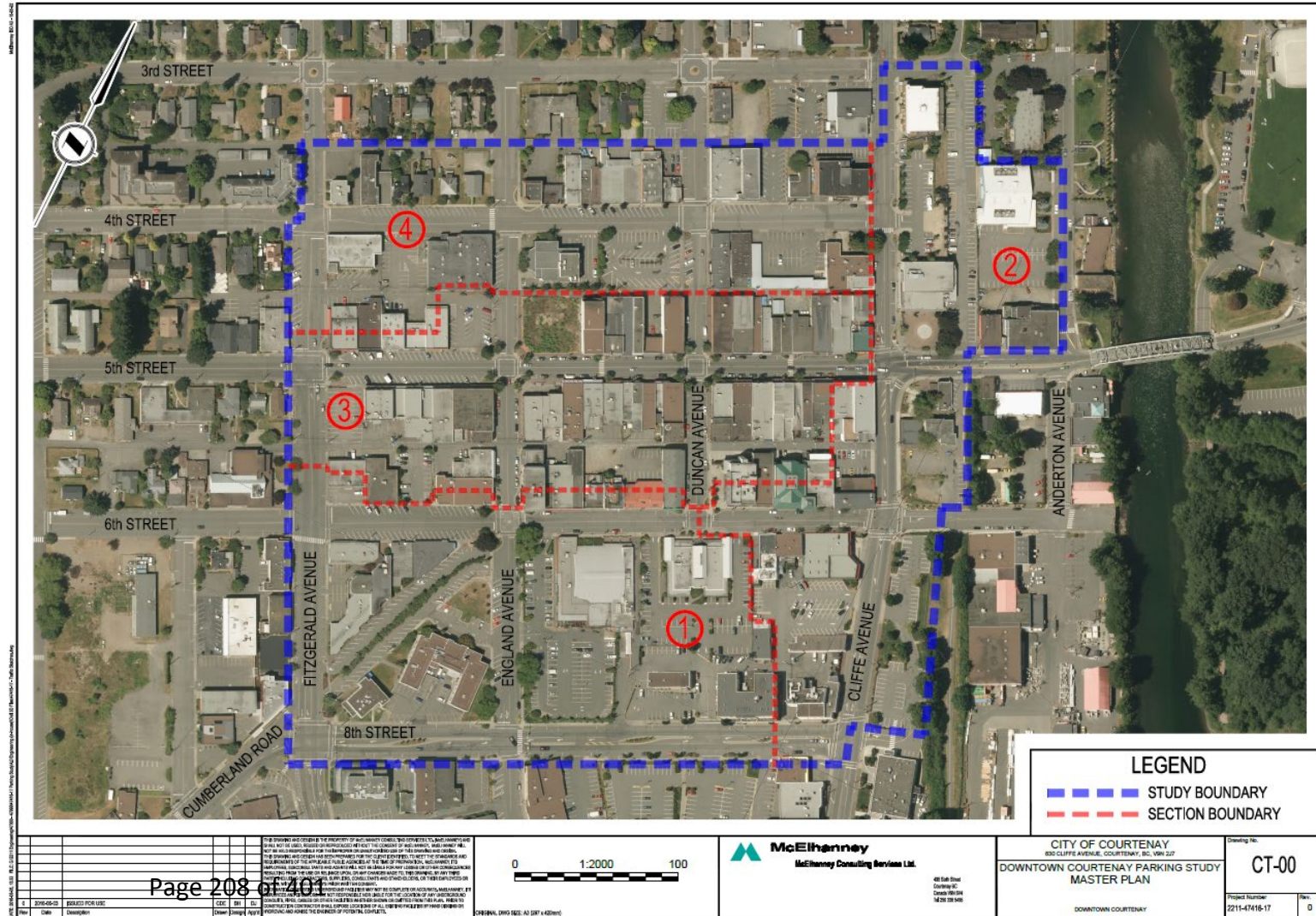


Traditional Methodology

Previous Parking Study

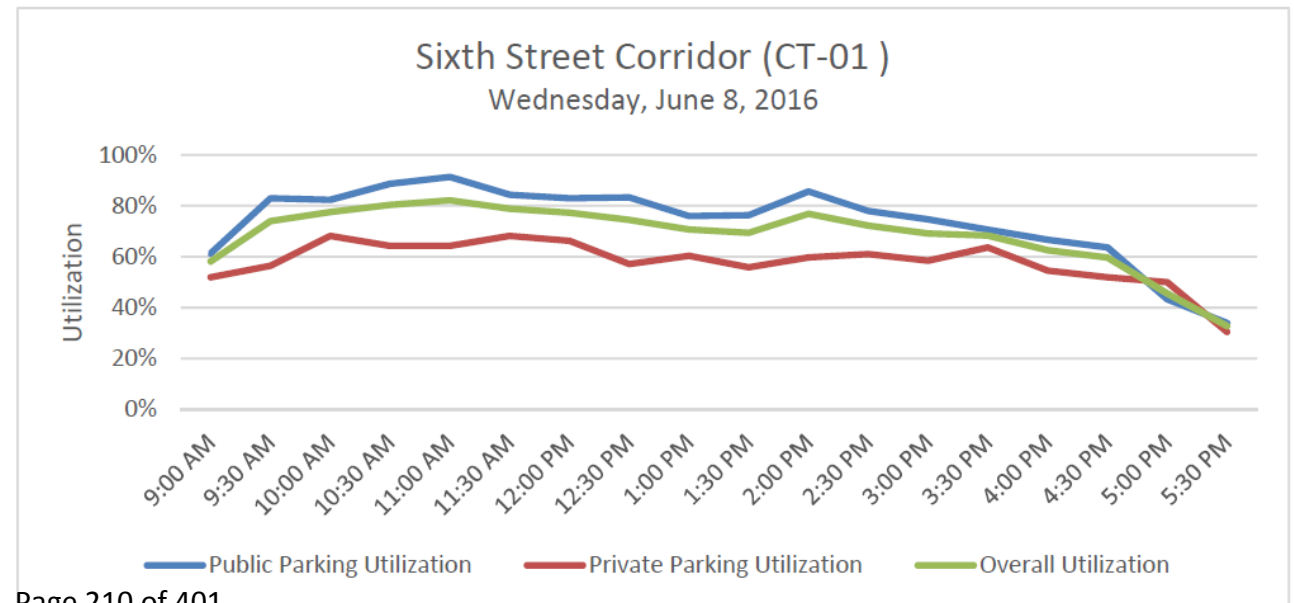
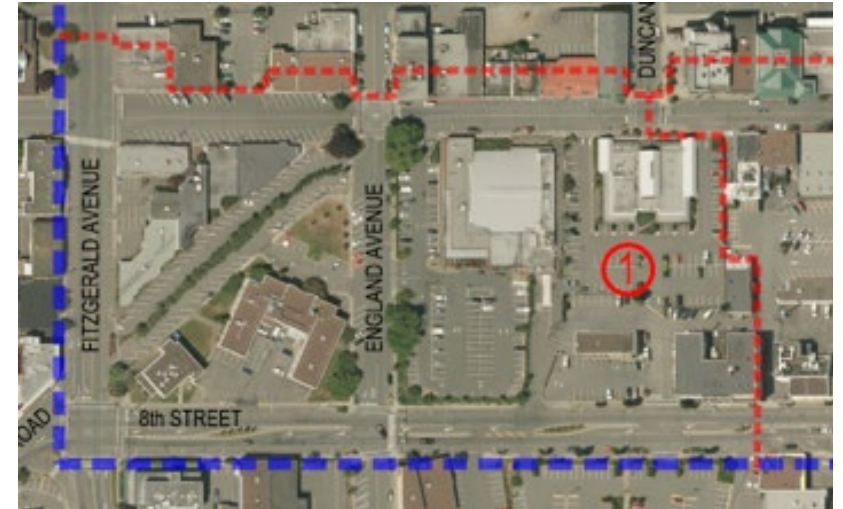
Previous Parking Study

- 2017 study focused on a smaller downtown core and was divided into four sectors;
- Easier for technicians to walk around and count cars within a 30-minute time-period
- Counts only differentiated between public and private parking



Previous Parking Study

- The utilization chart of the “Sixth Street Corridor”, for example, is referring to the *entire* Sector #1 area
- What does this tell the City?
- What planning actions can you take based on this?
- For example; what would happen if they were to transition the angled parking on Sixth Street to parallel?
- Is there a better way to do this?



A New Approach

Drones and GIS

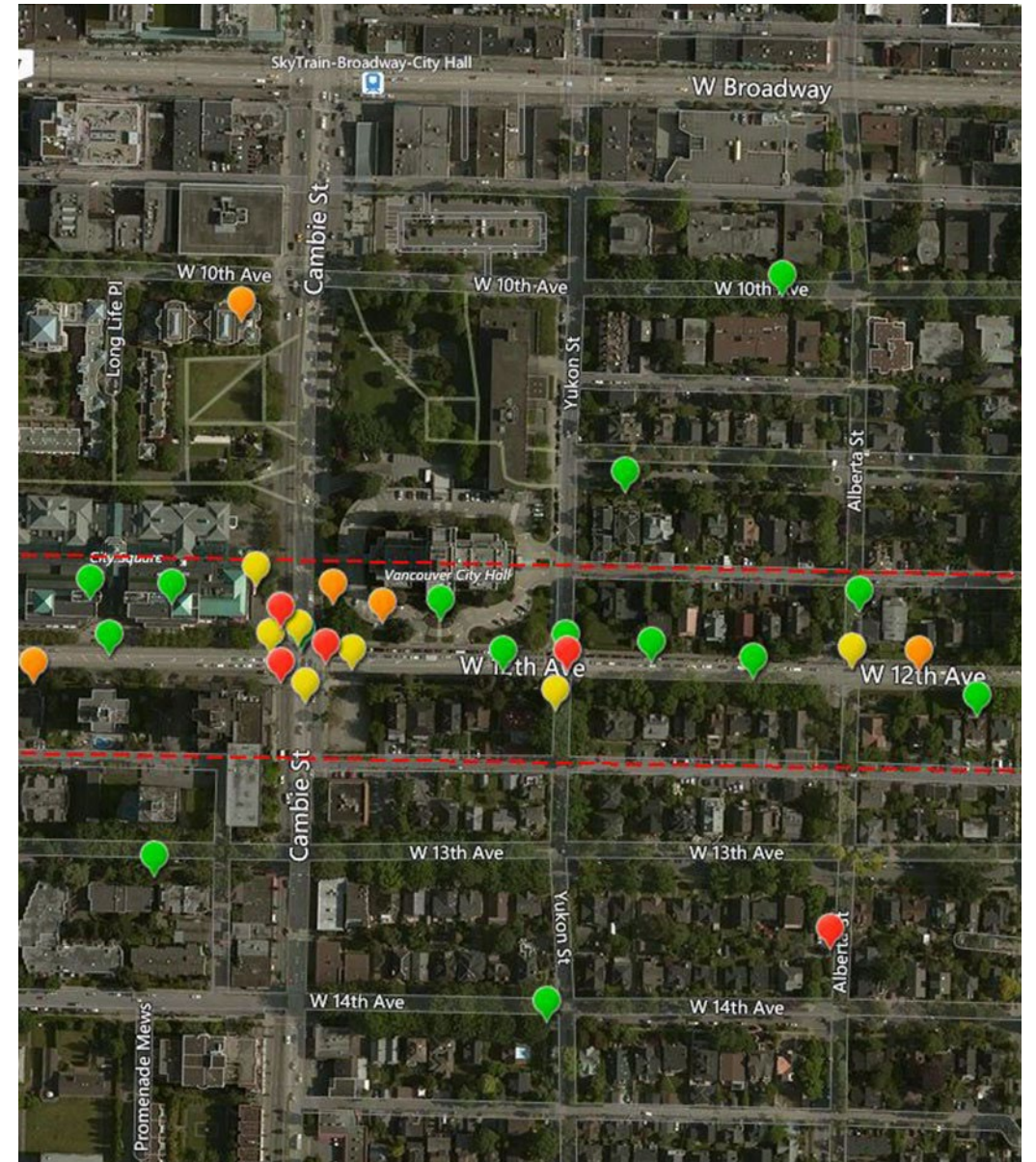
Proposal

- McElhanney is at the forefront of Remotely Piloted Aircraft Systems (RPAS) technologies in Canada.
- We have 150+ pilots and over than 60 registered drones tackling a wide array of challenges across western Canada



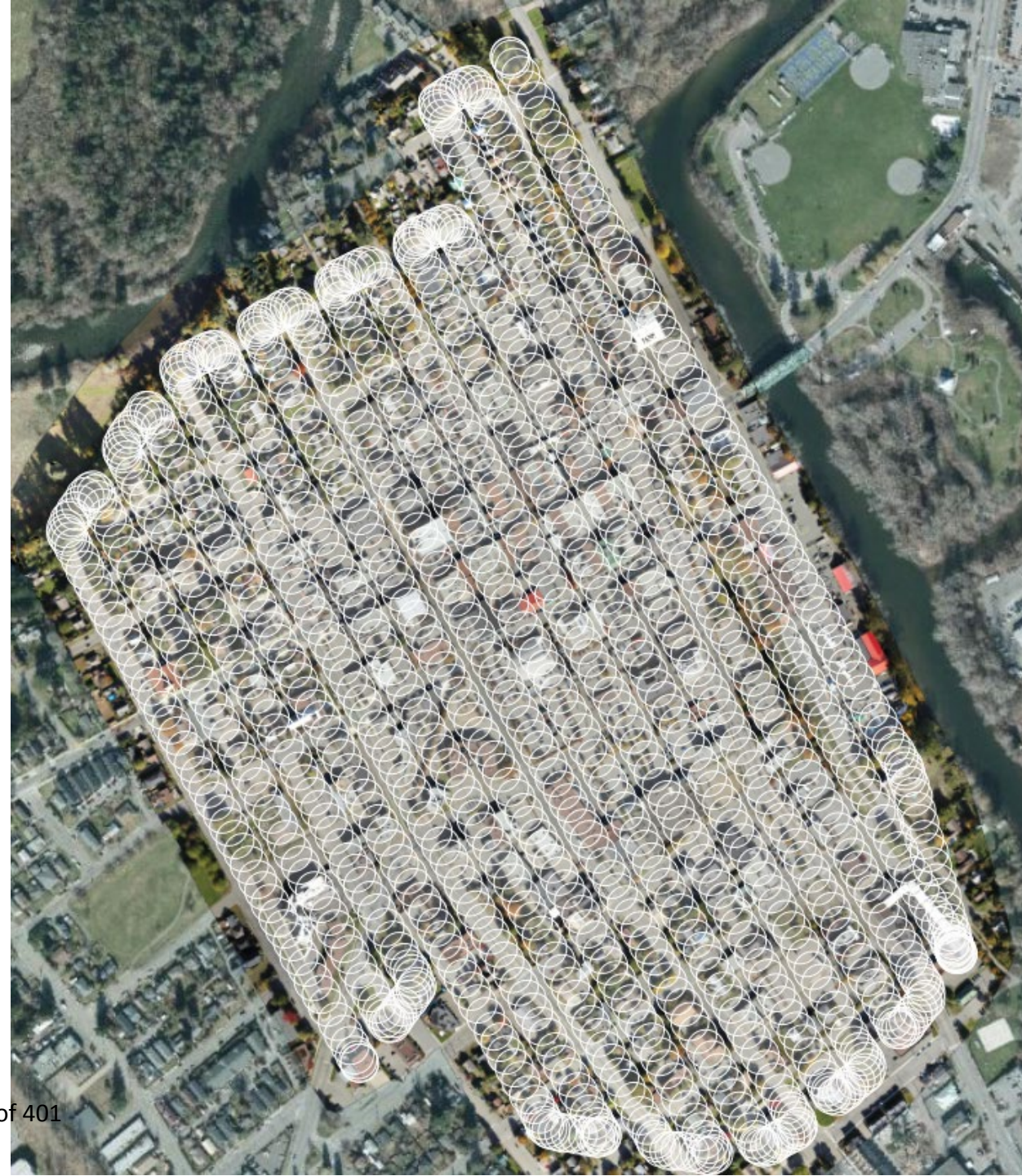
Proposal

- We also have a talented GIS and Spatial Analysis team, working in mapping, spatial analysis, data handling, analysis and visualization.
- The plan was to bring these two disciplines together to find a new approach to conducting parking data collection and analysis



Proposal

- Our workplan proposed to fly a drone mission across the downtown of Courtenay, capturing hundreds of individual images
- These images would then be assembled into an orthomosaic that could be imported into a GIS database
- We would conduct analysis using vehicle recognition software in GIS





The Project

Processing and Rapid analysis

The Project

- We were able to fly four missions (2 weekdays and 2 Saturdays) using a single drone operator as the human resource
- The process to stitch the images together was seamless and provided an initial tool that had immediate benefits



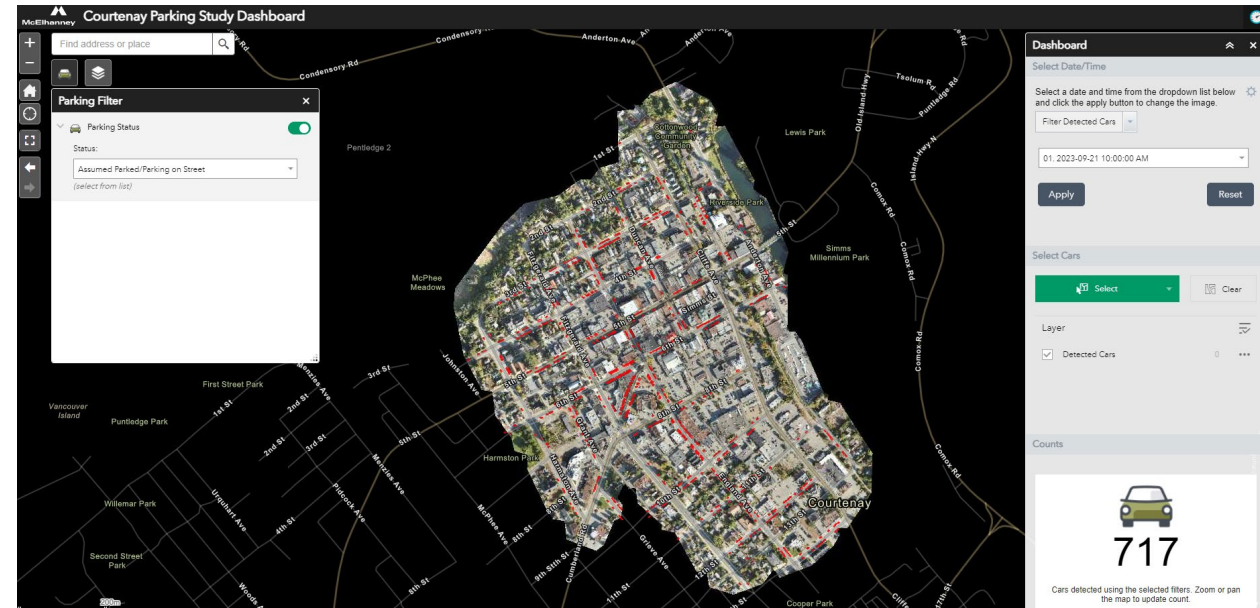
The Project

- Two orthomosaics can be compared by dragging across the screen
- The images show September 30th at 11am and 1:30pm along 5th Street
- Any individual location can be compared for two time periods, illustrating parking utilization, duration, informal parking, etc.



The Project

- The next step was to import the orthomosaics into GIS
- The dashboard created then allows for bespoke interrogation, based on vehicle identification software
 - 717 vehicles parked on-street at 10:00am on Thursday, September 21
 - 47 vehicles parked on-street on Sixth Street specifically
 - Data can be exported to excel for further analysis



► *Rapid analysis*

The Project

- Any individual area can be assessed for on/off-street parking and/or driving



and click the apply button to change the image.

Filter Detected Cars

01. 2023-09-21 10:00:00 AM

Apply Reset

Select Cars

Select Clear

Layer

<input checked="" type="checkbox"/>	Detected Cars	17	...
<input type="checkbox"/>	Bus Stop	0	...

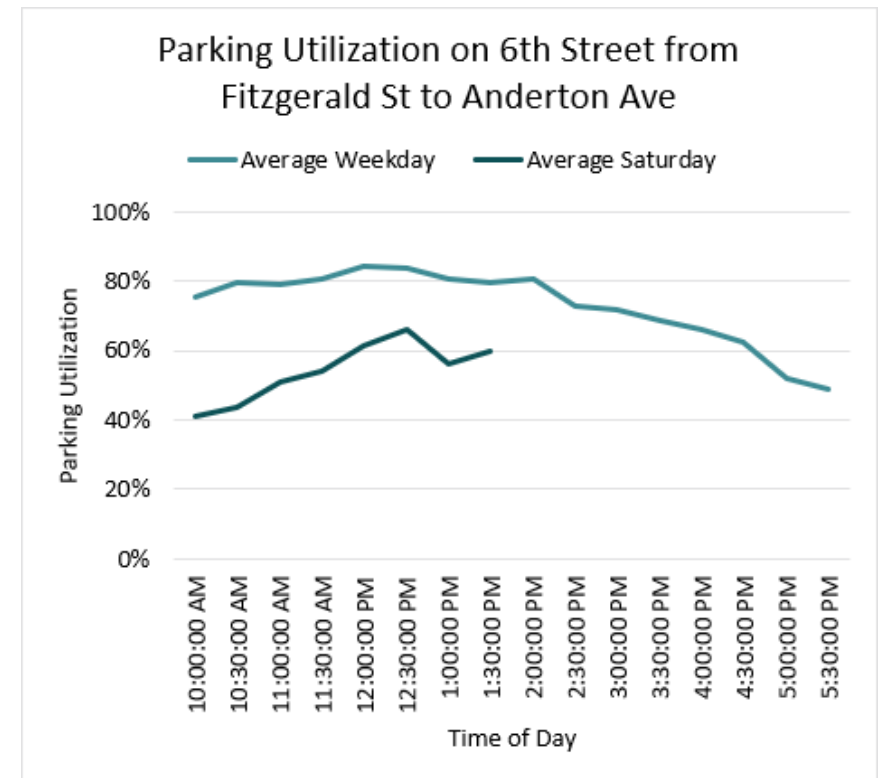
Counts


17

The Project

- Mid-project, staff mentioned the need for data collection in support of 6th Street Active Transportation Options Analysis
- Within the day, we were able to generate usable insights
 - Weekday and weekend utilization
 - Specific - to the block - along the corridor

Time of Day	Occupied		Utilization	
	Average Weekday	Average Saturday	Average Weekday	Average Saturday
10:00:00 AM	72	39	75%	41%
10:30:00 AM	76	42	79%	44%
11:00:00 AM	75	49	79%	51%
11:30:00 AM	77	52	81%	54%
12:00:00 PM	80	59	84%	62%
12:30:00 PM	80	63	84%	66%
1:00:00 PM	77	54	81%	56%
1:30:00 PM	76	57	79%	60%
2:00:00 PM	77		81%	
2:30:00 PM	69		73%	
3:00:00 PM	68		72%	
3:30:00 PM	66		69%	
4:00:00 PM	63		66%	
4:30:00 PM	60		63%	
5:00:00 PM	50		52%	
5:30:00 PM	47		49%	





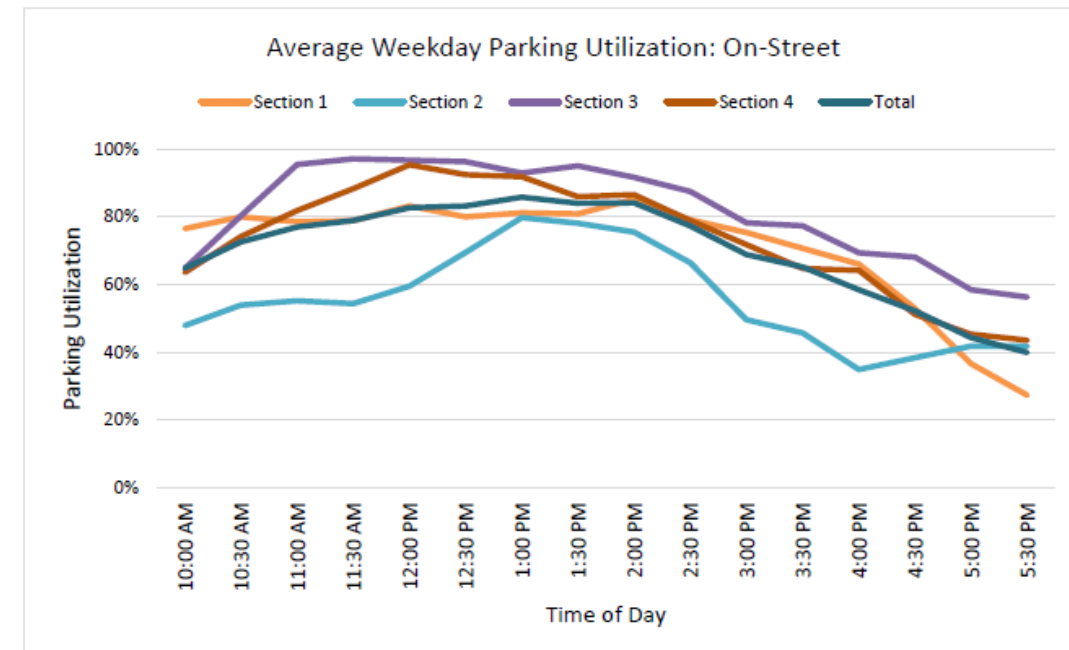
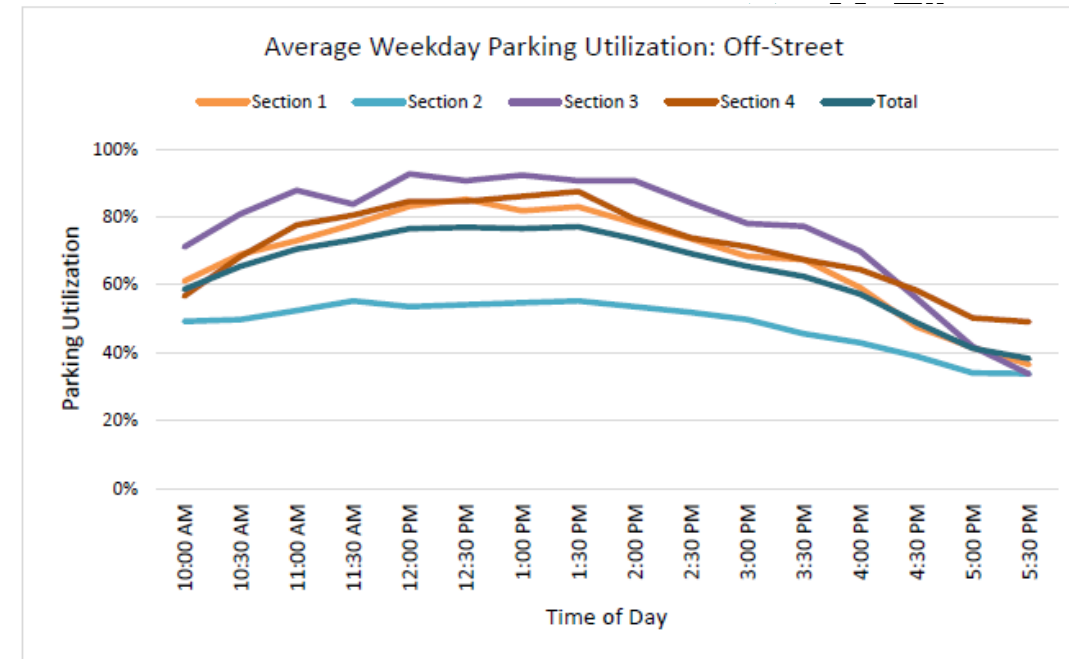
Key Findings

Summary and Site-Specific Observations

► Summary

Key Findings

- Total utilization for off-street parking never exceeds 80% for the downtown core
- On-street parking tends to be slightly higher, but both drop off dramatically after ~2pm
 - Both graphs are for the average weekday (without farmer's markets or other activities)
 - Weekend utilization is rarely more than 50-60% in total



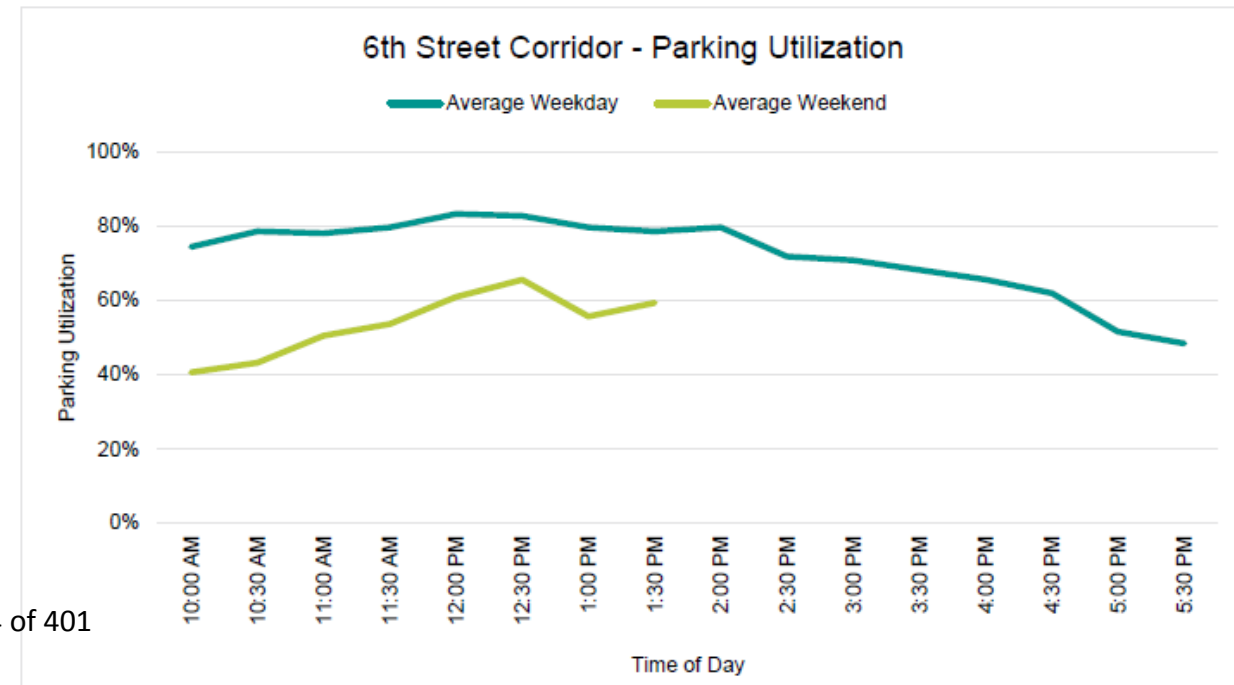
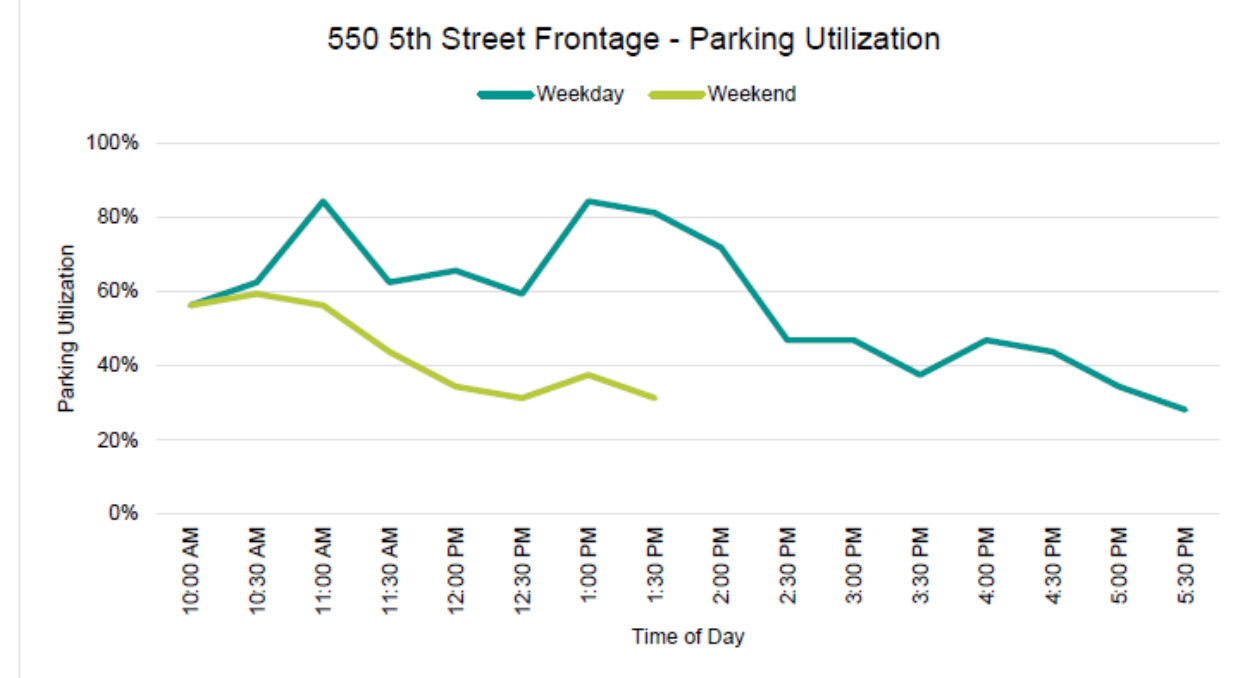
Key Findings

- Staff also asked us to look specifically at the following locations:
 - 550 5th Street frontage
 - 445 10th Street frontage
 - 5th Street corridor between Fitzgerald Ave and England Ave
 - 6th Street corridor
 - Old Orchard Area
 - Multiple off-street parking lots



Key Findings

- Because of the way the GIS is set up, we were able to rapidly complete bespoke analyses of each of these areas and pull meaningful insights that can guide future decision-making





Benefits and Future Uses

Summary

Summary

- Near-elimination of H&S risks associated with manual parking counts
- Granular data that can be interrogated for bespoke purposes, specific to the needs of the City
- Accuracy and validation of results
- Consistent data capture that can be updated at any time
- High-res images can identify a host of additional features:
 - Accessible parking utilization
 - Bike parking utilization
 - Long-term / abandoned parking
- The provides ongoing long-term opportunities for additional uses of the data and the City has access to the GIS portal

Summary

- We recently held a training session so that staff can access and utilize the portal at any time, to conduct whatever analysis is needed for future planning projects



Dashboard

Select Date/Time

Select a date and time from the dropdown list below and click the apply button to change the image.

Filter Detected Cars

01. 2023-09-21 10:00:00 AM

Apply Reset


Select Cars

Select Clear

Layer

Detected Cars 0

Counts


2,211

Cars detected using the selected filters. Zoom or pan the map to update count.

Thank-you

Matthew Browning, P.Eng.
236-317-3077
MBrowning@McElhanney.com





The Corporation of the City of Courtenay

Staff Report

To: Council

File No.: 0250-20 CVFMA

From: Director of Recreation, Culture and Community Services

Date: June 12, 2024

Subject: Comox Valley Farmers' Market Native Sons Hall Rental Fee Reduction Request

PURPOSE:

To provide Council options to consider in response to the Comox Valley Farmers' Market Association's (CVFMA) request for a Native Sons Hall Grand Hall rental fee reduction of their current fees based on the City of Courtenay Fees and Charges Amendment Bylaw No. 2947, 2018. The recommended option would result in:

- CVFMA paying the adult non-profit category rates as established in the Fees and Charges Bylaw No. 2988, 2024. The assistance through an adjustment to the categorization of the CVFMA's facility use would be administered through a partnering agreement, subject to CVFMA meeting service and reporting deliverables aligning with the objectives, policies and actions of the Official Community Plan, 2022.

EXECUTIVE SUMMARY:

CVFMA approached the City December 6, 2023 requesting a reduction to their rental fees which at the time were as per City of Courtenay Fees and Charges Amendment Bylaw No. 2947, 2018.

Council adopted the Corporation of the City of Courtenay Fees and Charges Amendment Bylaw No. 2988, 2024 on April 28, 2024 based on the Recreation Fees and Charges Framework adopted in June, 2022. As per Bylaw No. 2988, 2024 non-for-profit commercial use of indoor facilities is to be charged a rental rate equivalent to 100% of the unit-cost of providing that space, with no tax funded subsidy applied. As per the bylaw, this fee is phased in over a period of five years to incrementally adjust fees for rental groups.

Staff are proposing the following options for Council consideration in response to CVFMA's request for a fee reduction:

- Option 1: Apply the facility use category rate of *Not-for-Profit Commercial Use* as per Bylaw No. 2988, 2024
 - a. Approximate total of three-year rental charges: \$41,440
 - b. Approximate total of three-year subsidization of unit costs: \$18,955
 - c. Approximate annual increase in rental charges as compared to current rate: \$2,338.25 in 2024, \$5,418.00 in 2025, and \$7,098.00 in 2026.
- Option 2: City and CVFMA to enter into Partnering Agreement and City to apply the facility use category rate of *Not-for-Profit Adult* as per Bylaw No. 2988, 2024
 - a. Approximate total of three-year rental charges: \$29,587
 - b. Approximate total of three-year subsidization of unit costs: \$30,810
 - c. City and CVFMA to enter into Partnering Agreement
 - d. Approximate annual increase in rental charges as compared to current rate: \$481.25 in 2024, \$1,155 in 2025, and \$1,365 in 2026.

- Option 3: City and CVFMA to enter into Partnering Agreement and City to provide a grant to CVFMA to reduce the fees beyond those outlined in Bylaw No. 2988, 2024:
 - a) Option 3(a): Provide a grant in an amount that results in CVFMA continuing to pay the Community Daily Rate as per Bylaw No. 2947, 2018. Grant amount to reflect the difference between Community Daily rate and the not-for-profit adult rate listed in Bylaw No. 2988, 2024:
 - a. Approximate total of three-year rental charges: \$29,587
 - b. Approximate total three-year value of the grant: \$3,000
 - c. Approximate total subsidization of unit costs: \$33,810 (total of grant and facility subsidization costs)
 - d. Approximate net (after grant) total of three-year rental charges: \$26,586
 - b) Option 3(b): Provide a grant in an amount that results in a further reduction of fees for CVFMA from what they are currently charged under Bylaw No. 2947, 2018:
 - a. Approximate total of three-year rental charges: \$29,587
 - b. Total three-year value of the grant: \$15,000 (\$5,000 per year)
 - c. Approximate total subsidization of unit costs: \$45,810 (total of grant and facility subsidization costs)
 - d. Approximate net (after grant) total of three-year rental charges: \$14,586

Staff are recommending that Council approve Option 2, whereby the City and CVFMA to enter into a Partnering Agreement and the City applies the facility use category rate of Not-for-Profit Adult as per Bylaw No. 2988, 2024 to the CVFMA's use of the Native Sons Hall Grand Hall. The recommended option allows time for CVFMA to explore alternative locations for a year around permanent farmers' market, treats CVFMA equally to other adult not-for-profits and is within the policy and bylaw approved by Council. This option allows time for CVFMA to participate in City land use and park planning processes that may contribute to achieving a common vision for a permanent year-round farmers' market that has adequate supporting infrastructure in place.

BACKGROUND:

At the December 6, 2023 Council meeting, CVFMA requested a reduction in the cost of the Native Sons Hall Grand Hall rental fees. With increasing operating costs, CVFMA reported that they have reduced and eliminated expenses where possible, including optimizing their paid employees and numerous volunteers. CVFMA shared that the board is seeking ways to reduce market expenses without reducing programming, markets, or staff. A reduction in the cost of venue rentals was identified as a way to reduce market expenses.¹

In response to CVFMA's delegation Council resolved the following on January 10, 2024:

THAT Council direct staff to prepare a report on the request from the December 6, 2023 Comox Valley Farmers' Market delegation to reduce rental fees at the Native Sons Hall for fall and winter markets.

Recreation Fees and Charges Framework and Bylaw

In July 2022, Council adopted the Recreation Fees and Charges Framework and directed staff to proceed with the implementation of the framework. Staff were not in a position to report back on the CVFMA request until the implementation of the Recreation Fees and Charges Framework had been completed.

At the April 24, 2024 Council Meeting, Council adopted the Corporation of the City of Courtenay Fees and Charges Amendment Bylaw 2988, 2024. The updated recreation indoor rental and user fees were

¹ December 6, 2023 CVFMA presentation slides p.21

established based on the benefits-based approach outlined in the Recreation Fees and Charges Framework where greater levels of subsidy (funded through taxation) are applied to rentals with increasing levels of community benefit as per the Benefits Continuum illustrated in Table 1.

Table 1: *Benefits Continuum*²

Benefits Continuum					
Who Benefits	Community only	Mostly the community and partially the user	Community and user equally	Mostly the user and partially the community	User only
	←-----→				
Who Pays	0% User Fee 100% Tax Support	25% User Fee 75% Tax Support	50% User Fee 50% Tax Support	75% User Fee 25% Tax Support	100% User Fee 0% Tax Support

The updated fees and charges result in not-for-profit commercial uses, such as the Comox Valley Farmer’s Market, moving from paying a community rate that historically had the highest level of subsidy, to a rate which the user pays the full portion of the unit cost of the City providing the space. Under Bylaw 2988, 2024 CVFMA would no longer pay a rental rate where a portion of costs are supported through tax funded subsidy. With the updated bylaw rental facility rates in effect September 1, 2024, CVFMA would need to pay an additional \$2,338.25 in 2024, \$5,418.00 in 2025, and \$7,098.00 in 2026. Due to the cumulative number of bookings CVFMA has in a year, the compound effect of the changes to the fees and charges is much more when compared to a once a year vendor market.

The Recreation Fees and Charges Framework however does provide flexibility for certain types of activities that clearly achieve a high-level of public benefit. When considering alternative approaches to setting fees, the following factors are to be considered:

- Whether the intent is commercial or political,
- What the intent of the use/user is,
- If there is public benefit sought,
- Who benefits from the use; and
- How public the event is (e.g. open to the public).

It is via these considerations and the strategic alignment of the Comox Valley Farmer’s Market with the City of Courtenay’s OCP, 2022 Food Security policies that staff have reviewed the fee reduction request.

Comox Valley Farmer’s Market in Courtenay

The Comox Valley Farmers’ Market has held a winter market in the Native Sons Hall since 2008-2009, operating from mid-October to the week before Christmas. Since 2012, CVFMA winter market extended its months to run mid-October to early April on an annual basis with the exception of March 2020 when CVFMA, despite being declared an essential service, was unable to open to the public under Provincial Health Order due to space limitations at the Native Sons Hall.

² City of Courtenay Recreation Fees and Charges Framework, 2022 p. 13

In order to enable CVFMA to operate its winter market under the physical distancing requirements of the Provincial Health Order, the City granted CVFMA permission to expand into the Native Sons Hall parking lot in October 2020. CVFMA does not pay fees for the use of the parking lot, however they are responsible for arranging barriers and signage to inform the public of the parking lot closure. CVFMA has continued to use the Native Sons Hall parking lot in conjunction with its use of the Grand Hall each Saturday from approximately 7:00am to 2:00pm, October to April annually.

The City also provides in-kind event space for the Downtown Courtenay Summer Street Market. CVFMA informed the City that due to budget constraints and given the uncertainty about the fees for Native Sons Hall, CVFMA has elected not to coordinate a Summer Street Market this year. In lieu of the Summer Street Market, the Duncan Pilot Project Team, which CVFMA is a part of, are working with staff to hold a series of public space placemaking activities, as well as actively seeking funding for infrastructure (water, electrical, furniture, etc.).

DISCUSSION:

Farmers' Markets, Food Security & Sustainable Food Systems

Unlike other products and services, food is not simply a commodity; it is a basic human need as well as a social good. As recent disruptions to supply chains have demonstrated, food security for community members is increasingly dependent on foods shipped in from afar. Support of food policy and programs to strengthen the sustainability of local food systems is expanding across the region and province. Municipalities are now often participating at food policy tables to advance system changes where resources are leveraged across networks to support projects such as community gardens, food box distribution programs, sustainable food procurement programs, food hubs, and farmers markets.³

Farmers markets connecting the rural to the urban, the farmer to the consumer, and increasing equitable inclusive access to fresh ingredients year-round are proliferating, and becoming centre-pieces in communities.⁴ These markets deliver multiple benefits from strengthening the local economy to reducing food insecurity, and increasing social connectedness and community well-being.⁵ Farmers markets are understood to be an essential service, providing basic food needs in times of disruption to supply chains and supporting emergency preparedness in times of natural disasters.

In BC, more than 145 markets have registered as members of BC Association of Farmers Markets (BCAFM). Of these, 30 are year-round markets (operate during the winter) with 20 of the winter markets offered indoors. See Attachment 1: Sample List of Winter Markets in British Columbia. Winter markets, whether indoor or outdoor, are incentive for the many small-scale farmers to extend their growing period in order to produce more foods.

Winter markets operate with varied frequency from a couple of days to weekly over 6 months. Local governments, businesses, and community organizations offer a diversity of support to establish and maintain these markets in communities. The type of support may range from the provision of outdoor space, with shelter and utilities, to the provision of indoor space including utilities. Indoor winter markets provide the added benefit of a warm dry place to access foods and to socialize in the dark months.

³ Municipal Food Entrepreneurs: A preliminary analysis of how Canadian cities and regional districts are involved in food system change, (CAPI, 2014)

⁴ The Benefits of Year-Round Farmers' Markets, (IJLRAVS, 2022)

⁵ Need has Skyrocketed for Comox Valley Farmer's Market coupon Program , Chek TV, (Feb, 2024)

According to a 2022 BCAFM member survey, markets pay in the range of zero to \$30,000 annually for venue rentals (encompassing indoor and outdoor, seasonal and year-round markets). It is difficult to determine the amount of support provided by municipalities for winter markets, in particular indoor markets, from this data. From information that staff were able to review, the level of support can vary considerably. For example:

- Township of Esquimalt provides year-round indoor and outdoor space and utilities for their farmers' market in the form of in-kind and grant valued at approximately \$20,000.
- The City of Duncan provides year-round outdoor space plus winter shelters and utilities in the form of a grant valued at approximately \$18,000.
- In Nanaimo, Penticton, and Victoria, community NPOs, food and or agricultural co-ops, businesses, and school districts are the primary support for winter market facilities.

Factors affecting municipal support may include, but are not limited to, limited suitable space, demand on space, the amount of resources available including grant funds, and the extent to which resources are found elsewhere.

Courtenay's Official Community Plan, 2022 (OCP) reinforces the social, health, and economic benefits of farmers markets and outlines objectives and policies related to food security including working with the Comox Valley Farmer's Market to identify a permanent, year-round location, preferably in downtown Courtenay, with adequate supporting infrastructure. While the OCP, 2022 identifies objectives and policies related to support for local food systems and farmers markets, guidelines regarding the level of support to be applied are not currently in place or defined.

Fee Reduction Request Considerations

Recreation Fees and Charges & Comox Valley Farmers Market

Rental and user fees for indoor recreation facility use are governed by a fees and charges bylaw. Current indoor rental fees are outlined in Fees and Charges Bylaw No. 2947, 2018. As of September 1, 2024, fees and charges for rental of indoor recreation facilities will be based on rates as listed in Fees and Charges Bylaw No. 2988, 2024. Unless authorized by Council, all fees applied to the rental uses of recreation facilities must adhere to the respective fees and charges bylaw.

In addition, although the Comox Valley Farmers' Market Association is a non-profit society, many of its members are for profit businesses which could lead to the perception that the City is assisting businesses. While section 25 of the Community Charter prohibits the assistance of a business, Section 21 allows for a municipality to enter into a partnering agreement for the provision of a service through a third party on behalf of a municipality. If Council approves a fee reduction to CVFMA it is recommended that the City enter into an agreement outlining the financial assistance the City is providing and the service deliverables required of CVFMA in exchange for the assistance. The agreement would acknowledge CVFMA's role in actioning the City's OCP, 2022 objectives and policies as they relate to farmers' markets and food policy and other applicable OCP policy areas.

For 2022 and 2023 winter market seasons, CVFMA paid approximately \$8,862 for each winter market season (based on 24 bookings at the \$369.25 daily room rate per Bylaw 2947, 2018) in facility rental costs (excludes additional facility charges, such as custodial). Based on the unit cost that the City incurs to operate and provide the Native Sons Hall – Grand Hall, the total full cost of providing the space (based on 24 bookings at seven hours per booking) is estimated to be \$19,740 annually, resulting in a tax funded subsidized portion of \$10,878.

As with other rental groups seeking financial support from the City, staff encourage organizations to seek grant funding wherever possible. The City has entered into a Memorandum of Understanding with the Comox Valley Community Foundation to administer the City's grant-in-aid funds through the Foundation's grant process. Staff have confirmed with the Foundation that facility rental costs are eligible costs when considering grant applications in their priority areas.

Increased Demand for Community Space

With the growth of the community as well as pandemic recovery well underway, there is increasing demand for access to affordable and accessible community space. Courtenay Recreation is experiencing significant program growth and is managing multiple competing demands for access to space for both City programs and services, as well as rental uses and users. Additionally, the City is being called on to provide access to recreation facilities to meet other community needs such as extreme weather response initiatives including cooling and warming centres.

Through the Strategic Cultural Plan Community Engagement process arts and culture user groups shared concerns regarding the lack of available facilities to support their activities and identified a need for accessible and affordable indoor space to support the arts and culture sector in Courtenay.

The Native Sons Hall – Grand Hall is a large gathering space, well suited to meet the diverse needs and demands of community groups and recreation programming that must occur indoors, especially during winter months. Should CVFMA identify an alternative location to hold the winter farmers' market, the Grand Hall could meet this unmet demand.

Other Considerations

As identified in the OCP, 2022 and through discussions with CVFMA, the identification of a permanent, year-round location for the farmers market, preferably in or near downtown Courtenay is a long-term priority and strategic objective. There is an opportunity to engage CVFMA in upcoming land use and park planning processes such as the Harmston / Downtown Local Area Plan and Harmston Park Plan to explore the identification of a permanent location for the farmer's market and build considerations into the planning processes, including looking at a year-round outdoor location.

CVFMA currently moves the farmers' market to the Comox Valley exhibition grounds each April and remains there until October. CVRD staff have indicated they are exploring improvements to the grounds that may make it viable as a year-round site, however CVFMA has expressed that the lack of public transportation to the exhibition grounds does pose limitations from an accessibility perspective. There is strong preference to have a year-round market at one location as the changing locations from spring/summer to fall/winter and are disruptive to the public and vendors.

CVFMA has stated it would likely take several years to return to operating the Wednesday Downtown Summer Market due to financial viability concerns. Over and above the financial reasons, the location currently lacks the infrastructure required to support a summer market (electrical and water), combined with the lack of shade, these factors are contributing to why CVFMA has decided not to operate the Wednesday Downtown Summer Markets.

Options Available to the City

Based on comparative research, OCP direction, and strategies available through the Recreation Fees and Charges Framework, staff are proposing that Council consider the following options to address CVFMA's fee

reduction request. In light of upcoming planning processes, staff are recommending an option that provides a greater level of tax funded subsidy than outlined in the Fees and Charges Bylaw No. 2988, 2024 for not-for-profit commercial uses for a limited term to allow time for staff and CVFMA to work collaboratively to potentially identify a permanent, year-round location.

Financial summaries are provided for each option, however it is important to note that these are estimates and actual rental fees may be higher or lower due to requested additional services such as custodial services or other services that are recovered on a cost recovery basis, number of bookings, and or number of hours booked.⁶

Option 1: Apply the facility use category rate of Not-for-Profit Commercial Use as per Bylaw No. 2988, 2024

The bylaw to establish recreation facility rental and use fees was amended in April, 2024 and the new fees will be effective September 1, 2024. As per the bylaw, not-for-profit commercial uses are to pay rental fees at a 100% cost recovery rate in light of the commercial aspects of the use of space. Due to the increase in fees from current rates, the bylaw outlines a phased implementation of the increase in fees over five years. As such, groups in this category will continue to receive tax funded subsidy until the fees reach the 100% cost recovery target in 2028.

Staff understand that this option is not a sustainable or viable option for CVFMA as they have indicated they are actively managing increasing expenses and looking for ways to reduce costs. CVFMA would need to consider increasing their vendor fees to cover the increased rental costs, seek a lower cost alternative venue to hold the fall and winter market, and/or reduce or cancel the fall/winter markets.

The financial implications of Option 1 are as follows:

- a. Approximate total of three-year rental charges: \$41,440
- b. Approximate total of three-year subsidization of unit costs: \$18,955
- c. Approximate annual increase in rental charges as compared to current rate: \$2,338.25 in 2024, \$5,418.00 in 2025, and \$7,098.00 in 2026.

Option 2: City and CVFMA to enter into Partnering Agreement and City to apply the facility use category rate of Not-for-Profit Adult as per Bylaw No. 2988, 2024

In assessing CVFMA's request in consideration of the Recreation Fees and Charges Framework and the City's food security objectives, financial assistance could be provided through entering into a Partnering Agreement with CVFMA and applying the not-for-profit adult facility use category rate to CVFMA's rental of the Grand Hall. A three-year partnering agreement, with two additional one-year renewal options subject to CVFMA meeting service and reporting deliverables, would enable the City to provide financial assistance to the CVFMA by recognizing the community benefit as equivalent to the adult not-for-profit category. This timeframe provides CVFMA time to participate in the City's planning processes and explore other possible alternatives for a year-round permanent farmers market.

The purpose of the partnering agreement is for CVFMA to continue to operate a winter farmers' market once a week at Native Sons Hall Grand Hall and in the designated area in the adjacent parking lot from mid-October

⁶ For calculation purposes, the number of annual bookings used is twenty-four based on the actual bookings made by CVFMA in 2023. The rental rate applied is the hourly rate as the average booking time for each Saturday market is seven hours which is below the threshold (nine hours) at which staff apply the daily rental rate for the Grand Hall (note that this threshold has changed under Fees and Charges Bylaw No. 2988, 2024).

to mid-April, to support food security, support access to healthy food options, educate the community on agricultural issues and support local economic development in alignment with the City's OCP objectives and policies and CVFMA's priorities.

Examples of the Partnering Agreement service and reporting deliverables include:

- CVFMA would participate in the City's current land use planning initiatives: Harmston Local Area Planning, Harmston Park planning, and Downtown Playbook/Duncan commons initiatives. Through the planning initiatives, CVFMA would collaborate with the City to explore other locations for a permanent year-round market including the infrastructure required to support these markets.
- CVFMA to report back to Council annually on the qualitative and quantitative impact of the farmers market: attendance, vendors, nutrition program coupons, educational workshop, economic impact and community partnerships.
- CVFMA participates in regional food security planning and initiatives through engagement with the agricultural community and food policy council.
- CVFMA continues to seek external sources of funding (i.e. grants to help support a permanent farmers' market and the infrastructure required).

By pursuing this option, the City is recognizing the public benefit that the farmers market provides to the community and in return for CVFMA assisting in the delivering the OCP, 2022 objectives and policies for farmers' markets, the City is willing to provide additional financial assistance or subsidy.

The financial implications of Option 2 are as follows:

- a. Approximate total of three-year rental charges: \$29,587
- b. Approximate total of three-year subsidization of unit costs: \$30,810
- c. City and CVFMA to enter into Partnering Agreement
- d. Approximate annual increase in rental charges as compared to current rate: \$481.25 in 2024, \$1,155 in 2025, and \$1,365 in 2026.

Option 3 City and CVFMA to enter into Partnering Agreement and City to provide a grant to CVFMA to reduce the fees beyond those outlined in Bylaw No. 2988, 2024:

Council can also consider providing a grant to CVFMA to further reduce the rental fees beyond those outlined or available in Bylaw No. 2988, 2024. This option would be executed via a partnering agreement subject to CVFMA meeting the same service and reporting requirements identified in Option 2, with the addition of maintaining programming at the markets. In this option staff would recommend a shorter term of three years to encourage CVFMA to explore the development of a permanent year-round location and or build into their operating model increasing fees in future years.

Council could consider the following grant options:

- Option 3(a): Provide a grant in an amount that results in CVFMA continuing to pay the Community Daily Rate as per Bylaw No. 2947, 2018. The grant would reflect the difference between the Community Daily Rate and the not-for-profit adult rate listed in Bylaw No. 2988, 2024. This grant would be approximately \$3,000 total over a three-year period.
 - In option 3(a) the City would be increasing the tax funded subsidization along the Benefits Based Continuum that would value the community benefit of farmers' markets as exceeding the community benefit provided by other adult non-profit organizations, but not exceeding the community benefit associated with minor, senior, or Indigenous non-profit organizations.

- This option may be perceived as being over and above what the City would provide to another not-for-profit commercial user through the subsidy levels established through policy (Recreation Fees and Charges Framework and the newly adopted Fees and Charges bylaw).
- Option 3(b): Provide a grant in an amount that results in a **further** reduction of fees for CVFMA from what they are currently charged under Bylaw No. 2947, 2018. As per the request of CVFMA, the total grant amount requested in order to support its current level of staffing and programming would be \$5,000 per year.
 - In option 3(b) Council could direct staff to provide a grant in an amount that would result in the effective reduction of fees from the fees paid by CVFMA in the 2023 winter market season under Bylaw No. 2947, 2018.
 - As per the discussion of option 3(a) a grant of this amount would suggest that the community benefit provided by CVFMA is greater than adult non-profit organizations, and potentially also youth, senior, and Indigenous non-profit organizations, exceeding the subsidization levels defined in the Fees and Charges Bylaw No. 2988, 2024.

The City may consider these greater levels of subsidy in consideration of CVFMA's contribution towards food security objectives, which are policies within the City's OCP, 2022.

The financial implications of Option 3 are as follows:

- a) Option 3(a): Provide a grant in an amount that results in CVFMA continuing to pay the Community Daily Rate as per Bylaw No. 2947, 2018. Grant amount to reflect the difference between Community Daily rate and the not-for-profit adult rate listed in Bylaw No. 2988, 2024:
 - a. Approximate total of three-year rental charges: \$29,587
 - b. Approximate total three-year value of the grant: \$3,000.
 - c. Approximate total subsidization of unit costs: \$33,810 (total of grant and facility subsidization costs)
 - d. Approximate net (after grant) total of three-year rental charges: \$26,586
- b) Option 3(b): Provide a grant in an amount that results in a further reduction of fees for CVFMA from what they are currently charged under Bylaw No. 2947, 2018:
 - a. Approximate total of three-year rental charges: \$29,587
 - b. Total three-year value of the grant: \$15,000 (\$5,000 per year)
 - c. Approximate total subsidization of unit costs: \$45,810 (total of grant and facility subsidization costs)
 - d. Approximate net (after grant) total of three-year rental charges: \$14,586

Impacts

In discussion with CVFMA, CVFMA has indicated that in all options, except 3(b):

- CVFMA would not be able to hold the Downtown Wednesday Summer Street Market for several years until it is financially viable.
- There will be a reduction in market programming and events.
- CVFMA's participation and collaboration in regional food security planning and initiatives with the agricultural community and food policy council may be reduced.

CVFMA has indicated that options one and two include the potential that the number of winter markets may need to be reduced or cancelled. This would affect farmers' market customer access to local fresh produce including those who utilize the Farmers' Market Nutrition Coupon Program

Under 3(b) CVFMA would still not be able to hold the Downtown Wednesday Summer Street Market for several years until it becomes financially viable but the grant would provide the financial support for CVFMA to get the Wednesday Downtown Summer Market's operational again sooner and CVFMA would be able to

maintain its current level of staffing and programming at the winter market. CVFMA did note in their correspondence that a total grant amount of \$10,000 would be required in order to restart the Wednesday Downtown Summer Market next year. They will be actively seeking grants from other sources to also restart the Wednesday Downtown Summer Market.

The following table provides a high-level summary of the options discussed:

Table 5: Options Summary

OPTIONS:	OPTION 1: Not-For-Profit Commercial Use Rate per Bylaw No. 2988, 2024	OPTION 2: Not-For-Profit Adult Rate per Bylaw No. 2988, 2024 (Recommended Option)	OPTION 3: 3(a) – Maintain current fees via grant, Community Rate, Bylaw No. 2947, 2018 3(b) – Fee Reduction via grant
Total Facility Rental Fees (3 years)	\$41,440.25	\$29,587.25	\$29,587.25
Total Facility Unit Costs (3 years)	\$60,396.00	\$60,396.00	\$60,396.00
Total Value of Subsidization to CVFMA (3 years)	\$18,955.75	\$30,810.00	Option 3(a) \$33,810.00 Option 3(b) \$45,810.00
Agreement Required	No	Yes – 3-year term with two one-year renewal options	Yes – 3-year term
Value of Grant (3 years) included in the Subsidization Total Above	\$0	\$0	3(a) \$3,000.00 3(b) \$15,000.00
Allows time to explore a location for a permanent year-round market location	Yes – CVFMA would be an interest holder in the City’s planning processes.	Yes – through agreement	Yes - through agreement
Allows CVFMA to continue to operate the fall and winter market at the Native Sons Hall	CVFMA would either have to decrease market days or cancel the market and or reduce programming.	CVMA may have to reduce the number of winter markets and reduce programming.	3(a) CVFMA may need to reduce staff and modify some programming and food policy planning work. 3(b) CVFMA to maintain currently level of staffing and programming.
Return to holding the Wednesday Downtown Summer Market	Not likely until it’s financially viable.	Not for several years until it’s financially viable.	In both 3 (a) and (b): not for several years but 3 (b) would allow for the Wednesday Downtown Summer Market to start up again, sooner.
Within Bylaw 2988, 2024 Rate Categories	Yes	Yes. Fees and Charges Framework allows for flexibility to increase	No. However, Fees and Charges Framework allows for flexibility to increase

		subsidy based on high public benefit.	subsidy based on high public benefit.
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Recommended Option

Staff are recommending Option 2: that the City and CVFMA enter into a Partnering Agreement and the City apply the facility use category rate of Not-for-Profit Adult as per Bylaw No. 2988, 2024. Option 2 allows sufficient time for CVFMA to explore alternative locations for a year-around permanent farmers' market, treats CVFMA equally to other adult not-for profits, and is within the policy and bylaw approved by Council. This option allows CVFMA to participate in City land use and park planning processes that will be initiated in 2024 and that present an opportunity to achieve a common vision for a permanent year-round farmers' market that may also have adequate supporting infrastructure in place.

Providing a grant to further reduce rental fees exceeds the subsidization levels as recently defined in the Fees and Charges Bylaw No. 2988, 2024 and would suggest that the community benefit provided by CVFMA is greater than other adult non-profit organizations, and potentially also youth, senior, and Indigenous non-profit organizations. Council may consider these options in light of the CVFMA's role in supporting the achievement of food security objectives and policies as outlined in the OCP, 2022.

By supporting CVFMA in exploring other locations for a year-round permanent location, the City will be looking at possible long-term options to freeing up available space in the Native Sons Hall - Grand Hall. This would help support other local non-profit organizations seeking space for their activities and or provide much needed space for Courtenay Recreation to meet recreation program demands.

POLICY ANALYSIS:

Community Charter

Section 25 of the Community Charter prohibits assistance to a business. According to the Community Charter Schedule of Definitions and Rules of Interpretation, "business" means:

- (a) carrying on a commercial or industrial activity or undertaking of any kind, and
- (b) providing professional, personal or other services for the purpose of gain or profit

Section 21 allows for a municipality to enter into a partnering agreement for the provision of a service through a third party on behalf of a municipality.

OCP, 2022

Food Security Chapter:

- Objective 1: All residents have access to affordable, healthy, culturally appropriate, and local food outlets within walking distance, including food access services and programs when needed.
 - o FS 1: Support establishment of small-scale healthy food retail options such as both year-round and seasonal farmers markets, small to mid-size grocery stores, mobile food vendors, bakeries and restaurants within or in proximity to the Town and Neighbourhood Centres and Corridors including the Downtown Town Centre.
 - o FS 2: Work with the Comox Valley Farmers Market Association to secure a permanent site and facility for its year-round operation in a suitable and accessible location with necessary supportive amenities, preferably near the downtown.
 - o FS 3: Ensure adequate space and amenities such as loading zones, washrooms, water and electricity are provided whenever a farmers' market is planned.

In addition to Food security, support for Farmers Markets aligns with objectives and policies in the Local Economy, Social Infrastructure, and Arts, Culture and Heritage chapters.

Fees and Charges Amendment Bylaw 2988, 2024 – Recreation User and Rental Fees

The Fees and Charges Amendment Bylaw 2988, 2024 authorizes Council to impose fees and charges for rental uses of recreation facilities. Staff do not have the authority to waive the fees established by bylaw.

FINANCIAL IMPLICATIONS:

Options to reduce the rental fees charged to CVFMA from Bylaw 2988, 2024 have financial implications for the City.

- Option 2 (recommended): The reduction of rental charges through categorizing CVFMA's facility use in the adult not-for-profit group will result in a foregoing of rental revenue for Recreation, Culture, and Community Services (RCCS).
 - The foregone revenue is estimated at \$11,853 over a three-year period however the actual value will vary based on number of days and hours booked by CVFMA.
- Options 3 (a) and (b): The reduction of rental charges through the provision of a grant would:
 - Result in a foregoing of rental revenue for RCCS as per option 2; and
 - Require the allocation of grant funding from Gaming Funds for a term of three years.
 - Option 3(a) would require approximately \$3,000 in grant funding from Gaming Funds over a three-year term, with additional grants valued at approximately \$1,000 per year if the agreement is renewed for up to two more years (a total of five years).
 - Option 3 (b) would require \$15,000 (\$5,000 per year) in grant funding from Gaming Funds over a term of three years.

CVFMA will continue to utilize a portion of the Native Sons Hall parking lot for each Saturday market at no cost.

When CVFMA is able to operate the Wednesday Summer Downtown Market, there will be no cost to CVFMA for use of Duncan Avenue as in previous years.

ADMINISTRATIVE IMPLICATIONS:

The negotiation of the terms and conditions, as well as administration of the partnering agreement will be led by the Recreation, Culture and Community Services Department.

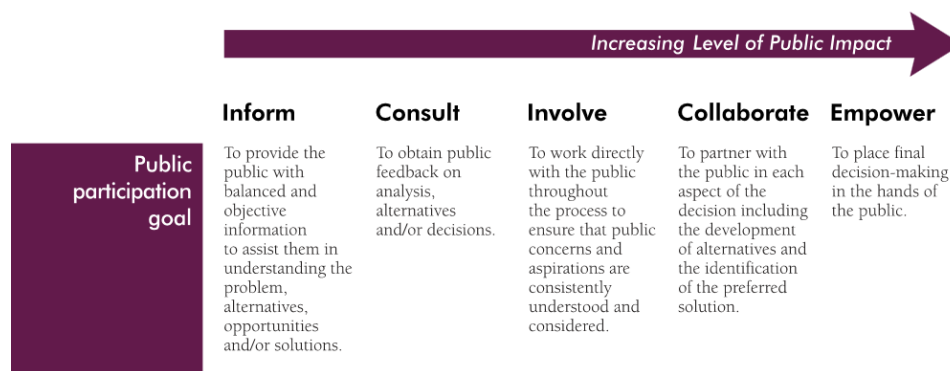
STRATEGIC PRIORITIES REFERENCE:

This initiative addresses the following strategic priorities:

- Food Systems - Identify roles for the City in the delivery of food security as outlined in the OCP
- Food Systems - Engage with Agricultural Community and Food Policy Council to identify needs and support relevant OCP policies

PUBLIC ENGAGEMENT:

Staff would inform the public based on the IAP2 Spectrum of Public Participation:



© International Association for Public Participation

Should a partnering agreement be entered into, proper notice would be required under section 24 of the Community Charter in advance of executing the agreement.

OPTIONS:

1. (Recommended)
 THAT Council approve Comox Valley Farmers' Market Association be charged the Adult Not-For-Profit Rate at the Native Sons Hall Grand Hall as established by the Fees and Charges Bylaw No. 2988, 2024; and
 THAT Council delegate authority to the Director of Recreation, Culture, and Community Services to determine the service and reporting deliverables and form and content of an agreement between the City of Courtenay and Comox Valley Farmers' Market Association for a three year term with two options to renew for an additional one year term each; and,
 THAT Council delegate authority to the Director of Recreation, Culture, and Community Services to execute any agreements related to the assistance both in-kind and financial provided to Comox Valley Farmers Market Association, subsequent to the publishing of notice.

2. THAT Council delegate authority to the Director of Recreation, Culture, and Community Services to determine the service and reporting deliverables and form and content of an agreement between the City of Courtenay and Comox Valley Farmers' Market Association; and
 THAT upon execution of the agreement, Council approve an annual grant to the Comox Valley Farmer's Market Association in the amount equivalent to the difference between the the Adult Not-For-Profit Rate at the Native Sons Hall Grand Hall as established by the Fees and Charges Bylaw No. 2988, 2024 and the Community Daily Rate as per Bylaw No. 2947, 2018 of \$369.25 for a three year term, ending March 31, 2027, to be sourced from Gaming Funds for meeting the service and reporting deliverables outlined in the agreement; and
 THAT Council delegate authority to the Director of Recreation, Culture, and Community Services to execute any agreements related to the assistance both in-kind and financial provided to Comox Valley Farmers Market Association, subsequent to the publishing of notice.

3. THAT Council delegate authority to the Director of Recreation, Culture, and Community Services to determine the service and reporting deliverables and form and content of an agreement between the City of Courtenay and Comox Valley Farmers' Market Association; and
 THAT upon execution of the agreement, Council approve an annual grant to the Comox Valley Farmers Market Association of \$5,000 per year for a three year term ending March 31, 2027, to be

sourced from Gaming Funds for meeting the service and reporting deliverables outlined in the agreement; and

THAT Council delegate authority to the Director of Recreation, Culture, and Community Services to execute any agreements related to the assistance both in-kind and financial provided to Comox Valley Farmers Market Association, subsequent to the publishing of notice.

4. THAT Council provide alternative direction to staff.

ATTACHMENTS:

1. Attachment 1: A Sample List of Winter Markets in British Columbia

Prepared by: Joy Chan, Manager of Business Administration, Recreation, Culture, and Community Services

Joanne Bays, Community Development Coordinator, Recreation, Culture, and Community Services

Reviewed by: Susie Saunders, Director of Recreation, Culture, and Community Services

Concurrence: Geoff Garbutt, M.P.I., MCIP, RPP, City Manager (CAO)

**Attachment 1: A Sample List of Winter Markets in British Columbia
BCAFM website, May 2024**

City	Market Location	Key Supporting agency
Armstrong	Oddfellows Hall (Indoor and outdoor space weekly Oct - April)	Community NPO
Ashcroft	Cache Creek Community Hall (Indoor 1x/month Nov – Dec)	Community NPO
Barrier	Barrier Seniors Centre (Indoor Weekly Oct – Dec)	Community NPO
Creston	Creston & District Community Complex (Indoor Nov – Dec, weekly)	Regional District
Comox Valley	Native Son’s Hall, Courtenay (Indoor, weekly Oct - March)	Municipality
Duncan	Downtown Duncan (Outdoor, weekly, year around. Streets blocked off, shelter and utilities provided)	Municipality
Esquimalt	Esquimalt Recreation Centre (indoor weekly Oct – Dec)	Municipality
Ft Nelson	Elks lake Lodge (indoor weekly, year around)	Community NPO
Ft St John	Pomeroy Convention Centre (1x in the winter)	Municipality
Gabriola	Gabriola Ag Assn and Co-op’s Agi Hall (indoor and outdoor, weekly year round)	Community NPO
Golden	Mt 7 Rex Plex (Indoor, 1X/month Jan – April)	Municipality
Grand Forks	Gyro Park (Indoor, weekly – Oct - March)	Municipality
Kamloops	Mt Paul Community Food Centre (Indoor – weekly Nov – Feb)	Community NPO
Kelowna	Parkinson Rec Centre (Indoor weekly Nov - March)	Municipality
Kimberley	TBD. (Indoor two days only)	TBD
Masset	Outdoor, weekly year around, street blocked off, shelter and utilities provided	Local government

Nakusp	Old Fire Hall (Indoor, weekly Oct – May)	Community NPO
Nanaimo	Centennial Building (Indoor weekly Oct – May)	Community NPO
Penticton	Cherry Lane Shopping Centre (Indoor, 2x/Month Nov and Dec)	Business Community
Port Alberni	Outdoor, weekly year-round. Square made available, shelters, utilities	Municipality
Prince George	Corner of 3 rd and Quebec (Outdoor in the summer, indoor in the winter)	Business and NPO Community
Port Mc Niel	Outdoor, year-round, every other week, shelter and utilities provided	Municipality
Port Moody	Port Moody Recreation Complex (Indoor, weekly Nov – Apr)	Municipality
Powel River	Outdoor weekly, year-round, in park like setting offered by the City. Shelter and Utilities provided	Municipality
Qualicum Beach	Indoor and outdoor in winter. Operates weekly all year round	Municipality and Community NPO
Salmon Arm	Outdoor downtown, weekly, year around. Streets blocked off. Shelter and utilities provided	Municipality
Squamish	Outdoor downtown, weekly year round. Streets blocked off, shelter and utilities provided.	Municipality
Vancouver	Hastings Park (Outdoor, weekly Nov – Apr) On fairgrounds With Shelter and Utilities)	Municipality
Vancouver	Riley Park (Outdoor, weekly Nov – Apr. In Park, with Shelter and Utilities)	Municipality
Victoria	Moss Street (Indoor, weekly in the winter in the Garry Oak Room of the Fernwood Community Centre and nearby Sir James Douglas School Gymnasium	Community NPO School District



HOPE AIR



GIVE HOPE
WINGS



Support the trailblazing Give Hope Wings event

This is your chance to fly with a dedicated group of Canadian pilots and make a lasting difference in the lives of patients who must travel long distances for vital medical care.

Give Hope Wings 2024 offers three unique regional expeditions offering a mix of adventure and impact through connection to our patient communities in June 2024. The multi-aircraft series visits locations throughout Canada, raising awareness and funds for Hope Air.

Founded in 2018, Give Hope Wings has raised more than \$2.3 million.

Get involved!

Learn more

hopeair.ca/GHW2024

Mayor & City Council
City of Courtenay
830 Cliffe Ave
Courtenay, BC V9N 2J7

29 May 2024

**Request to Erect a Marquee near the Courtenay Marina
In Support of “Give Hope Wings” Expedition – Saturday, 15 June 2024**

Dear Mr. Mayor and City Council,

“Give Hope Wings” is an organization made up of a dedicated group of pilots that launch annual air expeditions across Canada to raise funds for Hope Air, the only national charity and medical travel assistance program that offers free travel and accommodations for Canadians in financial need who must access medical care far from home. To date, “Give Hope Wings” has raised over \$2.3 Million for Canadians in need.

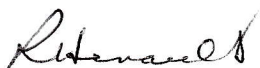
The expedition for Western Canada will be traveling through Courtenay on Saturday, June 15th enroute to several communities in northern BC, and has requested a one-night stopover for their aircraft at the Courtenay Airpark.

We are planning to greet the group (5 to 7 light aircraft) on arrival sometime in the mid-to-late afternoon of the 15th and would like to erect a marquee just outside the Airpark main gate, in the grassy area southwest of the Courtenay Marina, to provide a location for the group to interact with walkers on the Airpark Pathway or other interested members of the community. This would allow them to tell their story and accept donations from those who might wish to contribute to their cause for the few hours that they would be available.

Understanding that this is not normal practice under the Parks Control Bylaw, the Courtenay Airpark Association requests your approval to erect a marquee along the pathway on a one-time basis on June 15th to support the “Give Hope Wings” Expedition’s fundraising initiatives.

Your attention to this request is most highly appreciated.

Sincerely,



Raymond R. Henault
President
Courtenay Airpark Association

Phone: (250) 465-0086

June 5, 2024

Dear Mayor and Council,

We are seeking the City of Courtenay's support for **the designation of covenant be removed from our properties** which were affected by the unlawful actions in December of 2021 by Above & Beyond Tree Removal Service and SMG Management Ltd. d/b/a/ Japonica Park Trailer Park Management Company.

Below is information for you as to what has occurred and what we have been left to deal with (photos attached):

- December 2021, the guilty parties had **unlawfully removed trees located on the covenant area of our properties** without the proper permits from the City of Courtenay and/or permission of the property owners
- both parties were ticketed and fined by the City of Courtenay
- limbed trees were left standing with ropes still hanging from them along with rusty metal fence wire creating **safety hazards**
- the piles of very dry debris and standing dead trees left behind by the contractor for over two years are extremely close to our homes. They are a potential **wild fire hazard** and are of extreme concern to us, especially with the forecasted dry weather and fire season!

We are asking for your support in removing the covenant on Lambert Drive based on the following:

1. The Fisheries and Oceans Canada (DFO) have closed their investigation and will not be pursuing remedial works on the affected properties (attachment page 2).
2. Some sales of property have lost value because of the covenant.
3. There appears to be no uniformity of covenant use and size along Lambert Drive.

4. The Union of BC Municipalities (UBCM) also provides some insight on the impact of redundant covenants (see attachment pages 4 and 5).

Thank you for your consideration in reviewing this most distressing matter.

Yours very truly,

Linda and Terry Hamilton, 2074 Lambert Dr.
Charles and Laurie Beek, 2038 Lambert Dr.
Bill and Cathie Matthews, 2086 Lambert Dr.
250 871-1857, bmatthews941@gmail.com
Bill and Hazel Eliason, 2100 Lambert Dr.

Pagliericci, Gino

From: Pagliericci, Gino
Sent: Tuesday, June 21, 2022 1:43 PM
To: Pagliericci, Gino
Subject: City of Courtenay Message in Regards to the Investigation in regards to the tree cutting in the Covenant area

Hello All,

I was directed to provide you with a quick update on the investigation into the tree cutting in your backyards:

- 1). The City has completed its investigation and has charged two firms [REDACTED] tickets each. [REDACTED] per ticket;
- 2). Department of Fishery Office (DFO) is completing its investigation and will issue restoration orders if the creek is fish bearing to the individuals responsible;
- 3). The city is awaiting a response from the Provinces Natural Resource Officers, as they are also doing their own investigation;
- 4). A further written update from the City of Courtenay regarding restoration will be provided once we have more information from the DFO and the Province.

Thank you for your patience in this matter, as you can see it is a very complicated investigation conducted by several different organisations.

Yours truly,

Gino

Gino Pagliericci
Bylaw Enforcement Officer
City of Courtenay



830 Cliffe Ave.
Courtenay, BC V9N 2J7
Office: 250-334-4441 Ext: 7286
Cell: 250-207-7280
gpagliericci@courtenay.ca www.courtenay.ca

OUR CORE VALUES: **People Matter** | **Be Accountable** | **Depend on Each Other** | **Pursue Excellence** | **Celebrate Success**

I respectfully acknowledge that the land we gather on is on the Unceded traditional territory of the K'ómoks First Nation, the traditional keepers of this land



From: Gothard, Nancy ngothard@courtenay.ca
Subject: RE: Tree Removal Cleanup - [redacted] Lambert St.
Date: April 18, 2023 at 4:09 PM
To: O'Connell, Kate koconnell@courtenay.ca, Bill Matthews [redacted]
Cc: deGraaf, Peter pdegraaf@courtenay.ca

Hello Bill,

I am also sorry that this has happened to your property and that you are continuing to live with the unsightly premises.

I am challenged by finding a way to ensure that sensitive debris removal occurs and ideally restoration. As it is a riparian area there are stipulations around how the work is conducted.

I continue to have this on my list to find a solution or direction for you and your neighbours re: the clean up. I just don't have the answer at this moment. I will follow up as soon as I have a clear path.

I very much appreciate your continued patience.

Take care,

Nancy Gothard RPP, MCIP
Manager of Community and Sustainability Planning/ Acting Manager of Development Planning
City of Courtenay
(she/her) *Why I list my pronouns
I respectfully acknowledge that I live, work and play on the Unceded territory of the K'ómoks First Nation.

I must advise that due to high volume of work, competing needs, and temporary gap in capacity in this department, that I may personally not be able to read all my emails in a timely manner particularly in March & April. In the event of serious problem that requires quicker attention please direct your contact to our planning division administration at planning@courtenay.ca; or our Interim Director Rob Roycroft roycroft@courtenay.ca. Thank you for your patience and understanding.

Tel. 250 703 4831, ngothard@courtenay.ca
General planning inquiries may be sent to: planning@courtenay.ca

City Hall general number: 250 334 4441
830 Cliffe Ave | Courtenay, B.C. | V9N 2J7
www.courtenay.ca

OUR CORE VALUES: People Matter | Be Accountable | Depend on Each Other | Pursue Excellence | Celebrate Success

-----Original Message-----

From: O'Connell, Kate
Sent: Wednesday, April 12, 2023 8:54 AM
To: 'Bill Matthews' <bmatthews941@gmail.com>
Cc: Gothard, Nancy <ngothard@courtenay.ca>; deGraaf, Peter <pdegraaf@courtenay.ca>
Subject: RE: Tree Removal Cleanup - 2086 Lambert St.

Hello Bill,

I am very sorry that this has happened to your property and we will do what we can to assist you in navigating the remediation processes. I met with Nancy last week and she will be in touch with you regarding the options to address the debris trees including permits and the existing covenant.

Regarding your questions about the DFO, the DFO has advised the City that their investigation regarding the status of the waterway is closed as they have determined the waterway to be non-fish bearing and they will not be pursuing remedial works on the affected properties. The City's role, regarding the bylaw investigation has also come to an end as the investigation has concluded and the responsible parties have been ticketed under the authorities of the bylaw.

As private property, any remediation activities would be led by the property owner. Although I cannot provide you with any legal advice, in the event you choose to pursue legal action against the two businesses involved in the tree removal, we can provide you through our Access Request process any investigation documentation that may be needed. Should you wish to file an Access Request, please contact the City's Corporate Officer, Adriana Proton at aproton@courtenay.ca

Regards,

Kate O'Connell MPP (She/Her)
Director of Corporate Services

Corporate Support Services
 Bylaw Services
 830 Cliffe Avenue
 Courtenay, B.C. V9N 2J7



Phone 250-334-4441
 Fax 250-334-4241
 info@courtenay.ca
 www.courtenay.ca

File No. 4020-20

July 15, 2022

Cathie-Lyn Matthews
 William George Matthews
 2086 Lambert Drive
 Courtenay, BC V9N 9C9

REGULAR MAIL

Dear Mr. & Mrs. Matthews,

Update re: Unlawful Tree Cutting Investigation in the Vicinity of 20th Street and Lambert Drive, Courtenay, B.C.

As an affected property owner of the unlawful tree cutting activities that took place near Lambert Drive in December of 2021, this letter is to provide you with an update on the City's investigative process and next steps. We have not found any evidence that as the property owners of the subject property you were involved in the illegal activities. It was/is **not** the City's intent to penalize property owners who did not have a role in the unlawful tree cutting.

Upon conclusion of the investigation, it was determined that trees were unlawfully cut within a riparian area of the Piercy Creek watershed, and protected by covenants which required that it remain in a natural state. Having identified the responsible parties, the City of Courtenay issued 107 tickets to a contractor and a further 107 tickets to a property owner company for cutting or damaging over 100 protected trees, contrary to the *Tree Protection and Management Bylaw*. Fines associated with the tickets total \$107,000 (\$1,000 per protected tree) for each party, as per the City's *Municipal Ticket Information Bylaw*. These matters are now before the courts and are likely to heard next year.

In terms of site restoration and ecological preservation of the Tributary of Piercy Creek, the Department of Fisheries and Oceans (DFO) have conducted an initial investigation and are in the process of sending a biologist to the site. A timeline has not been forwarded to the City. The City has also notified the provincial Ministry of Forests natural resources officers who are responsible for the *Water Sustainability Act*.

Please do not hesitate to contact me directly at 250 334-4441 extension 7297, if you have any questions regarding this file.

Sincerely,

Peter de Graaf, CPHI(C), BA, MBA
 Manager of Bylaw Services

cc Kate O'Connell, Director of Corporate Support Services



Redundant Covenants on Property

Year: 2010

Number: B141

Sponsor(s): Maple Ridge

WHEREAS redundant covenants on title can contradict a local governments zoning bylaw and negatively impact development potential for private owned property; AND WHEREAS approvals for release of the charges by the signatories to the charges can be difficult to obtain and local governments are not authorized by the Land Title Act, the Community Charter or the Local Government Act to release the charges from title: THEREFORE BE IT RESOLVED that the Union of BC Municipalities lobby the provincial government to amend the Land Title Act to allow local governments to apply to the BC Land Title Survey Authority for the release of the charges from title.

Provincial Response

Ministry of Forests, Mines Lands It is assumed that the term redundant covenants as used in the UBCM resolution refers to privately held restrictive covenants whose terms have become obsolete. Although restrictive covenants registered on title to a property may, in some cases, appear to contradict or limit a local governments land use planning goals or zoning provisions applicable to a particular property, they cannot override anything prohibited by a local governments bylaws. However, restrictive covenants are enforceable as private contracts that attach to the land and bind successive owners of the land beyond what would otherwise have been permitted under local government bylaws. Options currently exist to allow the

removal of restrictive covenants from title. If a covenant is found to be unreasonable in terms of its impact on the persons having an interest in the land involved, the Supreme Court may order the covenant to be cancelled or modified under section 35 of the Property Law Act once the required criteria have been met to the satisfaction of the Court. Note that a local government would not likely have standing to make an application under this section unless it held an interest in the land that was subject to the covenant, which is uncommon. In cases where a local government is the beneficiary of a statutory covenant i.e. one that is registered under section 219 of the Land Title Act, the local government can apply unilaterally to the Land Title Office to have the covenant released without the consent of the party who granted the covenant in the first place.

Convention Decision

Endorsed

[Home](#) > [Convention & Resolutions](#) > [Resolutions](#) > [Resolutions Database](#)
> [Redundant Covenants on Property](#)







Kumugwe Cultural Society
3240 Comox Rd Courtenay BC, V9N 3P8

City of Courtenay
830 Cliffe Avenue,
Courtenay, B.C.
V9N 2J7

May, 2024

Dear Mayor Wells and Council,

Recently, it has been brought to the attention of the Kumugwe Cultural Society (KCS) board, that the Lewis Park totem poles within the Courtenay municipality are in dire need of refinishing. We are aware that the totem poles are ultimately the responsibility of the City of Courtenay, however, KCS would like to explore options on how we can collaborate to preserve the totem poles for many generations to come.

We feel this is an opportunity to continue fostering the long standing positive relationship our organizations have shared for over fifteen years.

Thank you for your time and I look forward to hearing from you.

Gilakas'la,

A handwritten signature in blue ink, consisting of several loops and a long horizontal stroke extending to the right.

Lee Everson
Administrator
Kumugwe Cultural Society
lee.everson@shaw.ca



Briefing Note

To: Council

File No.: 1845-20-SCS

From: Director of Recreation, Culture, and Community Services

Date: June 12, 2024

Subject: Strengthening Communities' Grant Update

PURPOSE:

To update Council and local partner governments on the progress made and key outcomes resulting from the Strengthening Communities' Grant. This report covers the period from January 1 – March 31, 2024.

BACKGROUND:

In 2021 the Comox Valley Regional District (CVRD) was awarded a regional grant of \$1.093 million through the Strengthening Communities' Grant Program to better support people experiencing homelessness in the Comox Valley. The City is administering those funds on the region's behalf. The project funding was set to conclude on June 30, 2023.

At the May 31, 2023 Council meeting staff informed Council that a request had been made with local government partners for additional funding to support a one-year extension of the Strengthening Communities' Grant. The funding request was to cover activities previously approved under the SCG that could not continue without another source of identified funding. This included staff and security at Connect for daytime services, peer outreach, community engagement, training, and rural outreach.

At the September 13, 2023 Council meeting staff informed Council that the extension had been approved and that the City would receive \$898,603 to fund approved services through June 30, 2024. This brought the total funding approved to \$1,961.763.

At the March 13, 2024 Council meeting, staff informed Council that funding had been received and that staff had amended service agreements with principal partner agencies to reflect the new term and funding services through to June 30, 2024 to support the initiation and/or continued implementation of grant funding activities.

DISCUSSION:

The Strengthening Communities' Grant (SCG) supports the following activities and it is important to note that funding is only eligible for the following approved grant activities:

- **A Daytime Community Access Hub** that operates six days a week providing services focused on health, hygiene, personal safety, and administrative support to those in need. Funding supports security services and the creation/facilitation of a Community Advisory Committee.
- **Rural outreach** on Denman and Hornby Islands to facilitate connections to housing and support services.

- **A peer-based outreach team** to provide outreach and conduct cleaning and remediation activities around locations where support services are provided.
- **Community engagement and anti-stigma** outreach activities, including expanded community meal events hosted at Sunday Service for residents, local businesses and people experiencing homelessness.
- **Training and capacity building** activities across municipalities for local government and First Nation employees, politicians, outreach workers and volunteers focusing on trauma-informed

Update since last report

In this period the City continued to support and to administer SCG funds to three principal partners – Comox Valley Transition Society (CVTS), Sunday Station (SS) and Hornby and Denman Community Health Care Society (HDCHCS). These agencies continued to provide a number of essential services to people who were experiencing homelessness, mental health issues and/or addictions – from referrals to physicians and other health and social service providers, to food, showers, outdoor gear and shelter. Remarkably, 84,616 interactions with workers in these service agencies, and/or the peer workers supported by these agencies, have been recorded – demonstrating the incredible impact these services are having and the significant need that exists in our community for those who are experiencing homelessness or the negative impacts of substance use or mental illness.

Milestones

- CVTS was awarded additional funds to hire more people with lived experience to participate in street clean-up crews. This enabled street clean-ups morning, afternoon, and evenings, seven days per week.
 - This funding was made available due to a change in service provider capacity over the course of the grant period.
- The Community Advisory Committee for Connect continued to meet regularly with representation from the RCMP, City (Bylaw Services and Community Services), CVTS, and VIHA.
 - The focus has been on the timely sharing of information, and the coordination of services to improve the health and well-being of people who depend on the services offered at Connect, as well as the surrounding community.
- The City's Recreation, Culture and Community Services division coordinated a new spring/summer series of free workshop and training opportunities for front line workers who are supporting or interacting with people experiencing homelessness, addiction or mental health challenges.
 - The current line-up of courses includes: *Responding with Respect on the Frontline, Introduction to Trauma Informed Practice, Let's Get REAL about Mental Health: Exploring Empathy, Mental Health First Aid Standard, Gender Diversity in the Workplace, Not Just Naloxone, The Power of Coercive Control, and Prevention First: Introduction to Crisis.*

- Through the SCG grant, organizations are also able to register for an annual membership with the Homelessness Services Association of BC, where they can access additional free training and resources.

While not directly related to the SCG, it is relevant to note that since the last briefing note BC Housing announced that land has been purchased and plans are underway to build a new supportive housing development, as well as a purpose-built shelter in Courtenay. These new services will make a significant impact in meeting the needs of those experiencing homelessness in our community, which is critical as the SCG funding comes to an end June 20, 2024. The statistics and reporting from through the SCG program undoubtedly contributed to the successful advocacy efforts that resulted in this housing investment in Courtenay.

Metrics (January 1, 2024 to March 31, 2024)

Qualitative Feedback

Impact Statement – Connect

- *‘I’m motivated to continue to do this work because I like working with people and believe that we all have a responsibility to support the most vulnerable individuals in our society. I believe that Connect is an essential part of this community, and were it not for the tireless efforts of our team I have no doubt at all that many, many, people would have died from overdoses or other adverse health effects resulting from homelessness. Though options for people to exit homelessness into affordable housing are very limited we do the best that we can to support the people who depend on Connect’.* Connect Staff.

Impact Statement – Recreation, Culture and Community Services, City of Courtenay

- *“Trauma informed training programs are really popular. The {Introduction to Trauma informed Practice} workshop is about trying to get people to gain an understanding about how trauma that people have faced in the past shapes how they behave as an adult. Trauma training can help reduce stigma. It can help people to understand it from a different lens, to take away stigma, whether it is fear or judgement or anything like that.”* - RCCS Staff.

Impact Statements – Hornby and Denman Community Health Care Society

- *“Having a weekly, consistent presence at outreach spaces has created a friendly, familiar, approachable face for our service in the community. Being present and supporting people in these meaningful spaces opens the door to be approached by community members afterwards to initiate more in-depth and private supportive conversations about mental health and substance use.”* - HDCHCS Staff.
- *“Prior to our providing outreach support at the Hornby Kitchen and Denman Island Foodbank, this work was being done informally by the staff and volunteers at these spaces, who were simultaneously trying to provide their own services. These workers have seen the*

need for professional outreach services and are grateful for our presence to provide practical and mental health support to service users.” – HDCHCS Staff.

- *“My position is that of a constant liaison between people and services. As a community-based service, I can cast a wide net to reach community members who may not access help through more conventional means. These participants can then begin to access the necessary services, resources, and care providers. I also act as an integral link in connecting one resource provider to another, allowing more wrap-around care for marginalized participants.” - HDCHCS Staff.*
- *“I just need to talk to someone who isn’t in my immediate circle of drama.” - Hornby Island Participant.*
- *“I’m starting to feel joy again for the first time in a long time!”- Denman Island Participant.*

Impact Statements -Sunday Station

- Photos of fresh food donation and hot meals prepared by volunteers.
- *“The effort and grace that Sunday Station volunteers put into each Sunday really shows. Our community relies heavily on this team to provide a nutritious meal to those in need.” - Community Volunteer.*
- *“Thank you for everything you do. Without this program, some of us wouldn’t eat.”- Participant.*
- *“Each Sunday a lot of us look forward to joining together at the train station. It provides a social activity for most of us.” – Participant.*
- *“I watched a short documentary about Sunday Station and it was so well presented to inform the community about Sunday Station. And an awareness about the people who are less fortunate and need community support.” Community Volunteer.*



Quantitative Measures

Sustainability Planning

With funding through the Strengthening Communities Grant set to expire June 30, 2024, staff have been in discussion with partners regarding the impacts of the termination of funding as well as plans for sustaining services following the end of the SCG program.

All principal partners state that they intend to continue to provide the services beyond the June 30, 2024 when SCG funding ends. Each has indicated that they have throughout the project, been able to utilize SCG statistics and funds to leverage additional funds which will allow for some level of service sustainability. That said, each service organization has also stated that the service levels will likely drop off to some extent after the June 30, 2024 date.

The services provided at Connect are likely to be the most significantly impacted by the cessation of funding through SCG. While the SCG funding was a key driver in CVTS securing BC Housing temporary shelter funding, BC Housing funding is limited to the operating costs related to a 24-

hour, 33 bed shelter, and does not contribute to the extensive day services provided to the broader community. Staff and CVTS are in discussions about the potential impacts and strategies for mitigation, however it is expected that the following will be significantly reduced or potentially eliminated completely:

- Peer-led street clean up: CVTS has indicated they may have the capacity to maintain a morning peer led clean-up crew, however they will not have funding to continue with the provision of afternoon and evening street clean up. The City is working with CVTS to explore the extent to which community partners, such as the DCBIA might resource this activity. CVTS is also exploring grant opportunities.
- Food Provision: CVTS currently provides meals to approximately 55 people per day. They will be increasingly dependant upon donations to top up food service for guests above and beyond the 33 shelter guests whose breakfast and dinner meals are covered by BC Housing funding.
- Security: CVTS currently has two security persons for 24 hours everyday of the week. CVTS are in discussion with BC Housing to explore the extent to which this level of service can be maintained. Without additional funding, this service will likely be reduced.

As staff and partners prepare for the end of the Strengthening Communities Grant program, they are planning to come together to discuss project impact, lessons learned, communicating the results, and sustaining momentum where possible.

NEXT STEPS

This SCG update for the period of January 1, 2024 to March 31, 2024 will be sent to the Comox Valley Regional District (CVRD), Town of Comox and the Village of Cumberland as partner municipalities. RCCS staff will continue to support the administration of the SCG on the region's behalf.

The Strengthening Communities' Grant will sunset on June 30, 2024, after which an update to Council and local government partners, including the final report, will be shared. For monthly progress updates, information on our social service partners, contact information and the SCG training and capacity building initiatives please visit: [Strengthening Communities Grant | City of Courtenay](#)

RECOMMENDATION: THAT Council receive the Strengthening Communities' Grant Update briefing note.

Prepared by: Joanne Bays, Community Development Coordinator

Reviewed by: Susie Saunders, Director of Recreation, Culture, and Community Services

Concurrence: Geoff Garbutt, M.P.I., MCIP, RPP, City Manager (CAO)



The Corporation of the City of Courtenay

Staff Report

To: Council

File No.: 3360-20-2105/RZ000058

From: Director of Development Services

Date: June 12, 2024

Subject: Adoption of Zoning Amendment Bylaw No. 3037, 2023 (1590 Piercy Ave)

PURPOSE:

For Council to consider adoption of Zoning Amendment Bylaw No. 3037, 2023 (1590 Piercy Ave) to permit a nine-unit multi-residential development for the property legally described as That Part of Lot 3, Section 41, Comox District, Plan 4764, Shown Outlined in Red on Plan 1402 R.

BACKGROUND:

Council gave Third Reading to Zoning Amendment Bylaw No. 3037, 2023 (1590 Piercy Ave) on July 26, 2023 and passed the following resolution:

That Final Reading of the bylaw be withheld pending:

- a. *Registration of a Section 219 covenant to secure community amenity contributions of \$40,000, to be provided prior to issuance of a building permit; and*
- b. *Registration of an easement to permit drainage from the subject property on to the Island Corridor Foundation lands.*

Zoning Amendment Bylaw No. 3037, 2023 (1590 Piercy Ave) rezones the property from R-2 to CD-39 to allow a multi-unit housing development consisting of nine town house units at the subject property. The proposal is described in further detail in the July 26, 2023 Staff Report (*Attachment No.1*).

DISCUSSION:

The conditions from Council's resolution at the regular meeting held on July 26, 2023 have been met and Council can now consider adoption of the Zoning Amendment Bylaw No. 3037, 2023 (1590 Piercy Ave). The legal documents have been executed and will be registered after adoption of the bylaw by legal consul.

OPTIONS:

1. THAT Council adopt Zoning Amendment Bylaw No. 3037, 2023 (1590 Piercy Ave).
2. THAT Council provide alternative direction to staff through resolution.
3. THAT Council not proceed.

ATTACHMENTS:

1. July 26, 2023 Staff Report at Third Reading
2. Zoning Amendment Bylaw No. 3037

Prepared by: Nancy Gothard, RPP, MCIP Manager of Community and Sustainability Planning

Reviewed by: Marianne Wade, RPP, MCIP Director of Development Services

Concurrence: Geoff Garbutt, M.PI., MCIP, RPP, City Manager (CAO)



STAFF REPORT

To: Council

File No.: 3360-20-2105/RZ000058

From: Director of Development Service

Date: July 26, 2023

Subject: Zoning Amendment Bylaw No. 3037 – Third Reading Report – 1590 Piercy Avenue

PURPOSE:

The purpose of this report is for Council to Consider Third Reading of Zoning Amendment Bylaw No. 3037.

LEGAL DESCRIPTION:

That Part of Lot 3, Section 41, Comox District, Plan 4764, Shown Outlined in Red on Plan 1402 R.

BACKGROUND:

The zoning amendment application is to establish a new Comprehensive Development 39 (CD-39) zone, and rezone the property from R-2 to CD-39 to allow a multi-unit housing development consisting of nine town house units at the subject property shown in Figure 1.

Council gave First and Second Reading to “Zoning Amendment Bylaw No. 3037” (1590 Piercy Avenue) on June 14, 2023 to create a new Comprehensive Development 39 Zone (CD-39) and rezone the subject property to CD-39.

At the June 14 meeting, Council requested that Electric Vehicle (EV) charging infrastructure be installed for vehicular parking stalls. The applicant’s proposal is included in the discussion below.

A Public Hearing was held July 19, 2023, during which a letter of support was provided and a number of questions and concerns were raised by three members of the public. Staff provide clarification on technical matters in the discussion below. The public comment is attached to this report and minutes form part of the July 26, 2023 agenda.

Figure 1 – Subject Property



DISCUSSION:

Public hearing comments and concerns

Questions were raised, and answered, as follows:

1. Concern over traffic impact and traffic safety to intersections of Piercy Avenue and Cumberland Road and Piercy Avenue and 17th Street.
 - Staff comment, and clarification: The parking studies provided for the proposal did not examine the impact of the development to the identified intersections. During the review of the application, staff and other agencies did not identify a concern with the service or safety levels of either of these intersections at this time.
2. Parking proposal is inadequate
 - Staff comment: the parking proposal is for 1 stall per dwelling unit and 1 visitor parking for the 9 unit development for a total of 10 stalls. This is a lower parking ratio than the standard vehicular parking ratios outlined in the Zoning Bylaw which would otherwise require a total of 13 stalls. AS this is a CD zone, the city can modify parking requirements to achieve the OCP policies to support active transportation and Climate Change.
 - As such the reduction of three parking stalls was permitted, as the parking studies identified the ability for e-bikes to provide meaningful alternatives to vehicle use given the close proximity of the proposed development to the city centre. Each dwelling unit will be provided with a minimum of two dedicated, secured and electrified bike parking stalls. Electrified visitor bike parking will also be provided.
3. How is the application meeting City's tree requirements, how will these be enforced?
 - Staff comment: The City's Tree Bylaw sets expectations regarding retention of healthy mature trees and replacement of trees when removing trees on a property, and that a target of 50 trees per hectare be achieved.
 - Properties smaller than 4000 m² (approximately 1 acre) may meet their tree density target (TDT) through retention, replacement of paying cash-in-lieu into the Tree Planting and Replacement Reserve Fund. Half of the replacement trees to achieve the Tree Bylaw replanting requirements shall be native.

This property is approximately 2000 m², and has a TDT of 10 trees. The applicant is proposing to retain two mature trees and replant 32 more. Of these, three replacements will be required to be native species which will be achieved through the landscape plan. The landscape plan is registered on title as part of the form and character Development Permit, which will be obtained following a successful rezoning outcome. The City requires that securities be provided for all landscaping to ensure that it is installed as planned.

4. How is the proposal contributing to affordable housing?
 - Staff comment: Affordable housing is a key amenity identified for rezoning application amenity negotiations. The OCP provides guidance on amenity negotiations, identifying the preference for 15% of net increase in number of units to be price restricted. The OCP also provides monetary targets options in lieu or in tandem with price restricted units (or provision of any other amenities). The target contribution is \$4,000/new multi-residential

unit that may be placed into the City’s Affordable Housing Reserve Fund. The applicant has offered \$40,000 into this fund. Price restricted units are evaluated as not appropriate for a development of this scale.

The application is for townhouse units in two and three bedroom configurations. These are commonly referred to as ‘missing middle’ forms of housing which inherently contribute to a greater diversity and affordability range within the larger housing market.

5. 1590 and 1580 Piercy Avenue shared property line: Garbage enclosure location concern and fencing
 - A neighbour stated concerns over placement of the waste receptacle facilities and odour.
 - Staff comment: The waste receptacle facilities location is adjacent that of the facility at 1580 Piercy Avenue, thus focusing the area impacted by waste facilities across the two developments. Other locations of the facility at 1590 Piercy would not be accessible by waste management services given the proposal’s overall site design. The enclosure will be designed to minimize odour and visibility with use of materials and screening.

The fencing between the two properties is in good shape and will continue to remain. No disruption to that existing fencing is proposed.

6. There are currently drought restrictions. Is there enough water to service this new development?
 - Staff response: Water use is estimated to nearly triple in the summer as a result of outdoor water use. Water conservation is particularly important during these summer months when the Comox Lake reservoir is being drawn down prior to the seasonal rainfall/snowpack melt replenishment. There is adequate water to meet new resident’s needs provided that water is used efficiently. The Comox Valley Regional District and member municipalities collectively are working towards water conservation by reducing water demand of outdoor water use during the summer months, and with improved efficiency of water fixtures, appliances, and usage behaviours.

EV parking stall charging

OCP policy supports EV charging as part of parking standards, and directs that the Zoning Bylaw be updated to incorporate EV standards. Until such time as the Zoning Bylaw is updated, the City lacks a regulatory framework to require or direct specifications on EV charging facilities in new developments.

The applicant has responded to Council’s request by proposing that all 10 parking stalls will include separate 240 volt “Level 2” charging facilities. This includes the one stall for each dwelling unit as well as the one shared visitor’s parking stall. The design details will be addressed during the building permit application.

OPTIONS:

1. THAT Council give Third Reading to Zoning Bylaw No. 3037, 2023 (1590 Piercy Ave); and THAT Final Reading of the bylaw be withheld pending:
 - a. registration of a Section 219 covenant to secure the community amenity contributions of \$40,000, to be provided prior to issuance of a building permit; and
 - b. registration of an easement to permit drainage from the subject property on to the Island Corridor Foundation lands.
2. THAT Council defer consideration of Bylaw No. 3037 with a request for more information.

3. THAT Council Defeat Bylaw No. 3037.

ATTACHMENTS:

1. June 14, 2023 Staff Report at First Reading
2. Bylaw No. 3037
3. Electric Vehicle parking stall charging proposal
4. Public Comment

Prepared by: Nancy Gothard, RPP, MCIP, Manager of Community and Sustainability Planning

Reviewed by: Marianne Wade, RPP, MCIP, Director of Development Services

Concurrence: Kate O'Connell, Acting City Manager



THE CORPORATION OF THE CITY OF COURTENAY
STAFF REPORT

To: Council
From: Director of Development Services
Subject: Zoning Amendment Bylaw No. 3037 – 1590 Piercy Ave

File No.: 3360-20-2105/ RZ000058
Date: June 14, 2023

PURPOSE:

The purpose of this report is for Council to consider a Zoning Bylaw amendment application to establish a new Comprehensive Development 39 (CD-39) zone, and rezone the property from R-2 to CD-39 to allow a multi-unit housing development consisting of nine town house units.

LEGAL DESCRIPTION:

That Part of Lot 3, Section 41, Comox District, Plan 4764, Shown Outlined in Red on Plan 1402 R.

EXECUTIVE SUMMARY:

This report details a zoning amendment application to permit a multi-unit housing development containing nine town houses in three buildings on a 2,032 m2 sized property. The subject property is located in a Secondary Growth Centre (McPhee Neighbourhood Centre) surrounded by a mix of multi-unit housing developments and single-residential homes. The proposed development is consistent with OCP land use policies. Development Services Department staff support this zoning amendment application.

BACKGROUND:

This application was originally received in May 2021. For various reasons including adoption of the new OCP, the application has not been presented for bylaw readings until now. This application is therefore reviewed in part with reference to both the old OCP and new OCP.

The subject property is approximately 0.203 ha in size and is located on Piercy Avenue between 17th Street and Cumberland Avenue. The site contains an existing single-residential building and is covered by a mix of shrubs, trees, and grasses. The site slopes west to east with an average slope of 2%. Vehicular access is from Piercy Avenue. **Figure 1** shows the subject property location.

Figure 1 – Subject Property



The subject property is within the Residential Two (R-2) zone. Surrounding properties are a mix of single-residential dwellings and multi-unit residential buildings. See Table 1 below.

Table 1 – Surrounding Zoning and Use

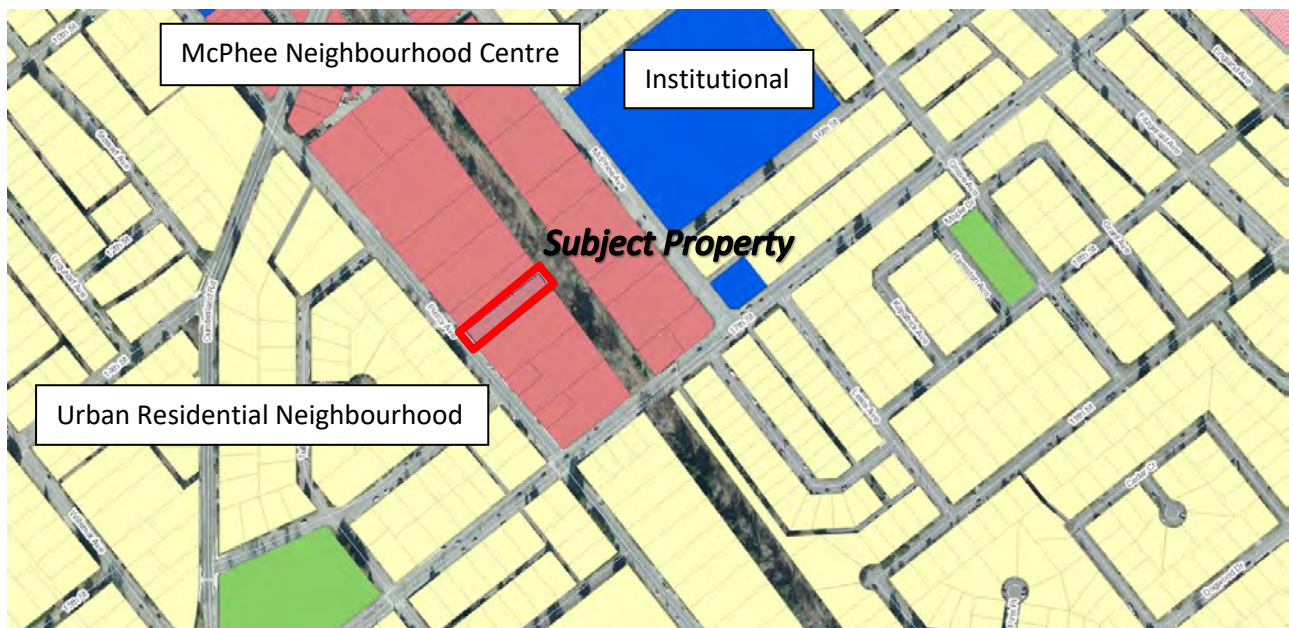
Direction	Zoning	Use
West	R-2 (Residential Two)	Multi-residential – 6 townhomes
South east	R-3 (Residential Three)	Multi-residential – 10 townhomes
South west (across Piercy Ave)	R-2 (Residential Two)	Single residential dwellings
North east (rear)	I-3 (Industrial Three)	Island Corridor Foundation right of way

By means of comparison, the subject property is slightly larger than the property at 1580 Piercy Ave, neighbour to the west that contains six units in the form of 3 duplex units. That property is approx. 1947 m² in area with a 20.5 m frontage. The subject property is 2,030 m² (0.5 ac) with a 21.6 m frontage.

The Official Community Plan (OCP) designates the subject property and properties on the east side of Piercy Avenue between as 17th Street and west of Cumberland Rd and properties across the Island Corridor as “Neighbourhood Centre”. A diversity of multi-residential housing choices and neighbourhood scale commercial uses are supported within this designation. Across Piercy Avenue the OCP Land Use designation for these properties is “Urban Residential” (**Figure 2**).

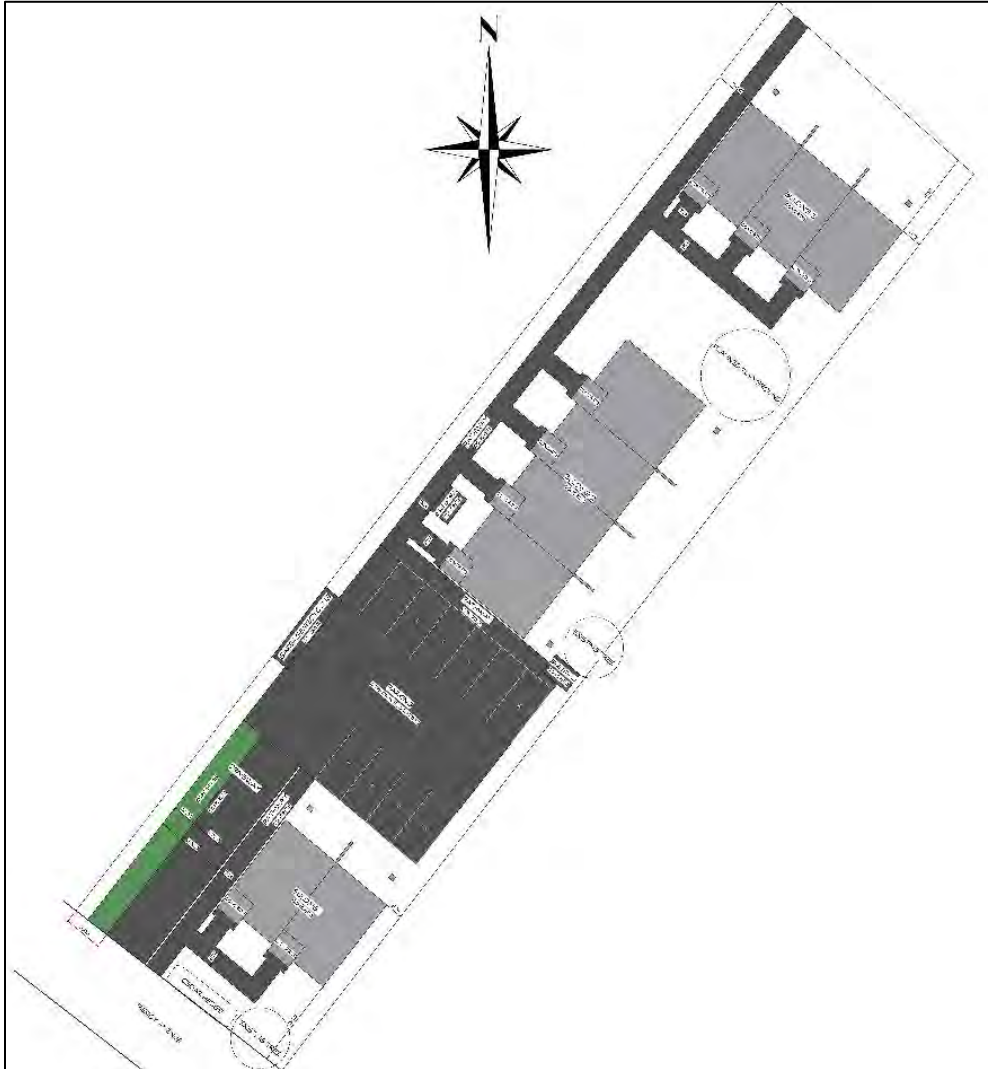
The OCP designates this subject property to be in the form and character Development Permit Area for Commercial, Industrial, Mixed-Use Developments, and Multi-Residential Dwellings with Three or More Units (DPA-1) and staff have reviewed for general compliance. There is no development permit application at this time and one will be required prior to building permit issuance.

Figure 2 – OCP Land Use Designations



The applicant proposes to amend the zoning bylaw to facilitate the construction of nine residential units in three buildings: a duplex that fronts Piercy Avenue; a fourplex located in the centre of the lot; and a triplex at the rear. See **Figure 3** for a site plan of the proposed development and **Figure 4** for a conceptual rendering of the development (without landscaping). The applicant’s proposal is summarized in **Attachment 1**.

Figure 3 – Conceptual Development Layout



Architectural and landscape drawings are shown in **Attachment 2 and 3**. These may be subject to refinement as part of subsequent development permit application process should the zoning bylaw amendment be approved.

Figure 4– Site Plan



DISCUSSION:

Zoning Review

The subject property is zoned R-2 which does not permit multi-residential development. The proposed development requires a zoning bylaw amendment to accommodate the infill development. Because an existing comparable zone (the R-3 zone) regulations would not permit this particular proposal, a comprehensive development zone (CD-39) based upon the R-3 zone has been drafted for Council’s consideration. The proposed CD-39 zone is attached as **Attachment 5**.

Table 2 below compares the proposed CD-39 zone to the R-3 zone to identify the differences. If the R-3 was applied to this property, these differences would require variances to be obtained by Council separately.

The proposed on-site parking reduction included in the table is supported pursuant to the parking studies (**Attachment 4**) submitted by the applicant.

Zoning Review - Table 2

	R-3 Zone	Proposed (CD-39 Zone)	Deviations that would require a variance
Lot size	1,250 m2	2,030 m2	Larger lot size
FAR	0.40	0.40	
Lot Coverage	none	22.3 %	
Lot Frontage	30.0 m	21.6 m	8.4 m less lot frontage width
Setbacks			
- Front	7.5 m	7.5 m	
- Rear	7.5 m	7.3 m	Rear yard setback is 7.3 m, 0.2 m less at the NE corner due to lot shape.
- Side	4.5 m total with a min of 1.5 m 6.0 m when the back of the building is adjacent to or faces a side lot line	2.8 m (NW) 1.5 (SE)	Side yard setbacks total 4.3 m, 0.2 m less due to lot width. Building 2 (triplex) rear elevation faces side lot line (east) with a 5.0 m setback due to lot width.
Building Height			
- Principle	10.0 m	6.5 m	Lower due to building form

	R-3 Zone	Proposed (CD-39 Zone)	Deviations that would require a variance
Usable Open Space	30.0 m ² for each DU with 1 bdrm 50.0 m ² for each DU with 2+ bdrms Total – 450 m ²	Total - 1,065 m ²	More useable open space to allow for private open space for each unit as well as community open suitable for family housing and will contain some furniture.
Accessory Building - Height - Size - Siting	4.5 m 50.0 m ² or 10% of rear yard In side or rear yard only. 1.5 m from side or rear lot line	Covered garbage enclosure 2.8 m 13.0 m ² Is at zero lot line, side yard (west)	Garbage enclosure is zero lot line so it's accessible to contractors.
Parking (Section 7) - Vehicular - Bicycle - EV Bicycle	1.2 per DU + 10% visitor Total – 11 + 2 = 12 Class II (Occupant) – 18 Class I (Visitor) – 2 9	1 per DU + 1 visitor Total – 9 + 1 = 10 Class II (Occupant)– 18 Class I (Visitor)– 8 9	Two fewer on-site parking stalls are proposed based upon parking analyses prepared by Watt Consulting and supported by the OCP.

Outdoor Amenity Space, Landscaping and Trees

The total outdoor amenity space is 1065 m² which includes front yards and private rear porches for each unit and a larger amenity area located between the four townhouses and three townhouses. This area will contain some outdoor furniture.

The garbage/recycling enclosure located west of the parking area is to be located at zero lot line. This enclosure is small at 13.0 m² and 2.8 m height. It will be constructed of materials that complement the building materials. The location of the enclosure at the zero lot line is necessary given site constrictions and waste receptacle access requirements. The enclosure of the garbage/recycling facility was proposed to address comments from the neighbour.

Landscape buffers will be planted around the perimeter of the property ranging from 1.2 m in width to 3.4 m in width for the north side yard (left), between 1.5 to 1.8 m in the south side yard (left) and 4.5 m buffers will be planted in the rear and in front yards except for driveway. In addition to the landscape buffer around the perimeter a 1.8 m wood fence will be installed as well as privacy fences between patios. Site furniture is also proposed including benches and a picnic table. Refer to **Attachment 3** of this report for the landscape plan.

The landscape plan shows protection of two mature maple trees and tree replacement for those that are being removed (32 new trees). The arborist report notes that trees located in the perimeter are cottonwood or red-alder and are not suitable for the landscape plan. These species are also not eligible for tree bylaw retention candidates (outside of Environmentally Sensitive Areas). Other trees have structural characteristics which preclude them from maturing into healthy, defect-free trees.

Access, Parking and Bikes

Parking for this project is detailed in the two traffic/parking studies prepared by WATT Consulting Group, dated March 30, 2020 and July 4, 2022 (***Attachment 4***). The studies determined the total expected parking demand for the proposed development is 10 parking spaces total: one per dwelling unit and one visitor parking space for a total of ten, or 1.11 spaces per unit.

The parking area and 2.0 m wide strip of the driveway will be permeable surfaces of core grass/core gravel and meets OCP policy NE 16 to limit the extent of impervious surfaces on private and public land.

On-street parking use was observed for both studies, in the evening hours, the peak demand time for residential use. The 2020 study observed a 9% occupancy rate, out of 157 total spaces. And the 2022 study observed a 23% occupancy rate based on 143 parking spaces. These findings reflect that the surrounding residential land uses do not rely heavily on on-street parking during peak demand time for residential use.

The subject property will be served by a 6 m wide driveway access. The parking area will be permeable surfaces of core grass/core gravel.

Infrastructure

The applicant proposes to discharge via point source flows directly into the adjacent private Island Corridor Foundation (ICF) property. The applicant has provided confirmation from ICF confirming they will permit this action and will enter into an easement with the proponent. This easement will be required to be registered on the title of the property prior to Council's consideration of final approval.

POLICY ANALYSIS:

This proposal meets the following objectives detailed in Official Community Plan Bylaw 3070, 2022.

Land Use:

- ***Objective 1:*** Community growth is located away from hazardous lands, agricultural lands and environmentally sensitive areas.
 - The subject property contains no environmentally sensitive areas.
- ***Objective 2:*** The majority of community growth is strategically guided into growth centres to create more 10-minute neighbourhoods
 - The subject property is located in the McPhee Neighbourhood Centre 1.1 km from commercial/retail amenities and personal and professional services. There are two bus stops within a 6-minute walk and another stop within 11-minute walk. Sidewalks are located on both side of Piercy Avenue. There is an existing bike route 600 m from the subject property and 900 m from a future proposed bike route. The Island Corridor multi-use pathway, runs along the rear property line.
- ***Objective 3:*** Moderate infill development occurs across the entire city outside growth centres
 - The development proposes a floor area ratio of .40 and is consistent with the R-3 zone, the most similar low-density multi-residential zone.
 - The nine units are located in three buildings and are similar to surrounding multi-unit developments.

Streets and Transportation:

- ***Objective 6:*** The amount of land dedicated to parking is minimized
 - The parking proposed is located on impervious surfaces. Two fewer parking spaces are proposed based upon two professional parking studies.

- *Objective 7:* Parking standards reflect electric vehicle and cycling needs
 - The proposal includes required bike parking and EV charging facilities.

Buildings and Landscape:

- *Objective 2:* New buildings are highly energy and water efficient, perform at net-zero emissions standard and produce renewable energy
 - These Part 9 buildings will be constructed to the BCBC Step 3 high energy performance standards.
- *Objective 4:* The designs of new buildings complement neighbourhood character
 - The proposed design and layout is consistent with the surrounding townhouse developments.

Affordable Housing:

- *Objective 1:* A variety of housing options are permitted and positively integrated in all neighbourhoods
 - *Policy AH 1:* Support higher housing densities, including amending the Zoning Bylaw to permit two dwelling units per single residential lot, in all residential land use designations, as described in the Managing Growth Policy section of this Plan, and in accordance with protection of Environmentally Sensitive Areas policies.
 - The proposed development increases the number of units from two to nine.
 - *Policy AH 6:* Encourage the provision of 3+ bedroom units as part of a mix of unit types in new multi-residential buildings to create more housing choices for families.
 - The proposal includes two-three bedroom units. Also, the large useable open space in the middle of the development will allow for outside play space for young children.

Natural Environment:

- *Objective 3:* Courtenay's air, water, and soil are clean
 - *Policy NE 16:* Limit the extent of impervious surfaces on private and public land.
 - The percent of impervious surfaces for paved areas and buildings is 48% in total. The pervious areas include the driveway, parking area, landscape buffers, and open space.

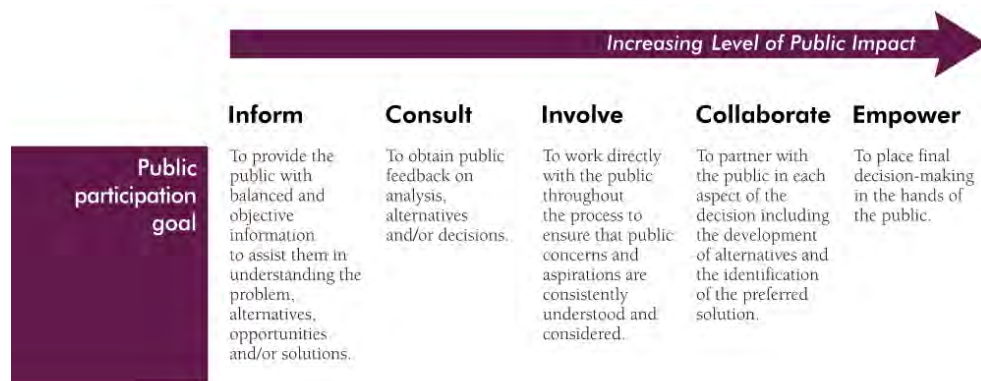
Community Amenity Contribution

The applicant has confirmed amenity contributions of \$40,000. See **Attachment 6** for a copy of the applicant's letter.

Prior to final adoption of Bylaw No. 3037, a section 219 covenant to secure the community amenity contributions of \$40,000 must be completed and registered on the title of the subject property. The amenity contribution will be provided prior to issuance of the building permit.

CITIZEN/PUBLIC ENGAGEMENT:

Staff will “Consult” the public based on the IAP2 Spectrum of Public Participation:



The applicant mailed out a public information package on May 28, 2021 to 80 property owners and resident occupiers within 100 m of the subject property. The Public Information meeting (PIM) mail out can be found in **Attachment 7**. The comments received by the City and applicant can be found in **Attachment 8** together with staff responses. The following summarizes some of the substantive comments received and staff responses:

1. The garbage area is located too close to the neighbouring units.
This comment resulted in the garbage area being redesigned so it is enclosed. The proposed location is necessary to accommodate the garbage trucks turning radius.
2. Proximity of one of the buildings to the neighbouring units is too close and privacy will be lost. The landscape buffer should be of larger trees.
The proposed duplex dwelling units will be located between 1.5 m (4.95 ft) and the rear triplex 2.2 m (7.3 ft) from the property lines. This siting is permissible in most residential zones. A large maple is being retained in the rear of the triplex building and a landscape buffer is proposed to be planted around the perimeter of the property.
3. The (first) parking study methodology has flaws regarding when it was completed, the comparable developments that were used and assessment of on street parking
This comment resulted in the applicant providing an updated parking study. Both studies are attached. The consultants concluded that the proposed number of on-site parking stalls is suitable for the proposed residential use and that there is adequate on street parking for the residential use.
4. Traffic light should be installed at 17th St and Piercy Ave.
The City's Transportation Plan (2019) does not identify this intersection as a high collision intersection. However, Piercy Ave itself is a recognized cycling route in the City's Cycling Network Plan and cycling improvements at the intersection of Piercy and 17th are budgeted in the 2023 work plan.

Should Zoning Amendment Bylaw No. 3037 receive First and Second Readings, a statutory public hearing will be held to obtain public feedback in accordance with the *Local Government Act*.

As required by Section 52(3) of the **Transportation Act** where the subject property is located within 800 m of a controlled access highway, the Ministry of Transportation must grant its approval prior to Council's consideration of approval. The subject property is located within 800 m of Cliffe Avenue, a designated highway.

REGIONAL GROWTH STRATEGY REFERENCE:

The Comox Valley Regional Growth Strategy (RGS) is a framework for future land use, and sets basic direction for planning, policies, and action for all member municipalities, including Courtenay. The RGS is guided by a number of growth management principles that are incorporated by this proposed application:

- Promote the efficient use of land, provide greater transportation choices, reduce public servicing costs, and achieve environmental benefits through compact growth.
- Promote intensification, compact growth and supportive public transit services throughout designated Municipal Areas as the primary means of accommodating population and employment growth.

FINANCIAL IMPLICATIONS:

There are no direct financial implications related to the processing of this Zoning Bylaw amendment application as the fees are designed to offset the administrative costs.

The application, if approved, will be required to apply for and be issued a form and character development permit. The development will also be subject to City and Regional District development cost charges which are determined at time of building permit based on the final building design. Because this is an infill development no new civil infrastructure is being developed beyond replacing existing.

ADMINISTRATIVE IMPLICATIONS:

Processing this application is part of the core duties of the Department of Development Services. Should Council not move forward with the zoning bylaw amendment a rezoning application could not be considered for a year unless Council considers a request to reconsider with 2/3 vote in favour.

ASSET MANAGEMENT IMPLICATIONS:

The applicant submitted engineered civil plans for water, sewer, and stormwater to service the development. The submissions are adequate to confirm the site can be serviced. Final engineering plans will be required as part of the building permit application.

The off-site transportation related improvements include removing and relocating the driveway let down and replacing the existing sidewalk on the Piercy Avenue frontage.

STRATEGIC PRIORITIES REFERENCE:

This initiative addresses some of the goals that guide Council's strategic priorities:

- The proposal provides nine two- and three-bedroom units of infill housing with common amenity space located in walking distance to the downtown and services. This location is along public transportation and cycling routes and cycling infrastructure is included in the development allowing for reduced vehicle dependence. Infill housing also makes efficient use of existing municipal infrastructure

OPTIONS:

1. THAT Council give First and Second Reading to "Zoning Amendment Bylaw No. 3037" (1590 Piercy Avenue) to amend Zoning Bylaw NO. 2500, 2007 create a new CD-39 Zone and rezone the subject property to CD-39 with the following conditions met prior to final adoption:
 - a. A section 219 covenant registered on title to secure the community amenity contributions of \$40,000 to be provided prior to issuance of the building permit.

b. An easement to permit drainage from the subject property on to the Island Corridor Foundation lands; and

THAT Council direct staff to advertise a statutory Public Hearing for “Zoning Amendment Bylaw No. 3037”.

2. THAT Council give First Reading of Zoning Bylaw No. 3037 (1590 Piercy Ave) and refer to staff for further information prior to second reading.
3. That Council deny Zoning Amendment Bylaw No. 3037 (1590 Piercy Ave) . (please see Administrative Implications).

Staff recommends Option 1

Prepared by: Brigid Reynolds, RPP, MCP, MCIP, Contract Planner

Reviewed by: Nancy Gothard, RPP, MCIP Manager of Community and Sustainability Planning
Marianne Wade, Director of Development Services, RPP, MCIP

Concurrence: Geoff Garbutt, RPP, M.Pl., MCIP, City Manager (CAO)

Attachments to the staff report

Attachment 1 - Applicant proposal

Attachment 2 - Architectural plans

Attachment 3 - Landscape plans

Attachment 4 - Parking studies

Attachment 5 - Bylaw 3037 and CD-39 Zone

Attachment 6 - Letter of confirmation for amenity contribution

Attachment 7 - PIM mail out notice

Attachment 8 - Comments received and staff responses

Nine Peaks – 1590 Piercy Avenue

Development Proposal

Perspective Design Build LTD is applying to change the Zoning of 1590 Piercy avenue from R-2 to R-3 for the purpose of constructing nine townhouse units. Seven of these townhouses will be two-bedroom units and two will be three-bedroom units. They will be separated into three buildings: a two unit building facing Piercy Avenue, a four unit building in the heart of the property and a three unit building along the rear.

The lot will be developed to have a driveway, onsite parking, a garbage enclosure, bicycle parking facilities, green space, an amenity area, pathways, and a variety of native and ornamental plants and trees. Two large maple trees on the property will be retained. The driving infrastructure and the parking infrastructure will be permeable and consist of core gravel and core grass.

The proposed development conforms with the R-3 Zoning as follows:

Section 8.3.1 Permitted Uses: The project is a multi-family residential dwelling which conforms to the usage.

Section 8.3.2 Lot Size: The lot is 2030 meters squared which is above the minimum of 1250 meters squared.

Section 8.3.4 Floor area ratio: The proposed floor area ratio is 38.27%, below the 40% maximum.

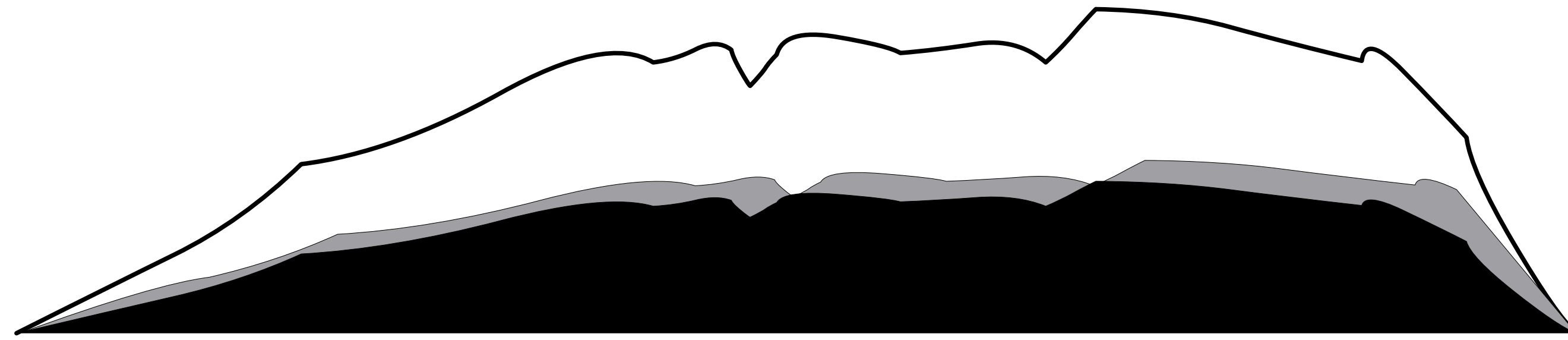
Section 8.3.5 Setbacks: All of the setbacks are observed.

Section 8.3.6 Height of Buildings: At 6.5 meters in height the proposed development is below the 10 meter maximum.

Section 8.3.7 Usable open Space: The development is required to have 450 meters squared of open usable space and the project is designed to have 1065 meters squared.

Section 8.3.9 Off-Street Parking: The on-site parking supply is 10 spaces and 30 bicycle parking spaces. A minimum of 10% of these spaces will have e-bike charging capacity and a minimum of 10% of these spaces are sized to accommodate cargo bicycles. These designs are in accordance with the parking study supplied by Watt Consulting for the project.

Section 8.3.3 Lot frontage: The required minimum lot frontage is 30 meters and this lot only has a frontage of 21.644 meters. The added setback challenges and lot coverage challenges have been addressed and the lot will function well as an R-3 lot. The neighboring lot Strata Plan EPS3363 Has a similar frontage and is a successful multifamily development.



NINE PEAKS

1590 PIERCY AVENUE, COURTENAY



**DRAWING INDEX
ARCHITECTURAL**

- P1 SITE PLAN
- P2 FOUNDATION PLAN
- P3 FLOOR PLANS
- P4 ROOF PLAN
- P5 SECTIONS
- P6 ELEVATIONS



**PERSPECTIVE DESIGN
BUILD LTD.**
1160 4TH ST., COURTENAY
BRITISH COLUMBIA V9N 1H8

**DUPLEX
DETAILED DESIGN**

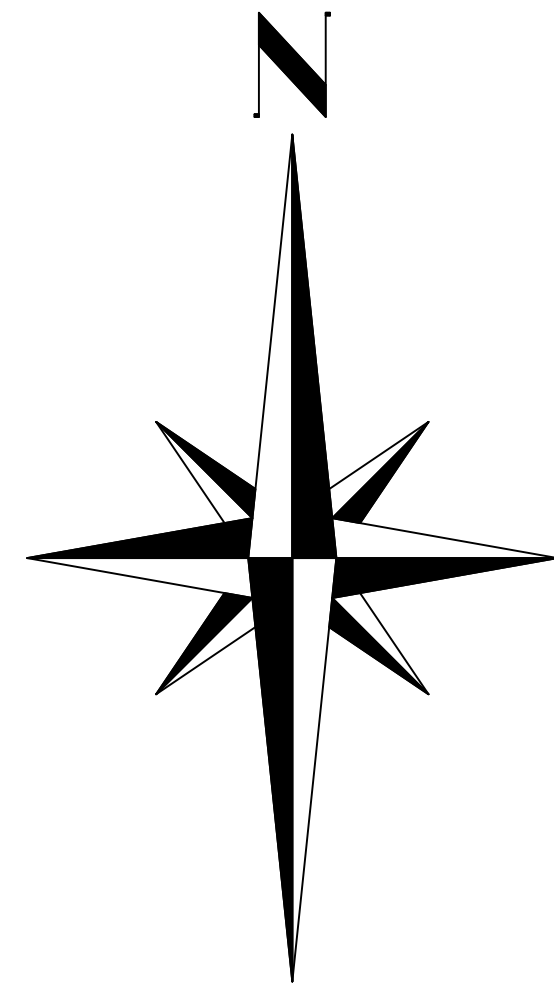
**TO SCALE ON 36X24
PRINT FORMAT ONLY**

COVER SHEET	0.0
REVISION	1

VERSION FOR RE-ZONING PURPOSES

COURTENAY DISTRICT, PLAN 4764 TOPOGRAPHIC SURVEY PLAN

1590 PIERCY AVENUE, COURTENAY
SCALE 1 : 150 (METRIC)



PROJECT DATA

ZONING R2 (TO BE RE-ZONED R3)
LOT AREA 0.5 ACRES (2030.5 SQ M)

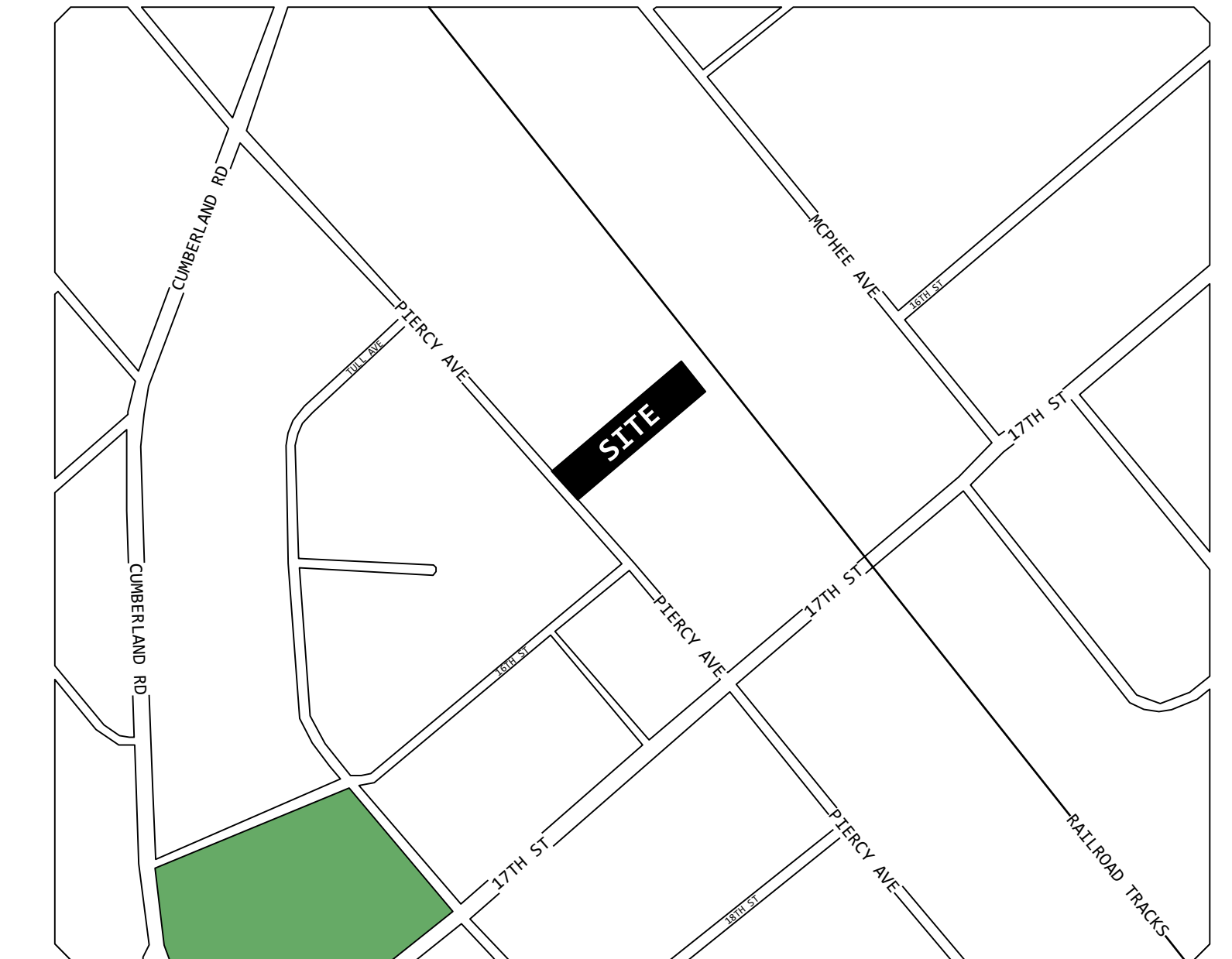
ITEMS	PERMITTED	PROPOSED
LOT COVERAGE /FAR	0.4	0.32
LOT COVERAGE TOTAL (SQ M)	812.2	651.9
HEIGHT (M)	10	6.1

SETBACKS (M)

FRONT	MIN 7.5	7.5
REAR	MIN 7.5	7.5
SIDE LEFT	MIN 1.5	3.3
SIDE RIGHT	MIN 1.5	1.7

FLOOR AREAS

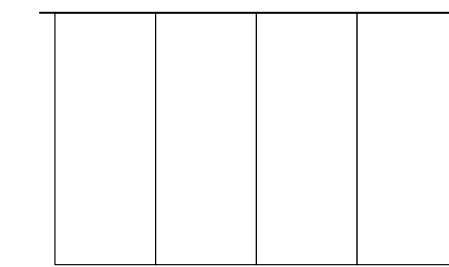
EXTERIOR FLOOR AREA OF 2 DWELLINGS BUILDING (SQ M)	174.5
EXTERIOR FLOOR AREA OF 3 DWELLINGS BUILDING (SQ M)	212.6
EXTERIOR FLOOR AREA OF 4 DWELLINGS BUILDING (SQ M)	264.8
EXTERIOR FLOOR AREA IN TOTAL (SQ M)	651.9
(9)ENTRIES IN TOTAL (SQ M)	42.8



OFF-STREET PARKING SPACES (PER DWELLING INCL.10% VISITOR PARKING)	REQUIRED	PROPOSED
	14 (1.5)	10 (1.1)

BICYCLE PARKING ADDITION

LONG TERM PARKING SPACES	-	30
SHORT TERM PARKING SPACES	-	8



TYPICAL PARKING SPACE LAYOUT
NO SCALE

OTHER PARKING LOT SPECIFICATIONS SHALL FOLLOW
DIVISION 7, BYLAW 2500 (2007) REQUIRMENTS

SEE PARKING STUDY ISSUED AND SIGNED BY WATT CONSULTING GROUP,
FILE NUMBER 2785.B01; MARCH 30, 2020

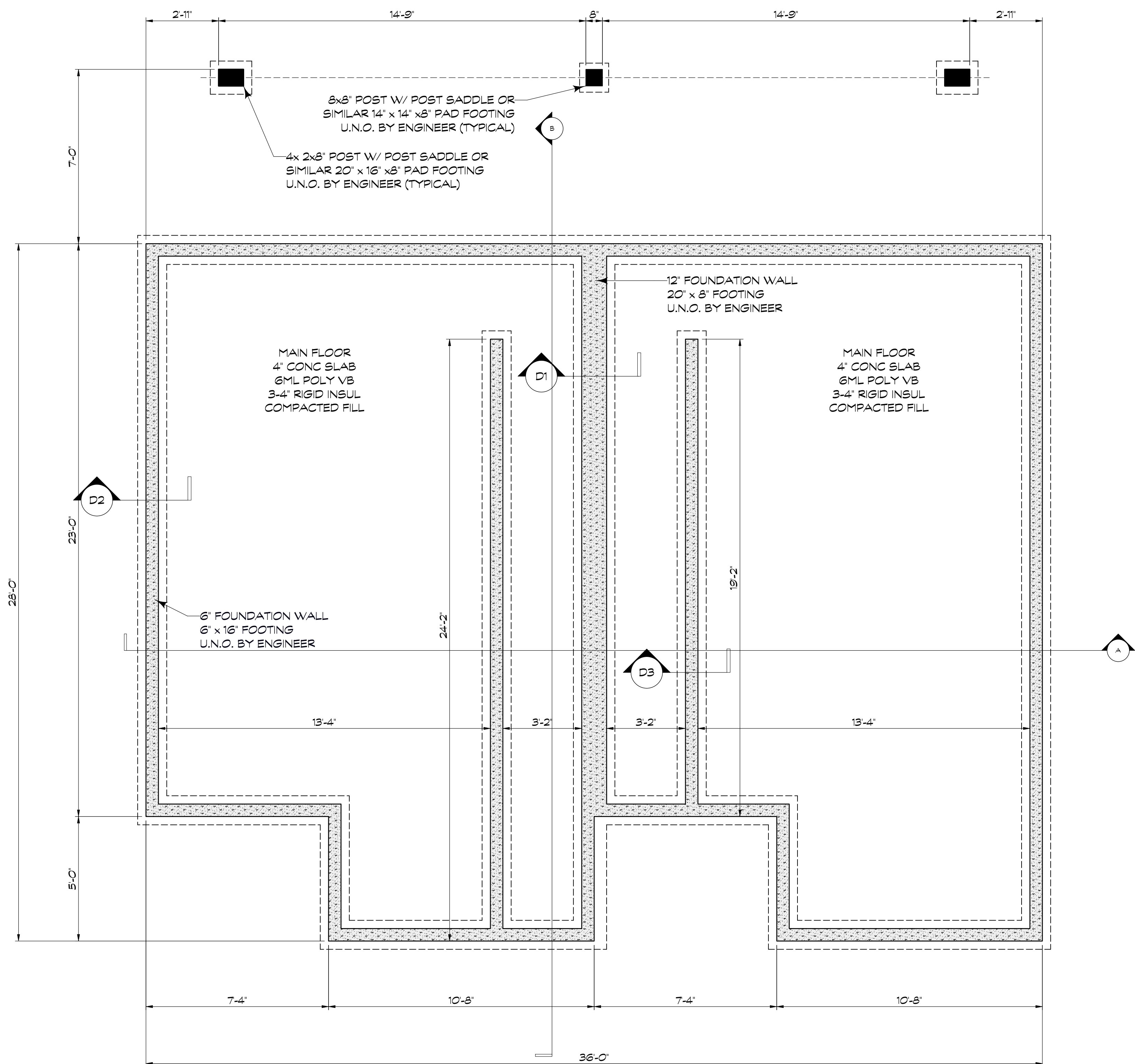
SITEPLAN NOTES

1. CARRY ON ALL SITE WORK SHOWN ON OR READILY INFERRABLE FROM THE PERMIT DRAWINGS; INCLUDING EXCAVATION, DEWATERING, BACKFILLING, ROUGH & FINISHED GRADING, BELOW-GRADE SERVICES, PLANTING, SODDING, FENCING, CURBING, PAVING, LINE MARKERS, SIGNAGE, ETC. CO-ORDINATE ALL WORK WITH THE SOIL REPORT & THE ITS ENGINEER. PROVIDE INSPECTIONS, TEST AND REPORTS, FOR ALL BEARING SURFACES, BACKFILL MATERIALS, AND COMPACTION.
2. LEGALLY DISPOSE OFF-SITE ALL EXCAVATED MATERIAL, & ITEMS REQUIRING REMOVAL.
3. PROTECT ALL EXISTING PUBLIC PROPERTY AND UTILITIES. PROVIDE HOARDINGS, BRACING, SHORING, COVERS, ETC. AS REQUIRED. CONTACT ALL REQUISITE MUNICIPAL AND UTILITY AUTHORITIES FOR LOCATES, AND CARRY OUT WORK TO THEIR APPROVAL. MAKE GOOD ANY DAMAGE.
4. DO NOT COVER-UP ANY WORK UNTIL PROPERLY INSPECTED BY THE PERTINENT CONSULTANT AND/OR AUTHORITY.
5. OBTAIN APPROVAL FROM GEOTECHNICAL / STRUCTURAL ENGINEER FOR ALL BEARING SURFACES, BACKFILL MATERIALS, AND COMPACTION.
6. SEE SURVEYOR'S PLAN FOR PROPERTY LINE DIMENSIONS AND DIRECTION.
7. SEE GEOTECHNICAL ENGINEER'S REPORT FOR SOIL / BEARING CONDITIONS.

McElhanney Associates Land Surveying Ltd.
495 6th Street | Courtenay BC V9N 6V4
T 250 338 5495 | C 250 897 5840
FILE: 2211-80169-08

VERSION FOR RE-ZONING PURPOSES

1.FEB01/21 Concept Design for Review	
Issue Notes	
PERSPECTIVE	Sheet No. 1
Project Location 1590 Piercy Rd., Courtenay, BC	
Project Title NINE PEAKS	
Sheet Title SITE PLAN	



FOUNDATION PLAN
SCALE: 3/8" = 1'-0"

~68 LINEAR FT PER DWELLING

NOTE:
ALL POINT LOADS MUST BE ASSESSED BY AN ENGINEER AND FOOTING SIZES INCREASE AS REQUIRED TO SUPPORT LOADS

NOTES:

GENERAL NOTES

- ALL WORK SHALL CONFORM TO CURRENT BUILDING CODES AND LOCAL BYLAWS
- WRITTEN DIMENSIONS TAKE PRECEDENCE OVER SCALED DRAWINGS

CONCRETE

- ALL CONCRETE TO HAVE MINIMUM COMPRESSIVE STRENGTH OF 20MPA AT 28 DAYS
- FOOTINGS SHOWN ON THESE DRAWINGS HAVE BEEN DESIGNED FOR SOIL BEARING CAPACITY OF 2500 PSF. LOCAL CONDITIONS AND / OR LOCAL PRACTICE MAY NECESSITATE A MORE STRINGENT FOOTING DESIGN WHICH, MAY REQUIRE CONFIRMATION BY A CERTIFIED STRUCTURAL ENGINEER. THIS WILL BE THE RESPONSIBILITY OF THE OWNER / CONTRACTOR TO PROVIDE.
- CONCRETE FOOTINGS MUST BE PLACED ON UNDISTURBED OR COMPACTED SOIL TO AN ELEVATION BELOW FROST PENETRATION OR ALTERNATIVE METHOD SHALL BE USED TO ENSURE INSULATION OF FOUNDATION AS PER LOCAL BUILDING CODE
- ALL CONCRETE AND MASONRY FOUNDATION WALLS EXCEEDING LIMITS STATED IN BUILDING CODES REQUIRE DESIGN BY A REGISTERED STRUCTURAL ENGINEER
- ALL FOOTINGS TO HAVE 2 ROWS OF MIN. NO. 4 REBAR 3' CLEAR FROM SIDE AND BOTTOM
- ALL WOOD CONTACTING CONCRETE TO BE SEPARATED WITH APPROVED MATERIAL

STRUCTURAL

- ALL WOOD FRAMING TO BE #2 OR BETTER DOUGLAS FIR OR SPRUCE
- ALL LINTELS TO BE 2-2X10 #2 DOUGLAS FIR OR BETTER UNLESS OTHERWISE NOTED
- LAMINATE STUDDING UNDER ALL LOAD BEARING POINTS
- JOISTS ARE TO BE DOUBLED UNDER PARTITIONS
- ALL ENGINEERED COMPONENTS TO BE INSTALLED TO MANUFACTURES SPECS.

PLUMBING

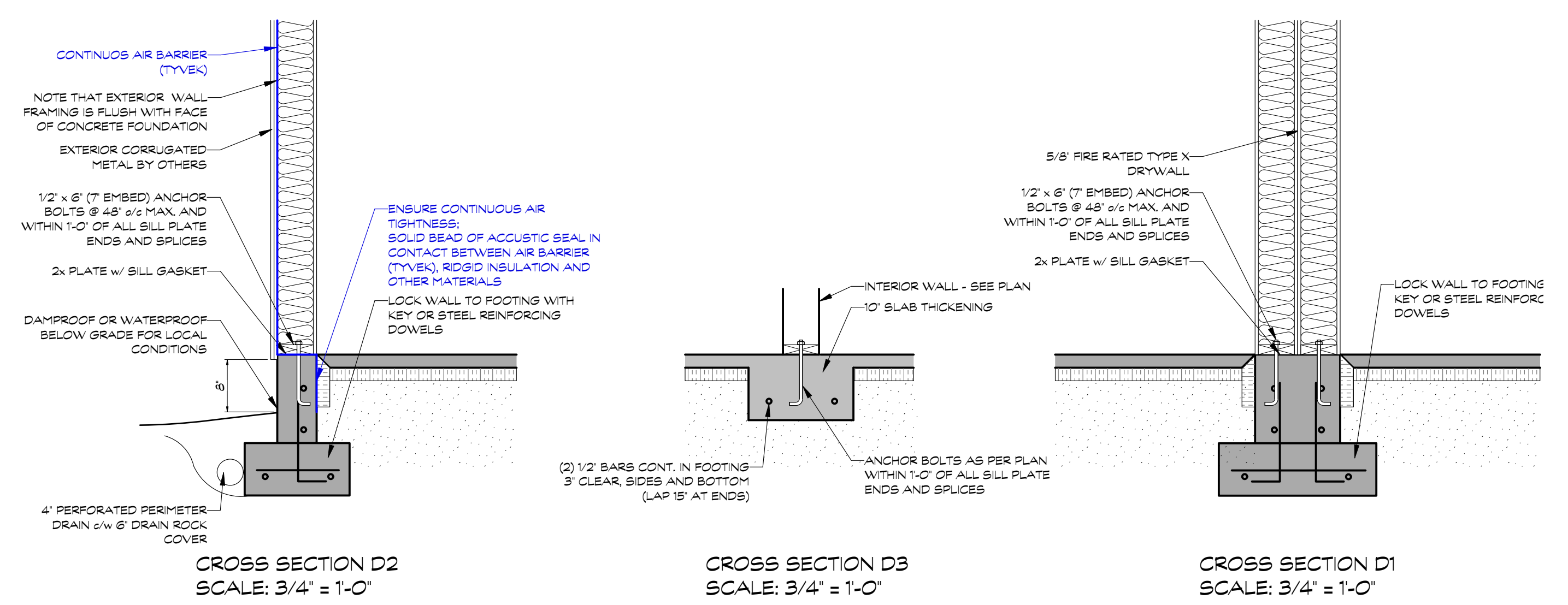
- PLUMBING FIXTURES AND FITTINGS SHALL MEET THE STANDARDS NOTED BELOW
- A) WATER CLOSET = 1.6 GAL PER FLUSH
- B) SHOWER HEAD = 2.5 GPM MAX
- C) LAVATORY FAUCETS = 2.2 GPM MAX
- D) SINK FAUCETS = 2.2 GPM MAX TITLE 24, VCBC, UPC

EGRESS WINDOWS (BEDROOMS)

- FINISHED SILL 59" ABOVE FLOOR MAX.
- NET OPENING TO BE 3.77 SQUARE FEET MINIMUM, WITH NO DIMENSION LESS THAN 15"

MECHANICAL AND VENTILATION

- BUILDING MUST BE PROVIDED WITH MECHANICAL VENTILATION DESIGN BY OTHERS
- ATTIC TO BE VENTED MINIMUM 1:300 OF AREA
- ONE HALF OF ROOF VENTS TO BE LOCATED IN UPPER PORTION OF ATTIC SPACE, AT LEAST 3'-0" HIGHER THAN EAVE OR CORNICE VENTS
- 22' x 30' MIN. ATTIC ACCESS. INSULATE AND WEATHER STRIP DOOR. 30' MIN. HEAD CLEARANCE



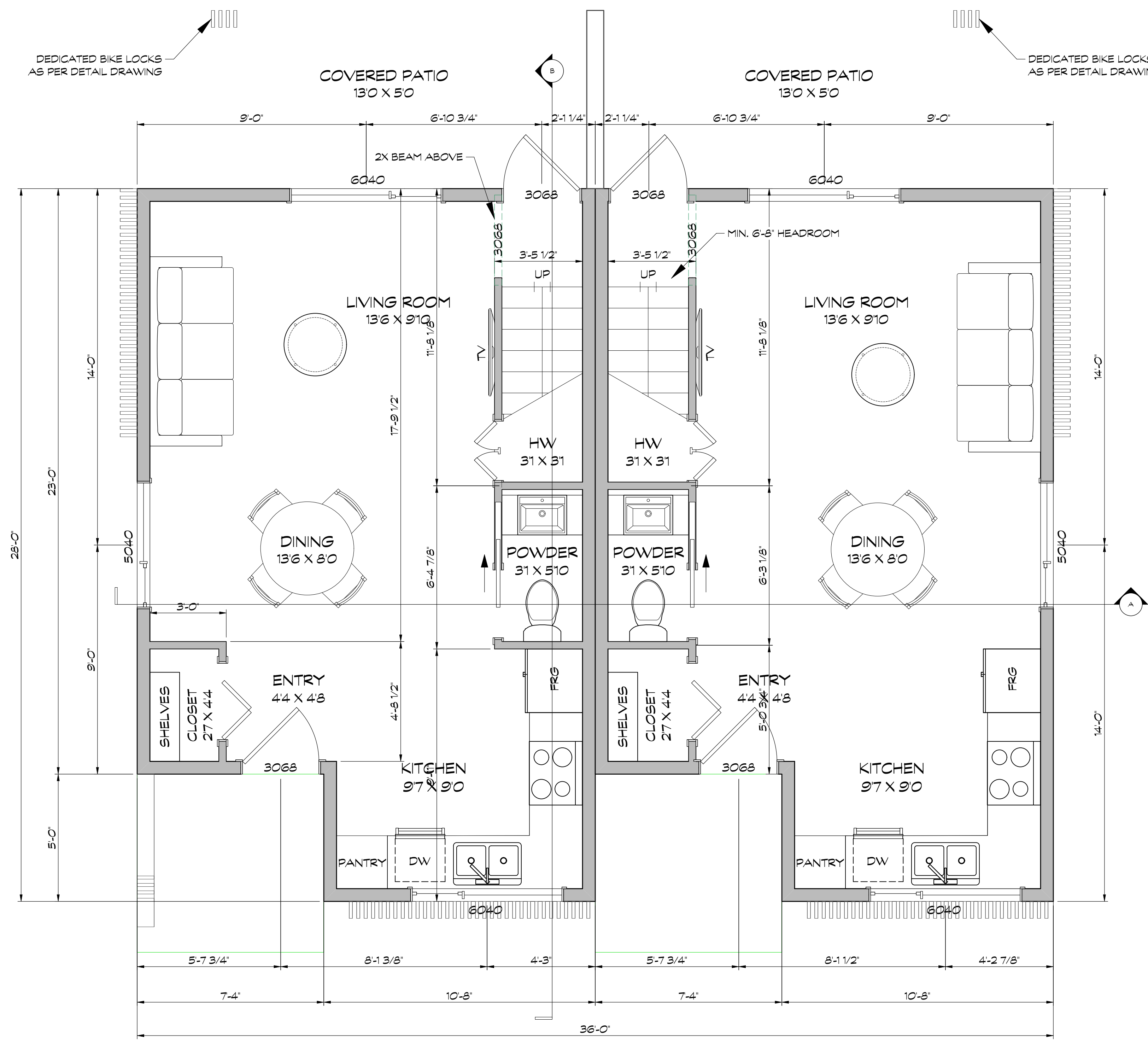
CROSS SECTION D2
SCALE: 3/4" = 1'-0"

CROSS SECTION D3
SCALE: 3/4" = 1'-0"

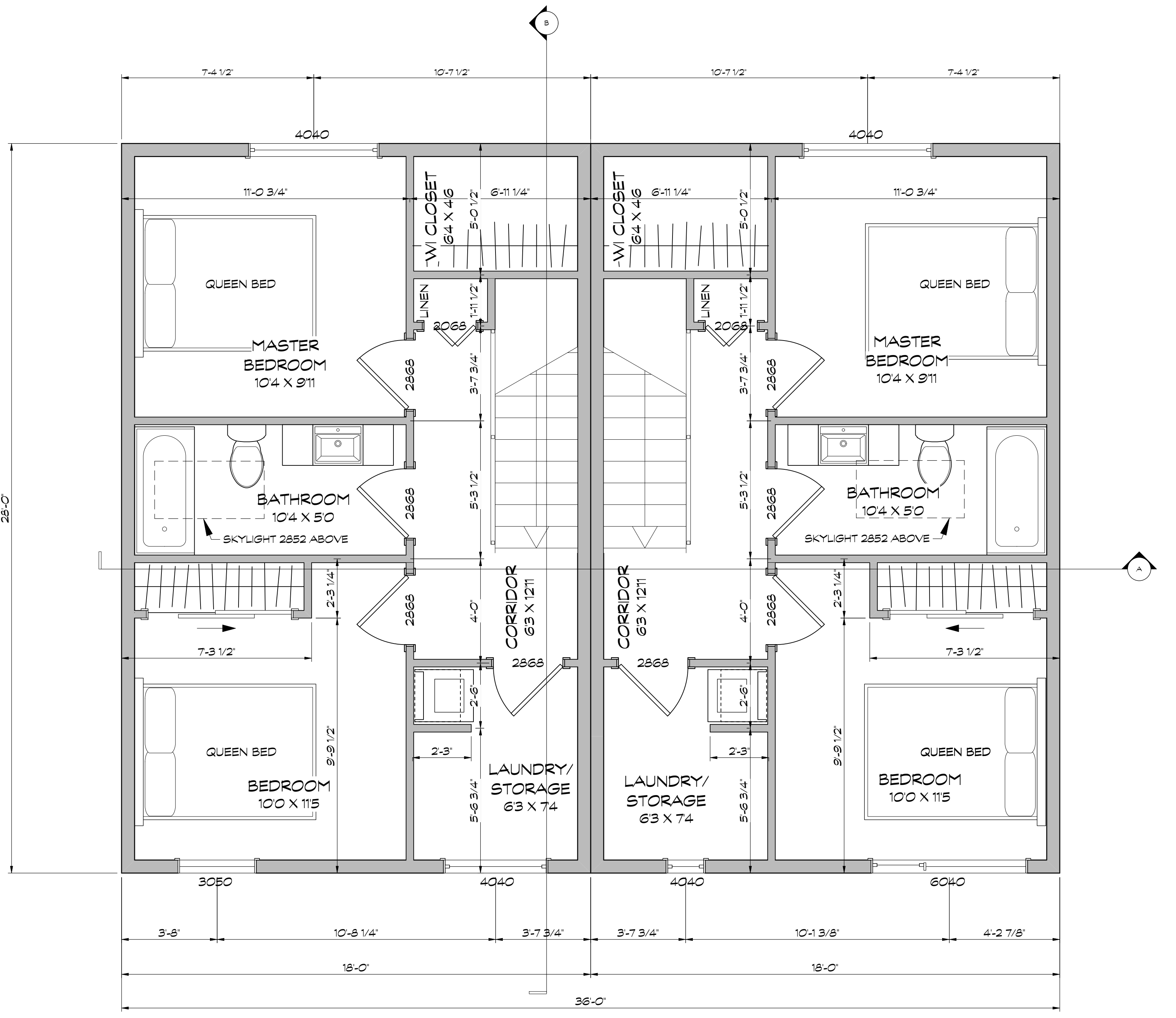
CROSS SECTION D1
SCALE: 3/4" = 1'-0"

VERSION FOR RE-ZONING PURPOSES

1.FEB01/21 Concept Design for Review	
ISSUE NOTES	
PERSPECTIVE	Sheet No. 2
Project Location 1598 Piercy Rd., Courtenay, BC	
Project Title NINE PEAKS	
Sheet Title FOUNDATION PLAN	



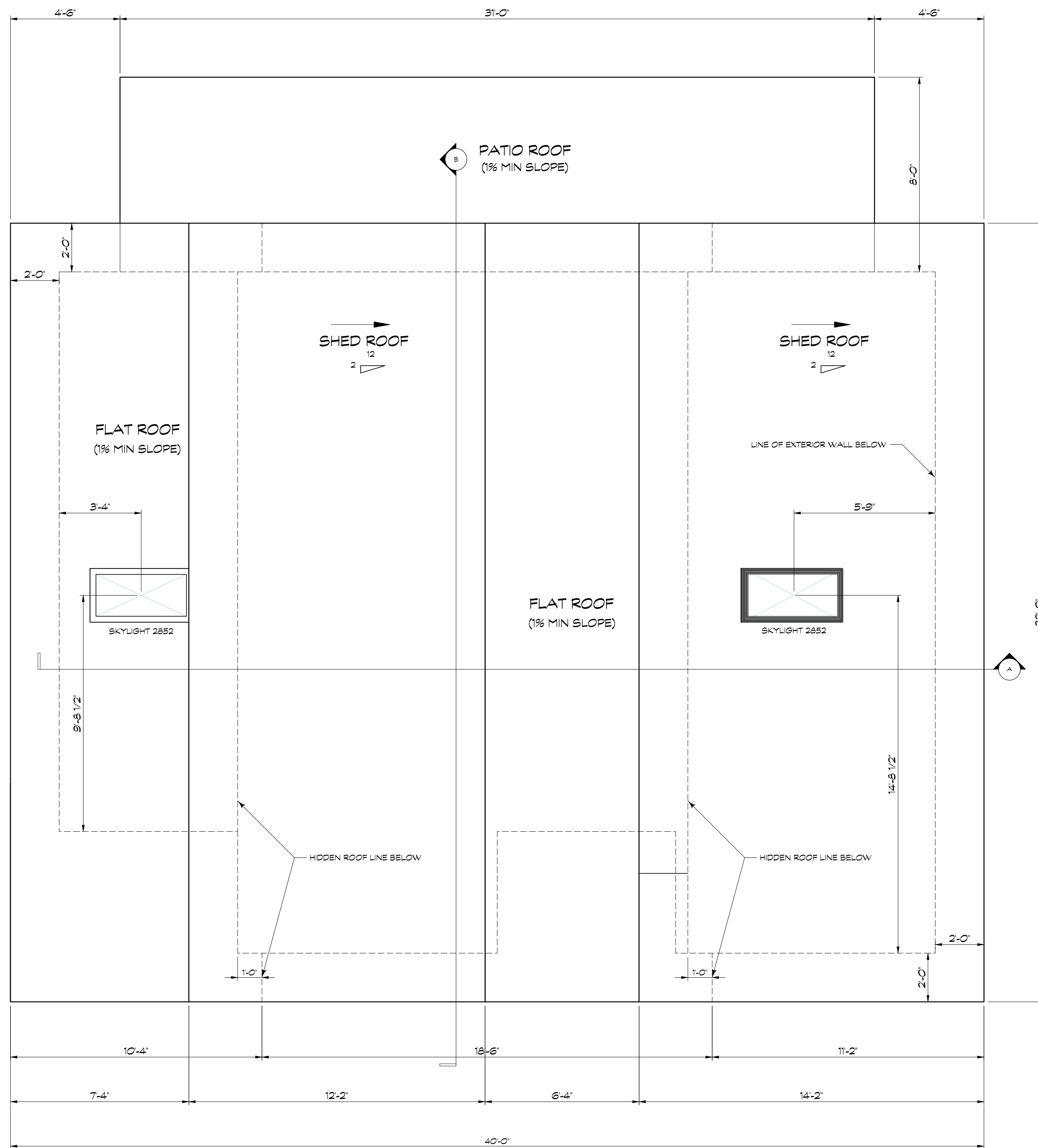
MAIN FLOOR PLAN
 SCALE: 3/8" = 1'-0"
 467 SQ FT (EXTERIOR FLOOR AREA PER DWELLING)
 51 SQ FT (ENTRY SLAB PER DWELLING)
 935 SQ FT (EXTERIOR FLOOR AREA IN TOTAL PER BUILDING)
 102 SQ FT (ENTRY SLABS IN TOTAL PER BUILDING)



UPPER FLOOR PLAN
 SCALE: 3/8" = 1'-0"
 472 SQ FT (EXTERIOR FLOOR AREA IN PER DWELLING)
 945 SQ FT (EXTERIOR FLOOR AREA IN TOTAL PER BUILDING)

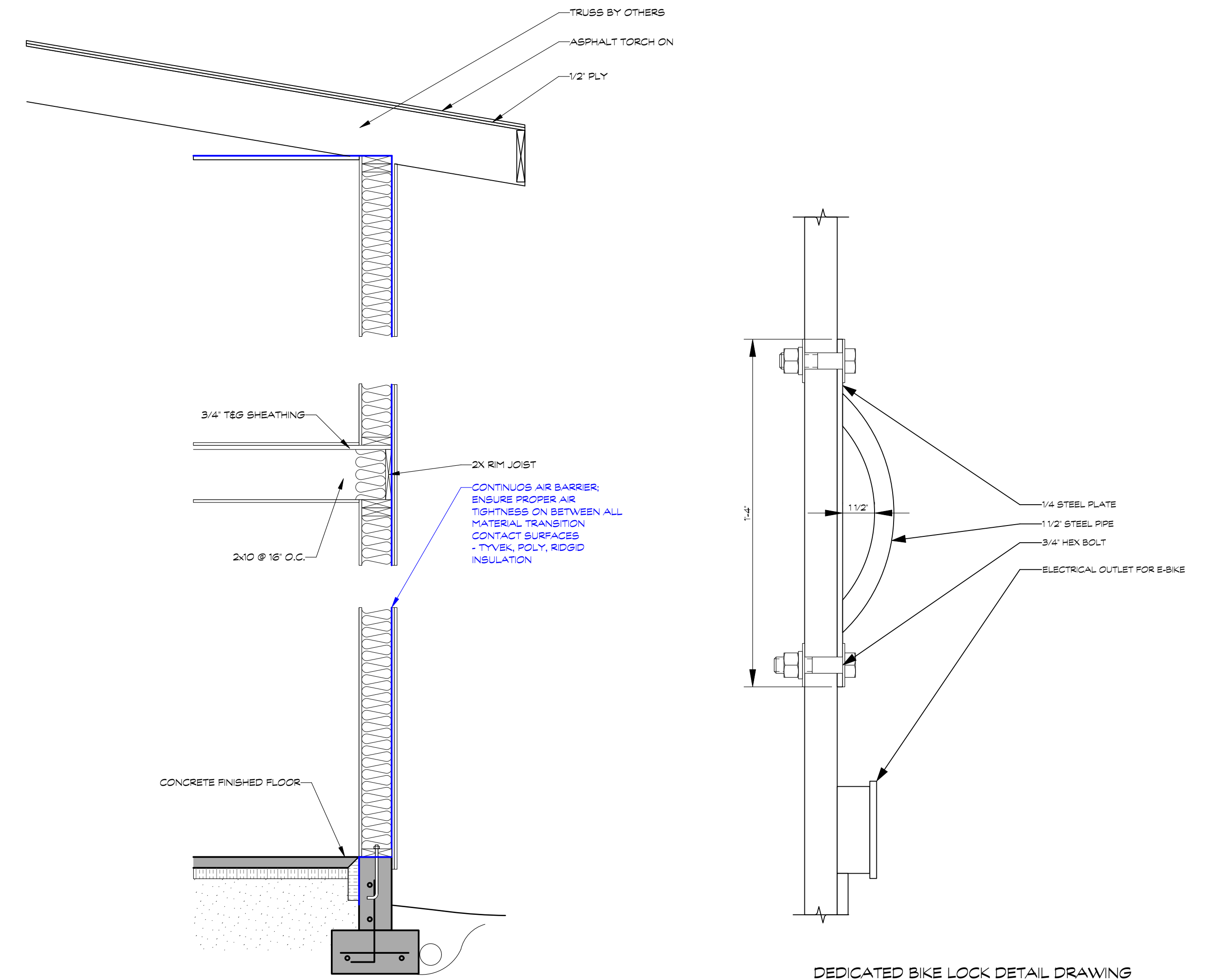
VERSION FOR RE-ZONING PURPOSES

1.FE801/21 Concept Design for Review	
Issue Notes	
PERSPECTIVE	Sheet No. 3
Project Location 1598 Piercy Rd., Courtenay, BC	
Project Title NINE PEAKS	
Sheet Title FLOOR PLANS	



ROOF PLAN
SCALE: 3/8" = 1'-0"

(1%) COVERED PATIO FLAT ROOF AREA = ~248 SQ FT
 (1%) FLAT ROOF AREA = ~541 SQ FT
 (9.5%) SHED ROOF AREA = ~880 SQ FT



TYPICAL WALL SECTION
SCALE 3/4" = 1'-0"

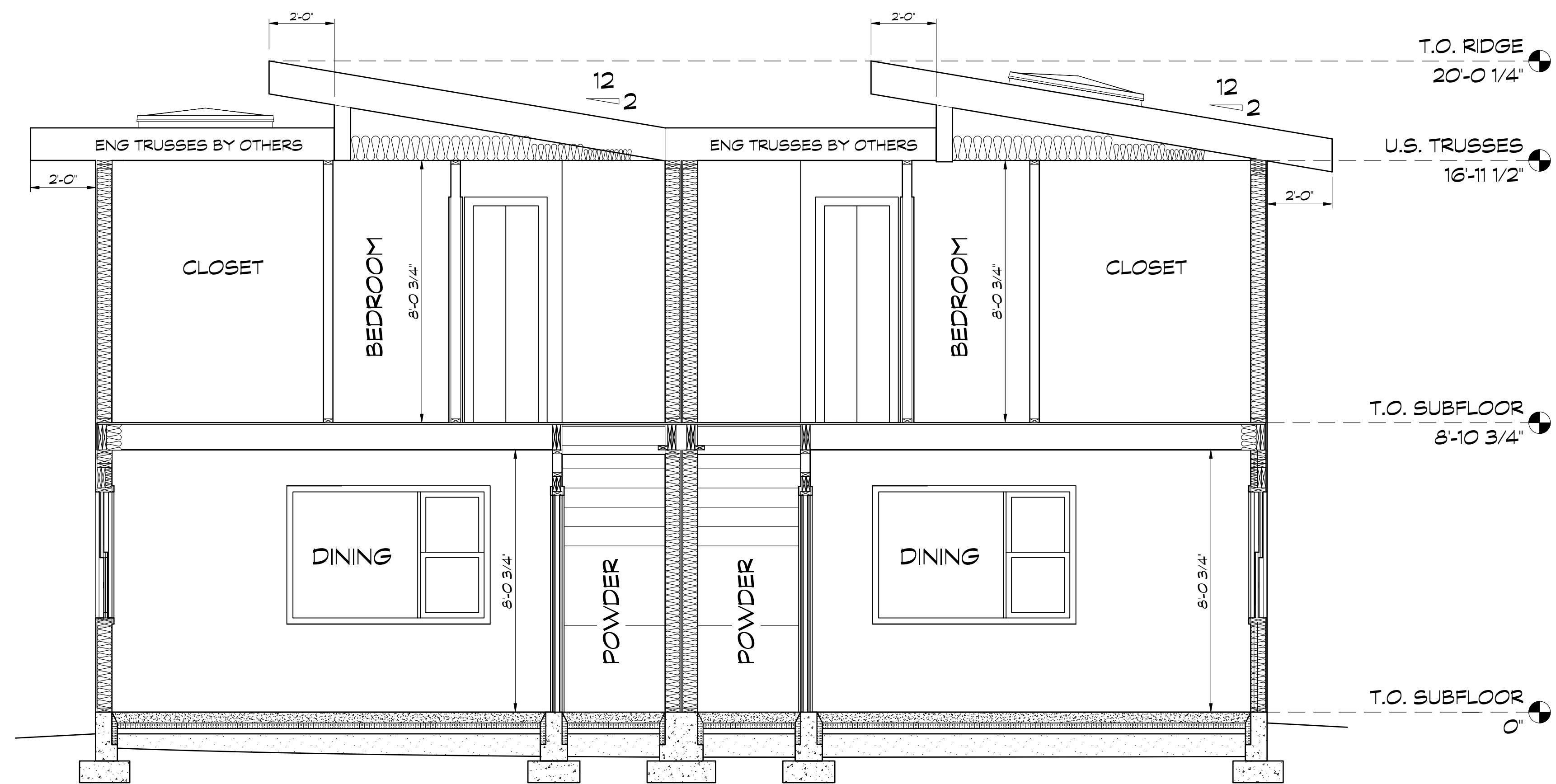
DEDICATED BIKE LOCK DETAIL DRAWING
SCALE: 3" = 1'-0"

VERSION FOR RE-ZONING PURPOSES

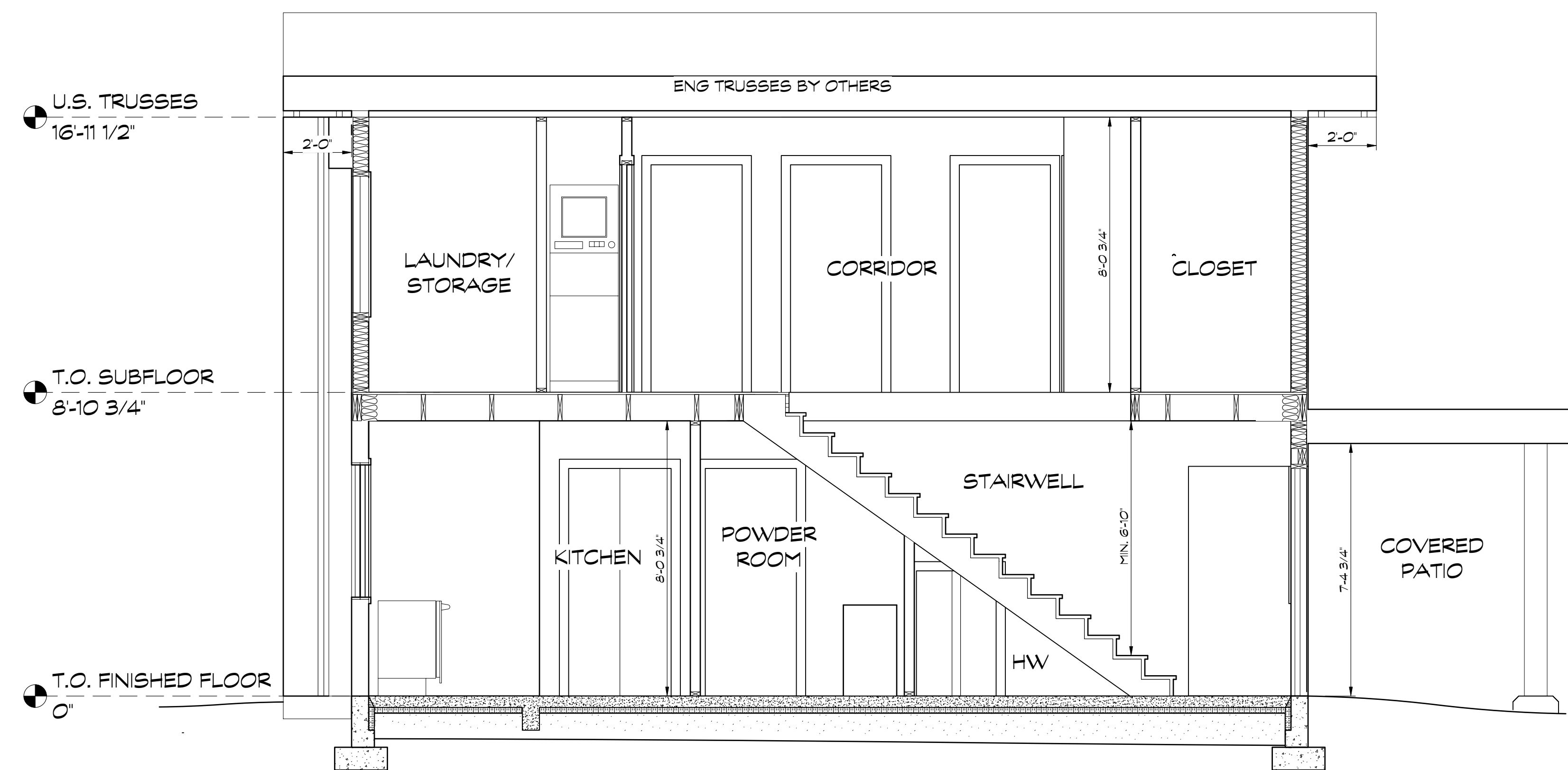
1.FEB01/21 Concept Design for Review	
Issue Notes	
PERSPECTIVE	Sheet No. 4
Project Location 1598 Piercy Rd., Courtenay, BC	
Project Title NINE PEAKS	
Sheet Title ROOF PLAN	

SEALED POLYETHYLENE AIR BARRIER - CODE REFERENCES
 9.36.2.9. AIR TIGHTNESS
 9.36.2.10. CONSTRUCTION OF AIR BARRIER DETAILS

- ENSURE AIR BARRIER IS CONTINUOUS AND AIR TIGHT AT:
- PARTITION WALLS AT EXTERIOR WALLS AND CEILING
 - RECESSED LIGHTS
 - DRYER AND RANGE HOOD EXHAUSTS
 - EXTERIOR DOOR AND WINDOW PENETRATIONS
 - BATHROOM CEILING FANS
 - PLUMBING PENETRATIONS
 - MECHANICAL PENETRATIONS
 - INTERIOR AND EXTERIOR ELECTRICAL PENETRATIONS
 - RM JOISTS
 - FOUNDATION WALL TRANSITION



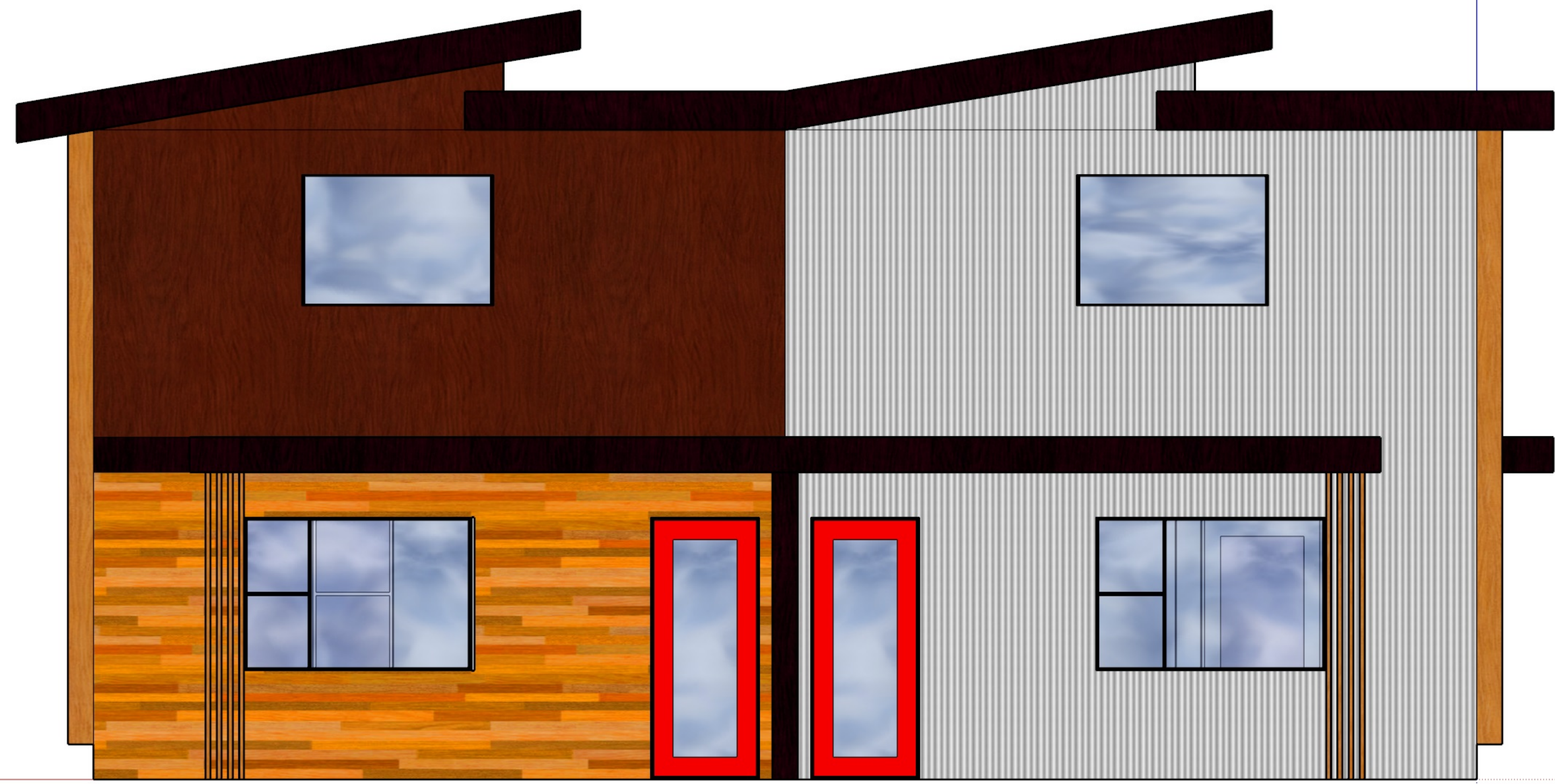
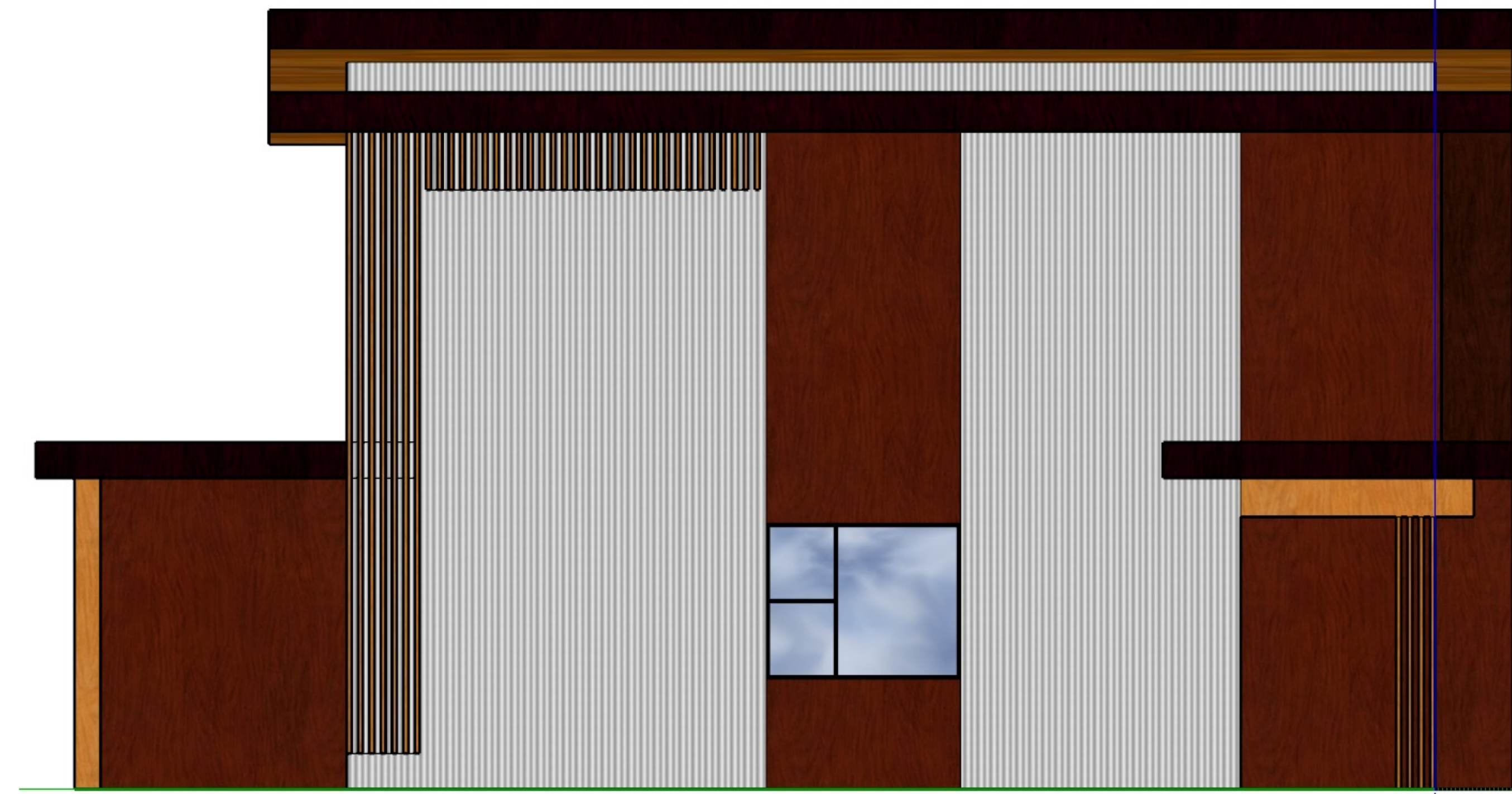
SECTION A
 SCALE: 3/8" = 1'-0"











SECTION B
 SCALE: 3/8" = 1'-0"

VERSION FOR RE-ZONING PURPOSES

1.FEB01/21 Concept Design for Review	
Issue Notes	
PERSPECTIVE	Sheet No. 5
Project Location 1598 Piercy Rd., Courtenay, BC	
Project Title NINE PEAKS	
Sheet Title SECTIONS	

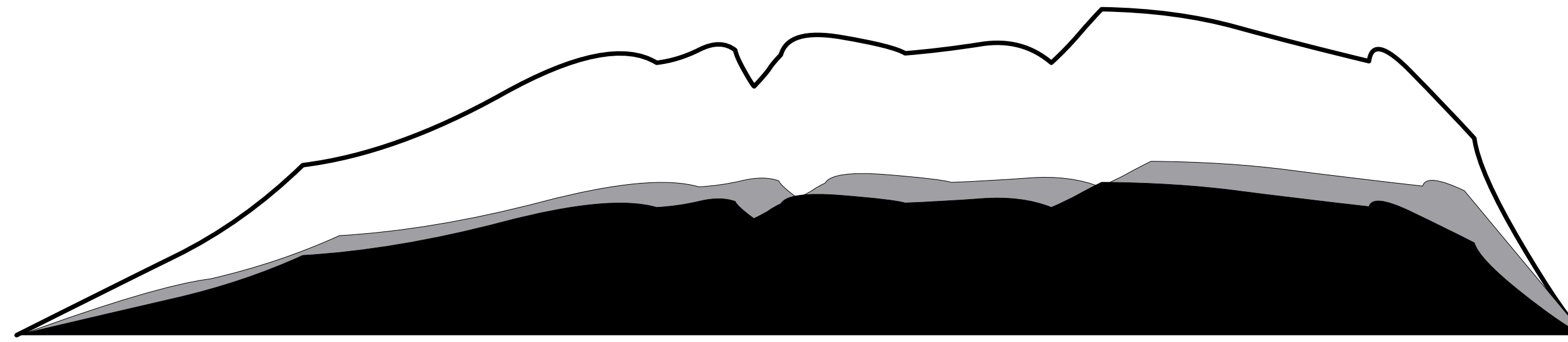


Materials Legend

 Wood Tone Fibre Cement Panel	 Fir Timber Framing and Ornamental Wooden Slats
 Cedar Siding	 Glazing
 Metal Siding	 Accent Paint
 Paint Grade Fibre Cement Panel	 Torch on Roofing Membrane

VERSION FOR RE-ZONING PURPOSES

1.FE001/21 Concept Design for Review	
Issue Notes	
PERSPECTIVE	Sheet No. 6
Drawn by MM	
PERSPECTIVE DESIGN BUILD LTD.	
Project Location 1590 Piercy Rd., Courtenay, BC	
Project Title NINE PEAKS	
Sheet Title ELEVATIONS	



NINE PEAKS

1590 PIERCY AVENUE, COURTENAY



**DRAWING INDEX
ARCHITECTURAL**

- P1 SITE PLAN
- P2 FOUNDATION PLAN
- P3 FLOOR PLANS
- P4 2ND FLOOR PLANS
- P5 ROOF PLAN
- P6 SECTIONS
- P7 ELEVATIONS



**PERSPECTIVE DESIGN
BUILD LTD.**
1160 4TH ST., COURTENAY
BRITISH COLUMBIA V9N 1H8

**FOURPLEX
DETAILED DESIGN**

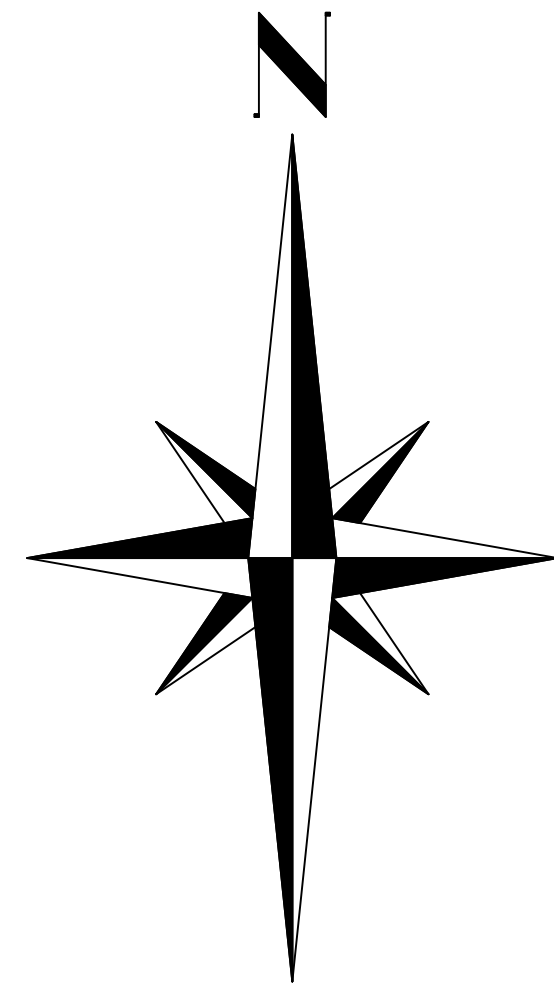
**TO SCALE ON 36X24
PRINT FORMAT ONLY**

COVER SHEET	0.0
REVISION	1

VERSION FOR RE-ZONING PURPOSES

COURTENAY DISTRICT, PLAN 4764 TOPOGRAPHIC SURVEY PLAN

1590 PIERCY AVENUE, COURTENAY
SCALE 1 : 150 (METRIC)



PROJECT DATA

ZONING R2 (TO BE RE-ZONED R3)
LOT AREA 0.5 ACRES (2030.5 SQ M)

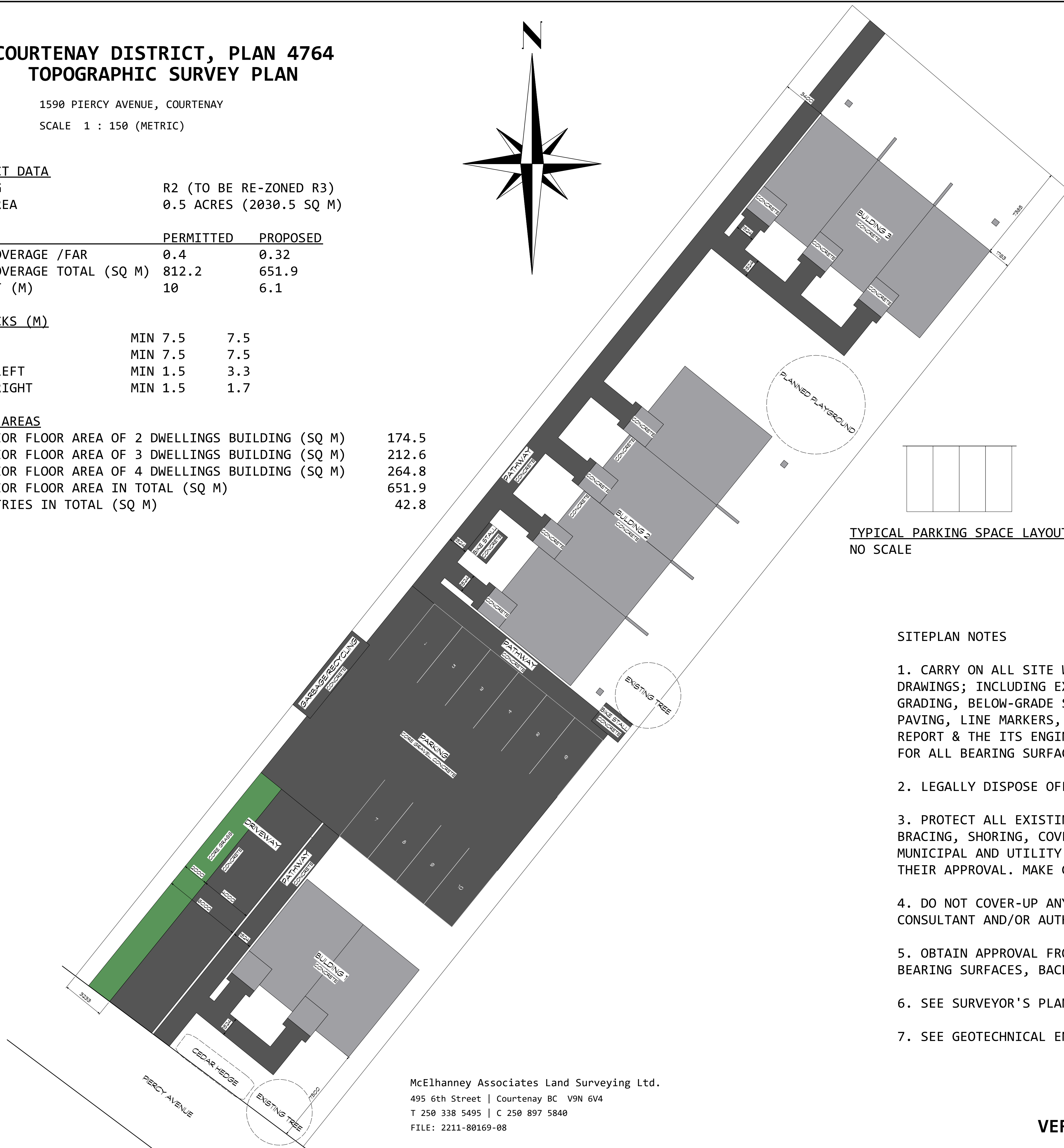
ITEMS	PERMITTED	PROPOSED
LOT COVERAGE /FAR	0.4	0.32
LOT COVERAGE TOTAL (SQ M)	812.2	651.9
HEIGHT (M)	10	6.1

SETBACKS (M)

FRONT	MIN 7.5	7.5
REAR	MIN 7.5	7.5
SIDE LEFT	MIN 1.5	3.3
SIDE RIGHT	MIN 1.5	1.7

FLOOR AREAS

EXTERIOR FLOOR AREA OF 2 DWELLINGS BUILDING (SQ M)	174.5
EXTERIOR FLOOR AREA OF 3 DWELLINGS BUILDING (SQ M)	212.6
EXTERIOR FLOOR AREA OF 4 DWELLINGS BUILDING (SQ M)	264.8
EXTERIOR FLOOR AREA IN TOTAL (SQ M)	651.9
(9)ENTRIES IN TOTAL (SQ M)	42.8



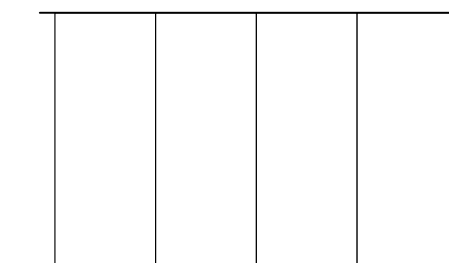
OFF-STREET PARKING SPACES (PER DWELLING INCL.10% VISITOR PARKING)	REQUIRED	PROPOSED
	14 (1.5)	10 (1.1)

BICYCLE PARKING ADDITION

LONG TERM PARKING SPACES	-	30
SHORT TERM PARKING SPACES	-	8

OTHER PARKING LOT SPECIFICATIONS SHALL FOLLOW DIVISION 7, BYLAW 2500 (2007) REQUIRMENTS

SEE PARKING STUDY ISSUED AND SIGNED BY WATT CONSULTING GROUP, FILE NUMBER 2785.B01; MARCH 30, 2020



TYPICAL PARKING SPACE LAYOUT
NO SCALE

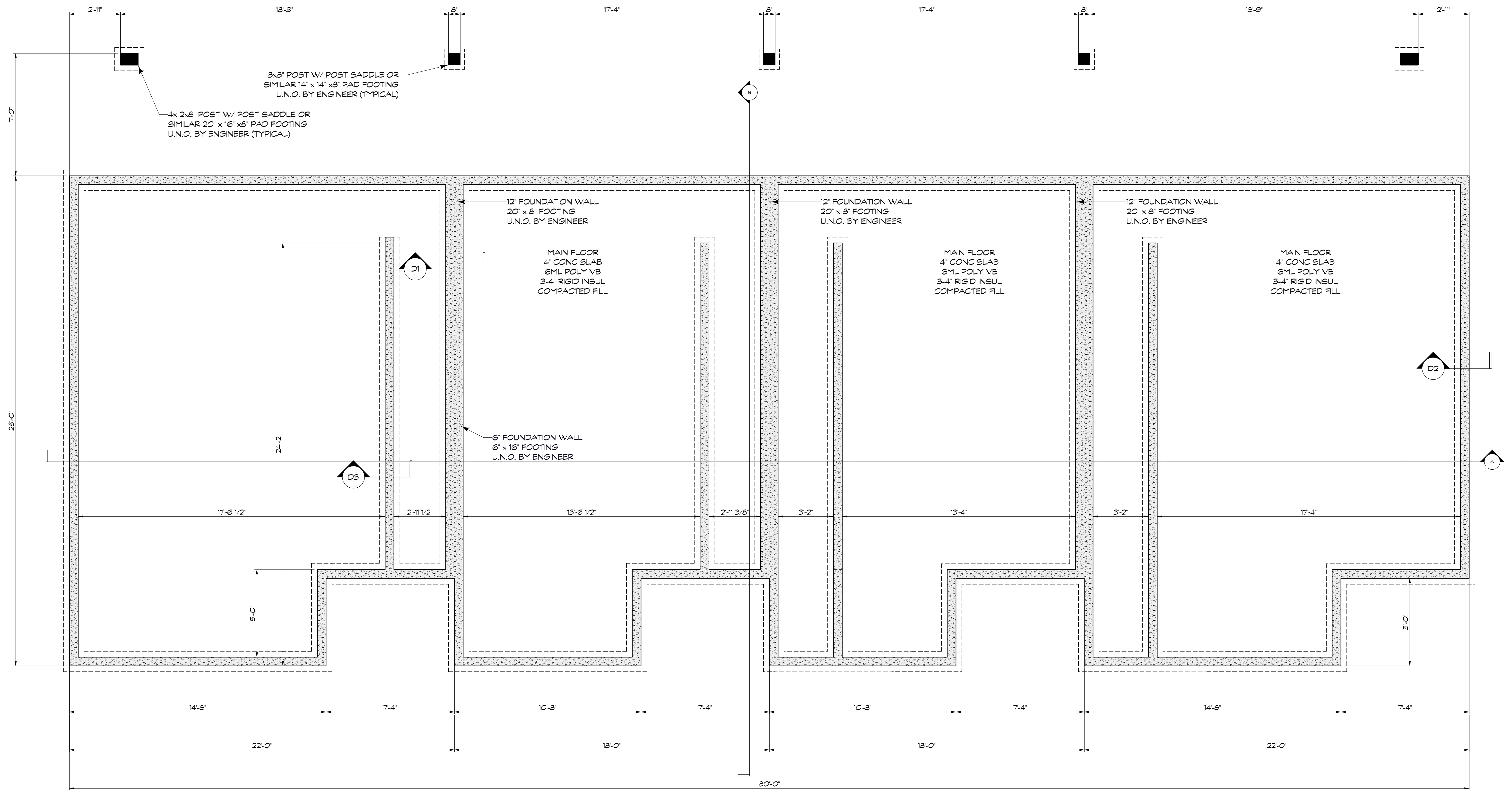
SITEPLAN NOTES

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McElhanney Associates Land Surveying Ltd.
495 6th Street | Courtenay BC V9N 6V4
T 250 338 5495 | C 250 897 5840
FILE: 2211-80169-08

VERSION FOR RE-ZONING PURPOSES

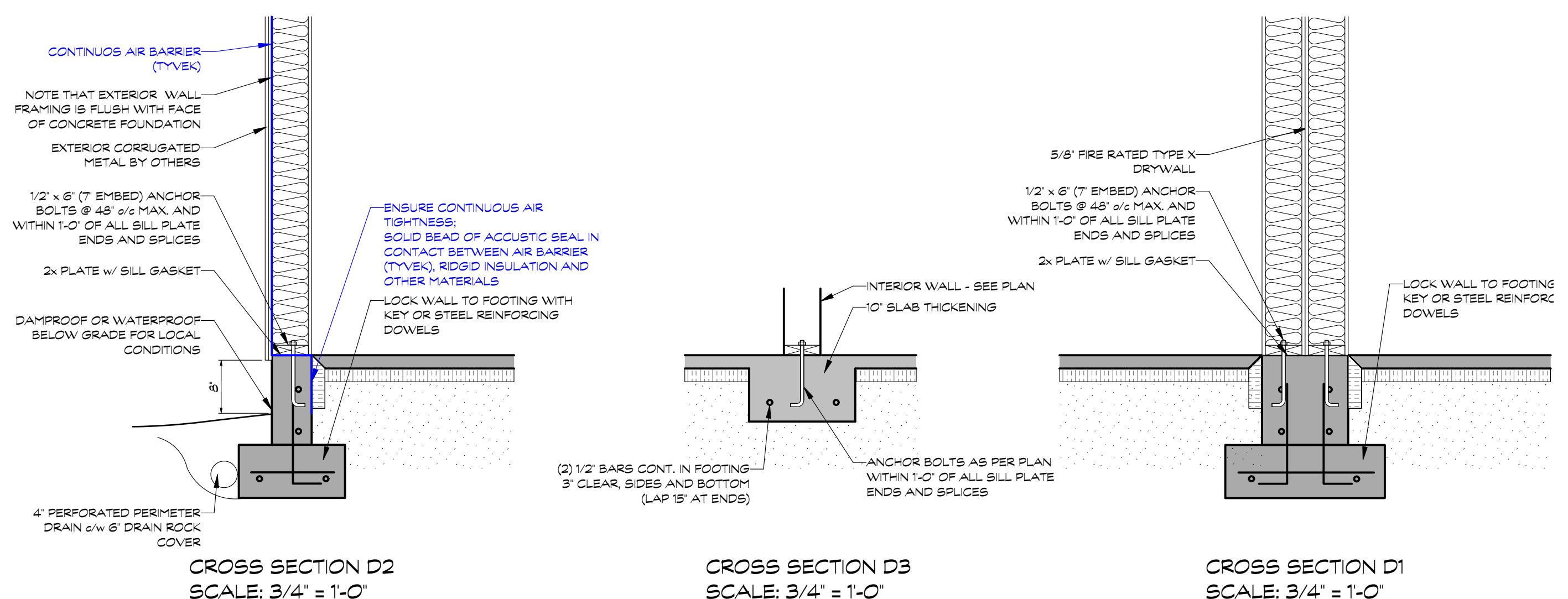
1.FEB01/21 Concept Design for Review	
Issue Notes	
PERSPECTIVE	Sheet No. 1
Project Location 1590 Piercy Rd., Courtenay, BC	
Project Title NINE PEAKS	
Sheet Title SITE PLAN	



FOUNDATION PLAN
SCALE: 3/8" = 1'-0"

~68 LINEAR FT PER SURROUNDED DWELLING
~76 LINEAR FT PER CORNER DWELLING

NOTE:
ALL POINT LOADS MUST BE ASSESSED BY AN
ENGINEER AND FOOTING SIZES INCREASE AS
REQUIRED TO SUPPORT LOADS



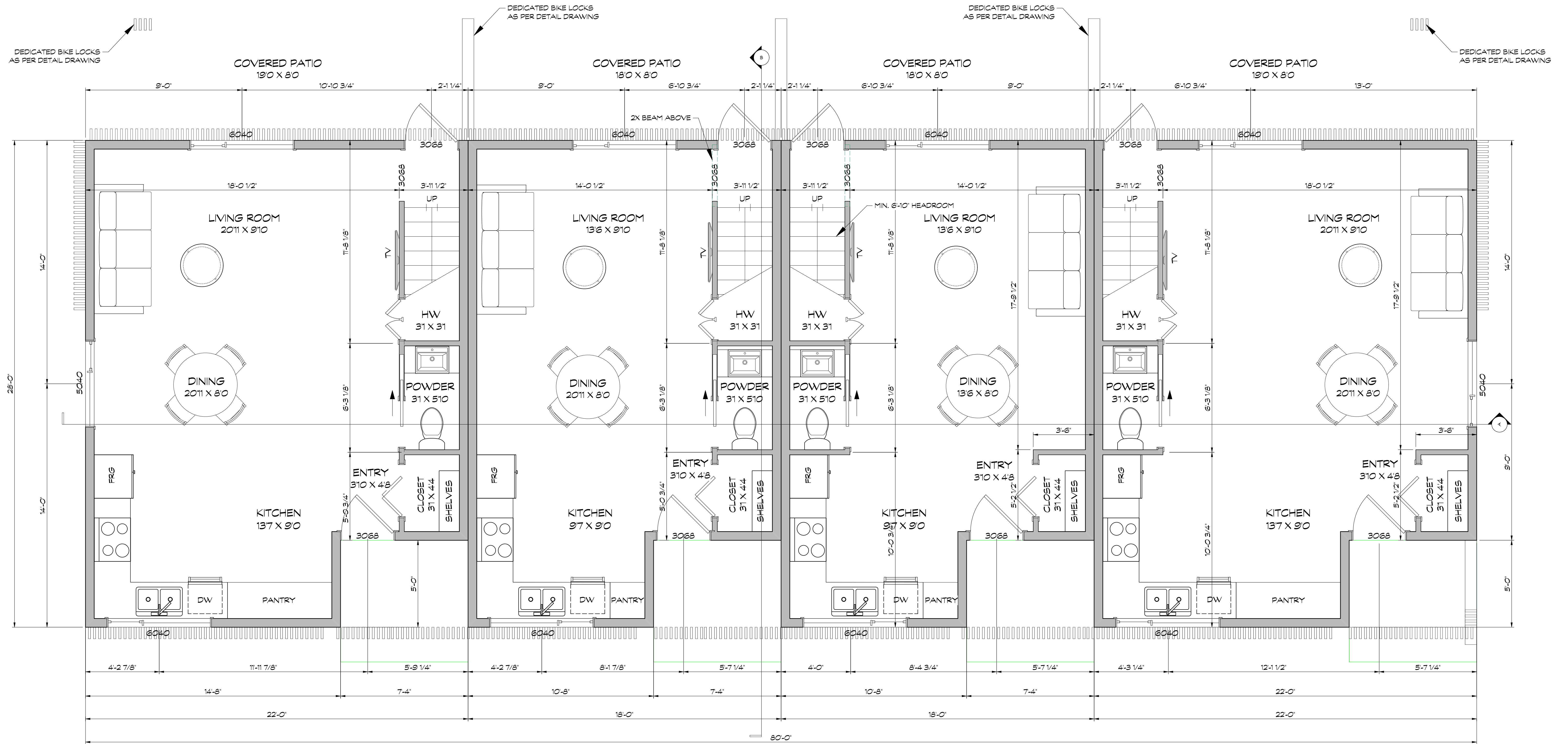
CROSS SECTION D2
SCALE: 3/4" = 1'-0"

CROSS SECTION D3
SCALE: 3/4" = 1'-0"

CROSS SECTION D1
SCALE: 3/4" = 1'-0"

VERSION FOR RE-ZONING PURPOSES

1.FEB01/21 Concept Design for Review	
ISSUE NOTES	
PERSPECTIVE	Sheet No. 2
Project Location 1590 Piercy Rd., Courtenay, BC	
Project Title NINE PEAKS	
Sheet Title FOUNDATION PLAN	



MAIN FLOOR PLAN

SCALE: 3/8" = 1'-0"

467 SQ FT (EXTERIOR FLOOR AREA PER 2BDRM DWELLING)

580 SQ FT (EXTERIOR FLOOR AREA PER 3BDRM DWELLING)

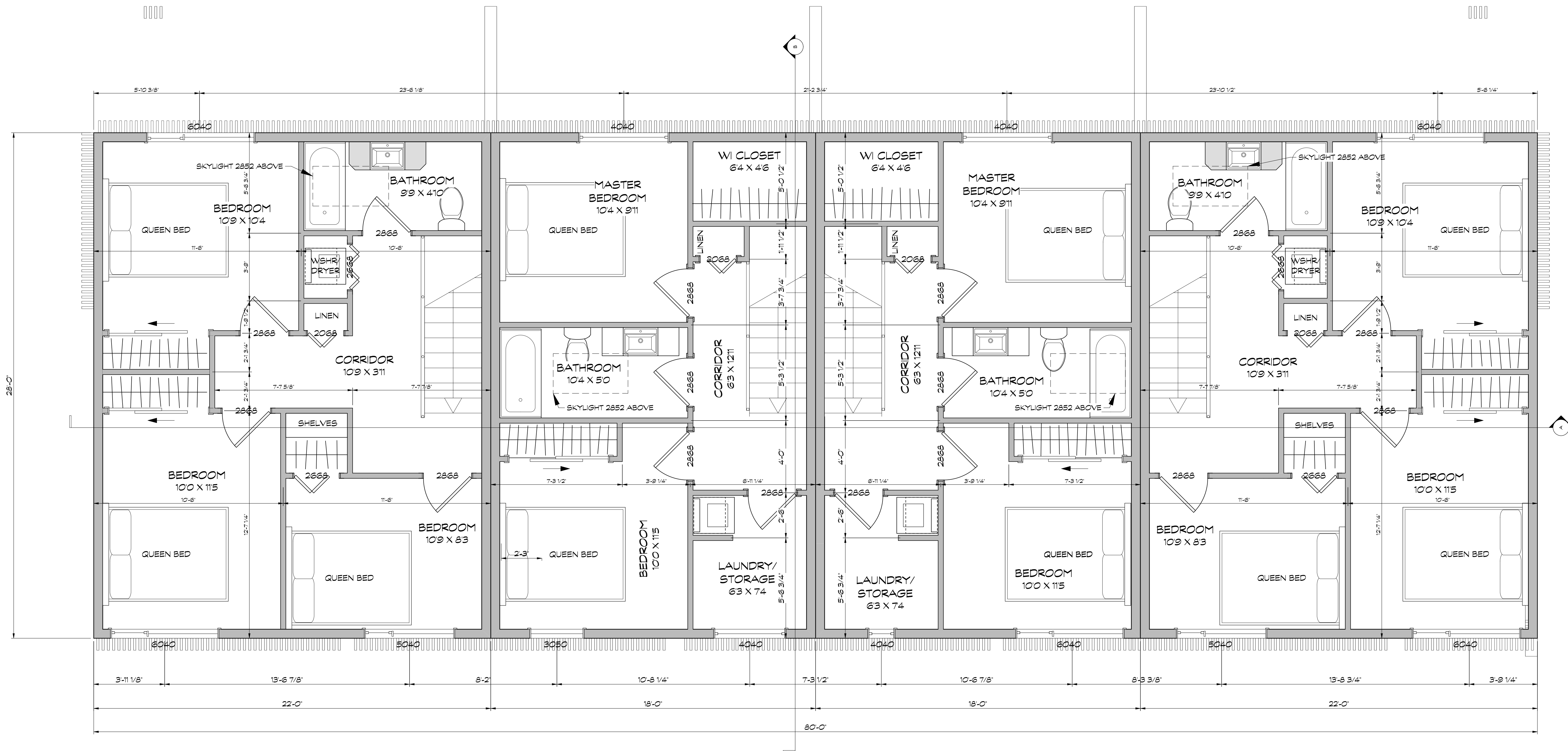
51 SQ FT (ENTRY SLAB PER DWELLING)

2094 SQ FT (EXTERIOR FLOOR AREA IN TOTAL PER BUILDING)

204 SQ FT (ENTRY SLABS IN TOTAL PER BUILDING)

VERSION FOR RE-ZONING PURPOSES

3.FE801/21 Concept Design for Review	
Issue Notes	
PERSPECTIVE	Sheet No. 3
Project Location 1598 Piercy Rd., Courtenay, BC	
Project Title NINE PEAKS	
Sheet Title FLOOR PLANS	



UPPER FLOOR PLAN

SCALE: 3/8" = 1'-0"

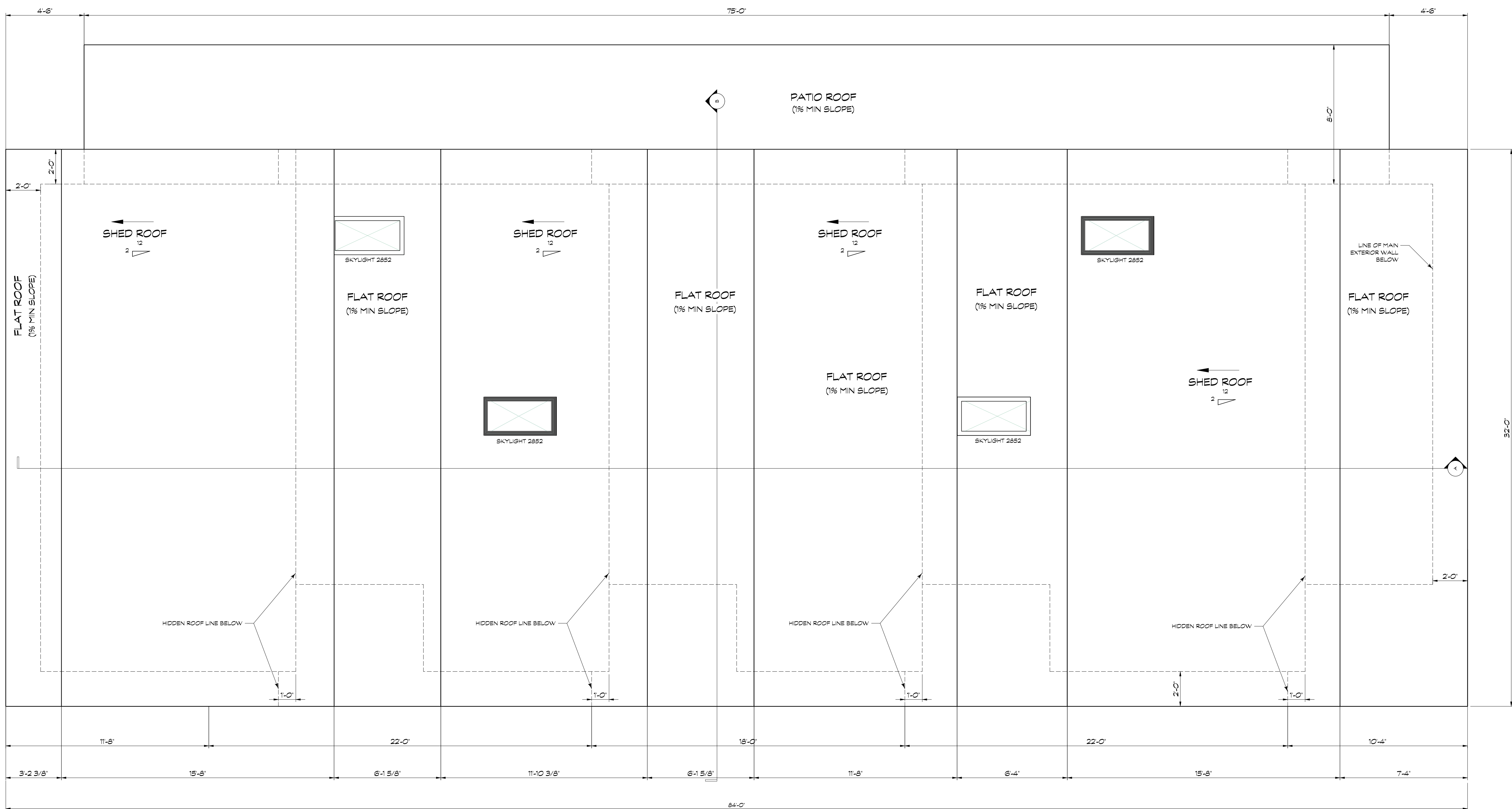
472 SQ FT (EXTERIOR FLOOR AREA IN PER 2BDRM DWELLING)

616 SQ FT (EXTERIOR FLOOR AREA IN PER 3BDRM DWELLING)

2176 SQ FT (EXTERIOR FLOOR AREA IN TOTAL PER BUILDING)

VERSION FOR RE-ZONING PURPOSES

1.FE801/21 Concept Design for Review	
Issue Notes	
PERSPECTIVE	Sheet No. 4
Project Location 1598 Piercy Rd., Courtenay, BC	
Project Title NINE PEAKS	
Sheet Title 2ND FLOOR PLANS	

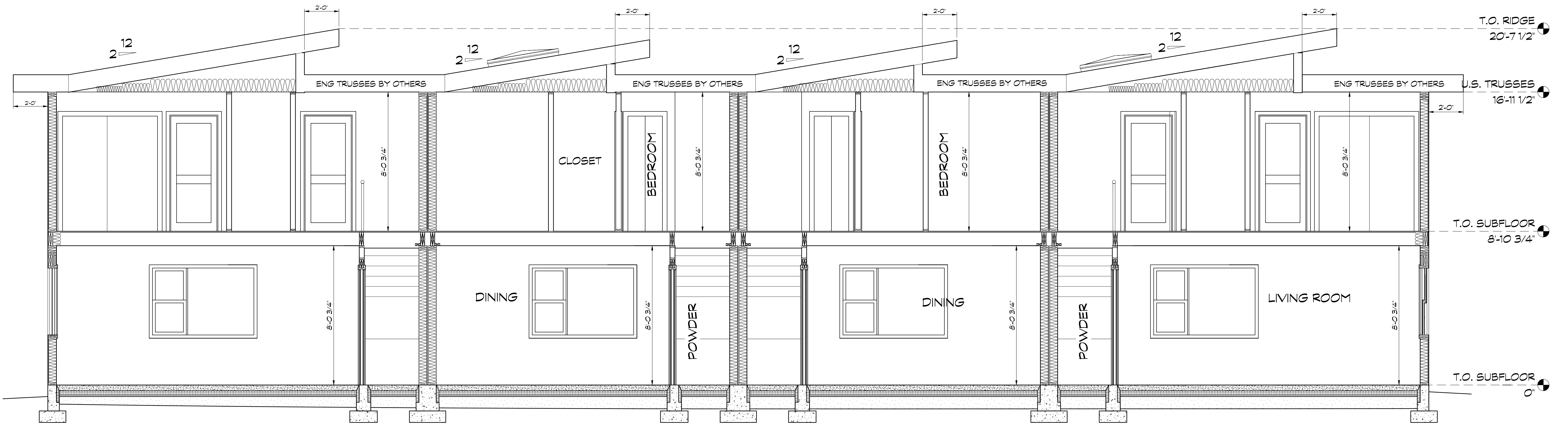


ROOF PLAN
 SCALE: 3/8" = 1'-0"

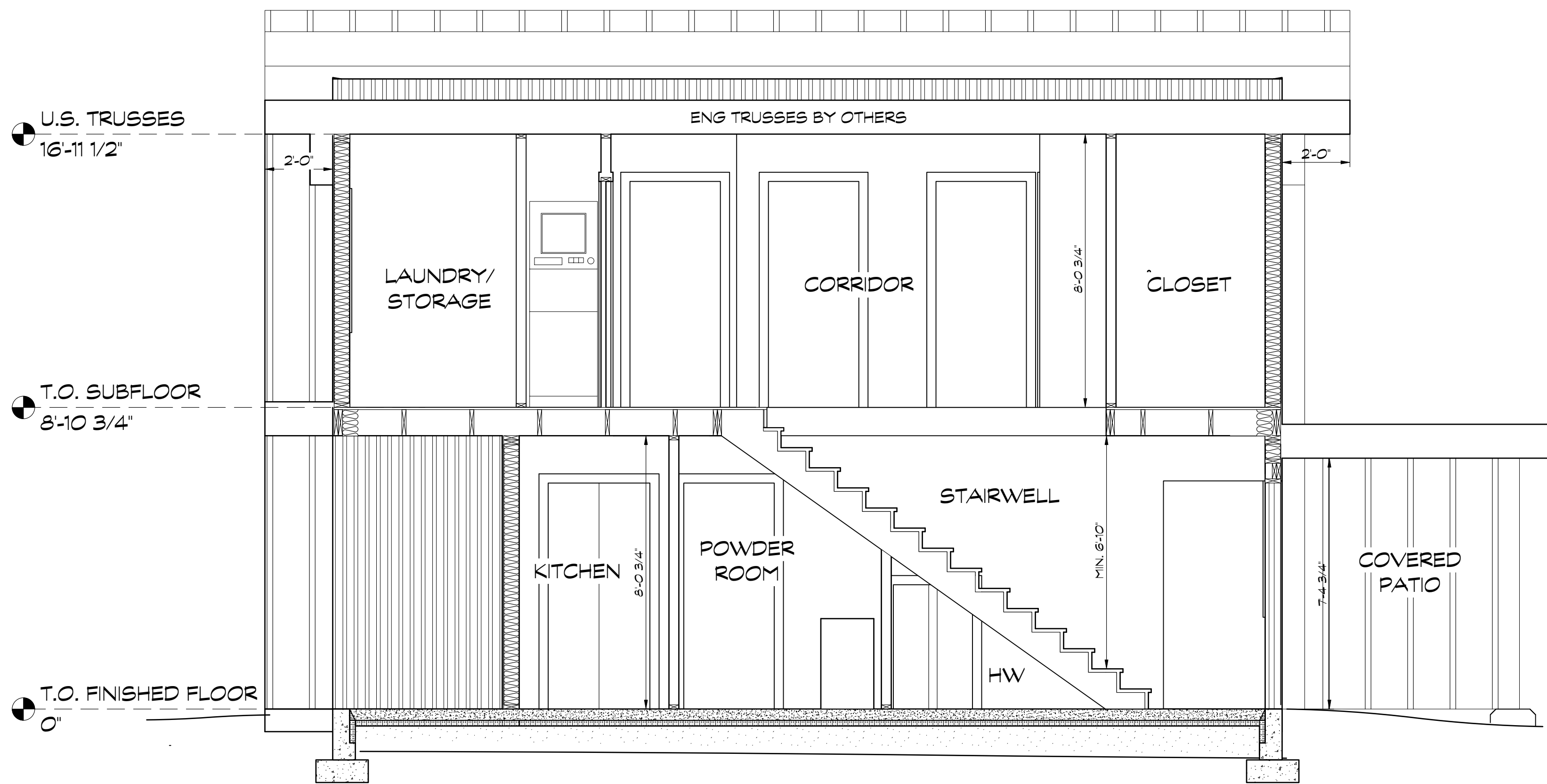
(1%) COVERED PATIO FLAT ROOF AREA = ~600 SQ FT
 (1%) FLAT ROOF AREA = ~1310 SQ FT
 (9.5°) SHED ROOF AREA = ~1569 SQ FT

VERSION FOR RE-ZONING PURPOSES

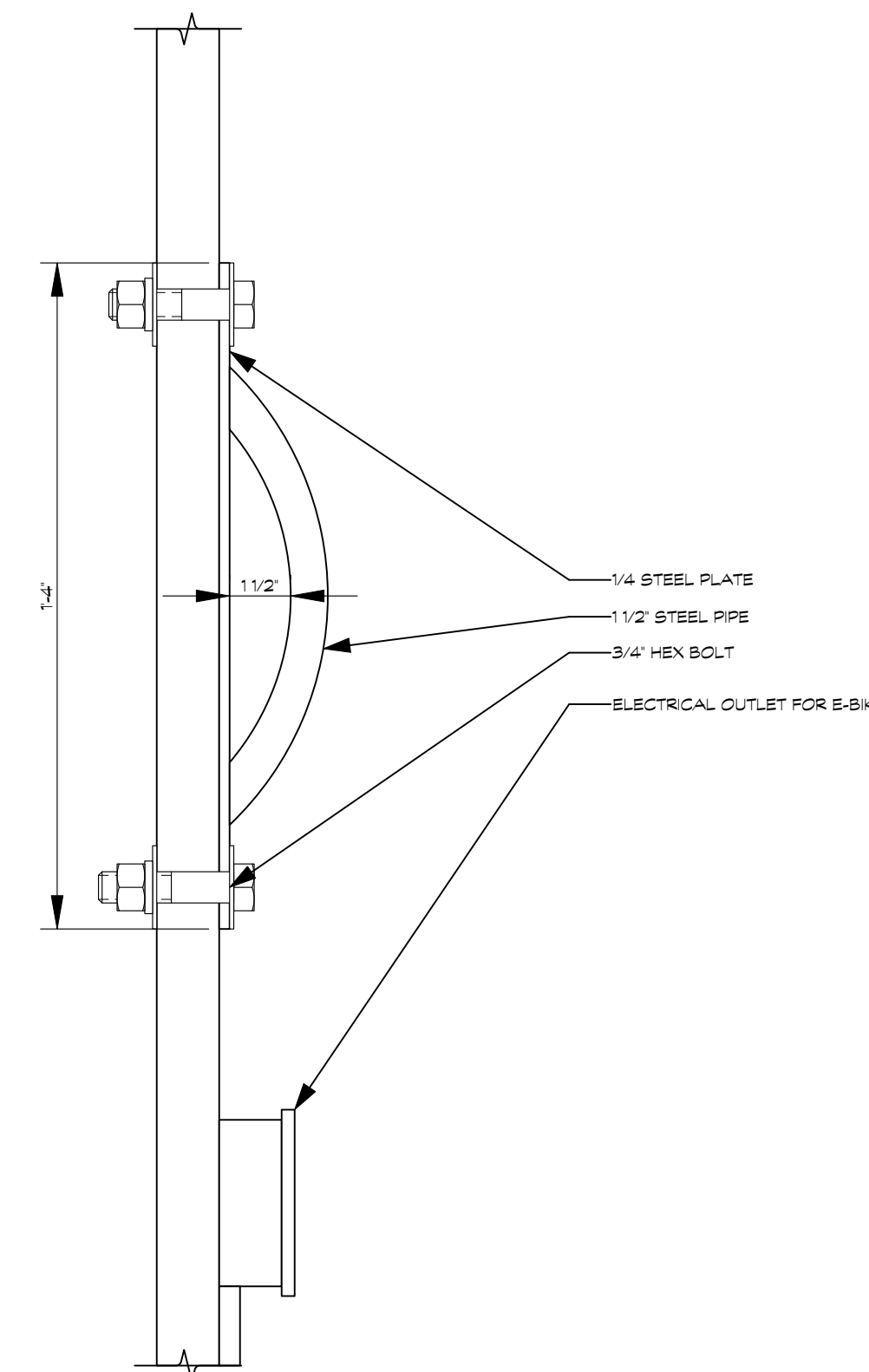
1.FE801/21 Concept Design for Review	
ISSUE NOTES	
PERSPECTIVE	Sheet No. 5
Project Location 1598 Piercy Rd., Courtenay, BC	
Project Title NINE PEAKS	
Sheet Title ROOF PLAN	



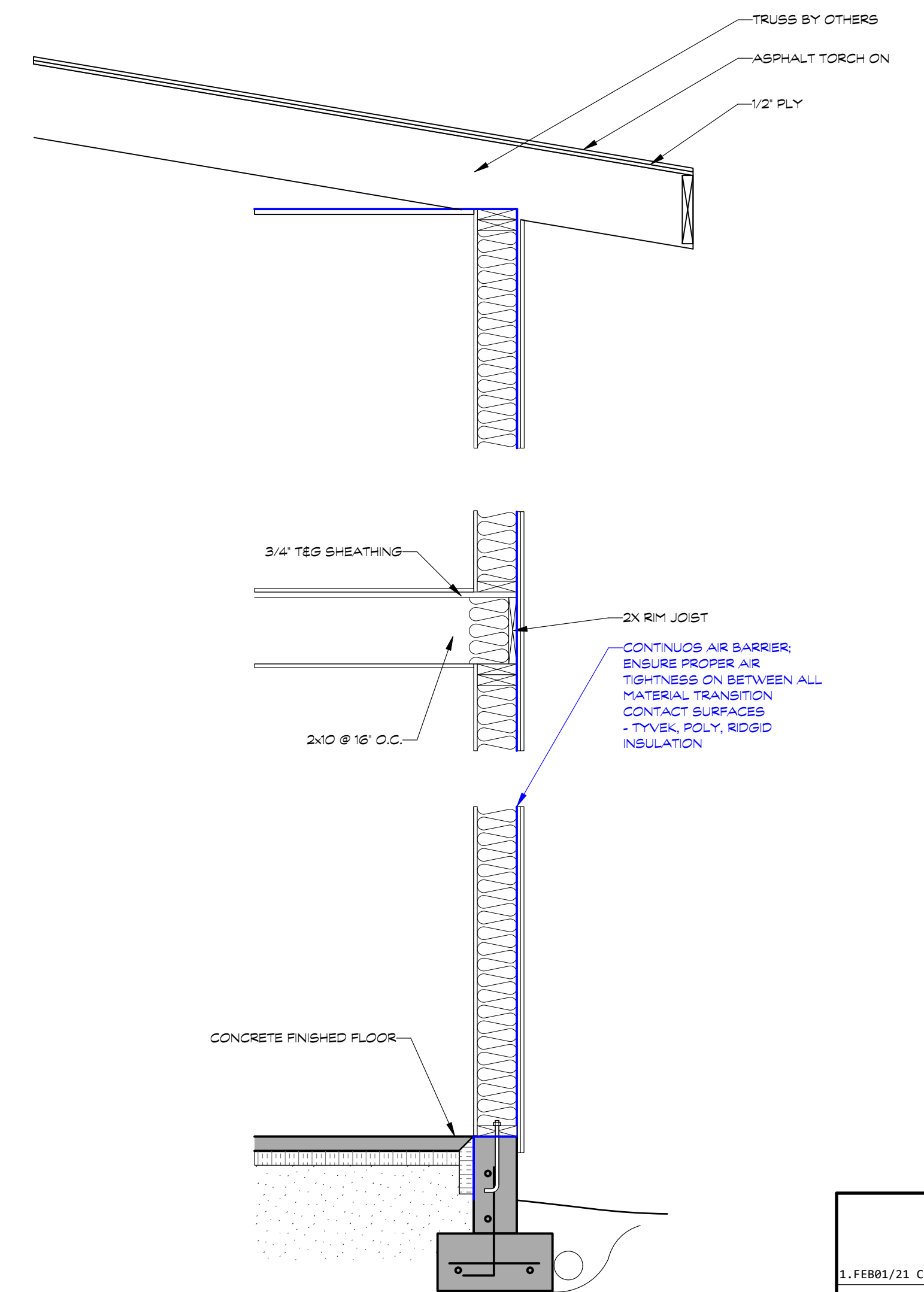
SECTION A
SCALE: 3/8" = 1'-0"



SECTION B
SCALE: 3/8" = 1'-0"



DEDICATED BIKE LOCK DETAIL DRAWING
SCALE: 3" = 1'-0"



TYPICAL WALL SECTION
SCALE 3/4" = 1'-0"

VERSION FOR RE-ZONING PURPOSES

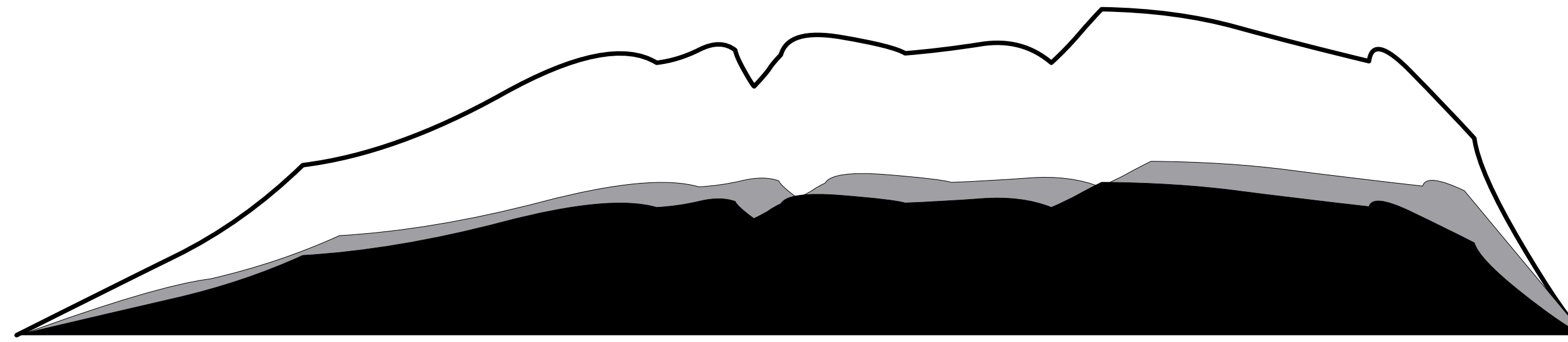
1.FEB01/21 Concept Design for Review	
Issue Notes	
PERSPECTIVE	Sheet No. 6
Project Location 1598 Piercy Rd., Courtenay, BC	
Project Title NINE PEAKS	
Sheet Title SECTIONS	



Materials Legend

1.FE801/21 Concept Design for Review	
Issue Notes	
PERSPECTIVE	Sheet No. 7
PERSPECTIVE DESIGN BUILD LTD.	
Project Location 1590 Piercy Rd., Courtenay, BC	
Project Title NINE PEAKS	
Sheet Title ELEVATIONS	

VERSION FOR RE-ZONING PURPOSES



NINE PEAKS

1590 PIERCY AVENUE, COURTENAY



**DRAWING INDEX
ARCHITECTURAL**

- P1 SITE PLAN
- P2 FOUNDATION PLAN
- P3 FLOOR PLANS
- P4 2ND FLOOR PLANS
- P5 ROOF PLAN
- P6 SECTIONS
- P7 ELEVATIONS



**PERSPECTIVE DESIGN
BUILD LTD.**
1160 4TH ST., COURTENAY
BRITISH COLUMBIA V9N 1H8

**TRIPLEX
DETAILED DESIGN**

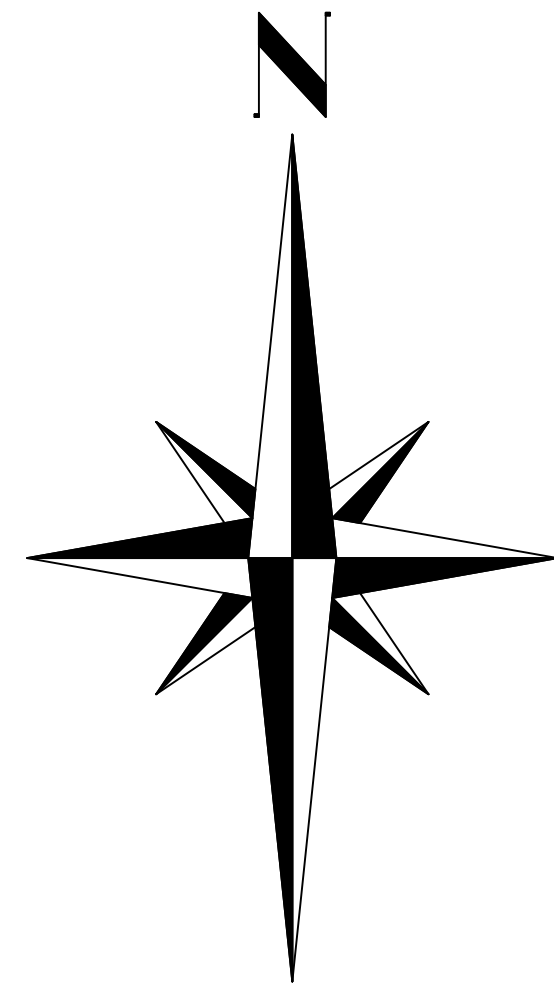
**TO SCALE ON 36X24
PRINT FORMAT ONLY**

**COVER SHEET 0.0
REVISION**

VERSION FOR RE-ZONING PURPOSES

COURTENAY DISTRICT, PLAN 4764 TOPOGRAPHIC SURVEY PLAN

1590 PIERCY AVENUE, COURTENAY
SCALE 1 : 150 (METRIC)



PROJECT DATA

ZONING R2 (TO BE RE-ZONED R3)
LOT AREA 0.5 ACRES (2030.5 SQ M)

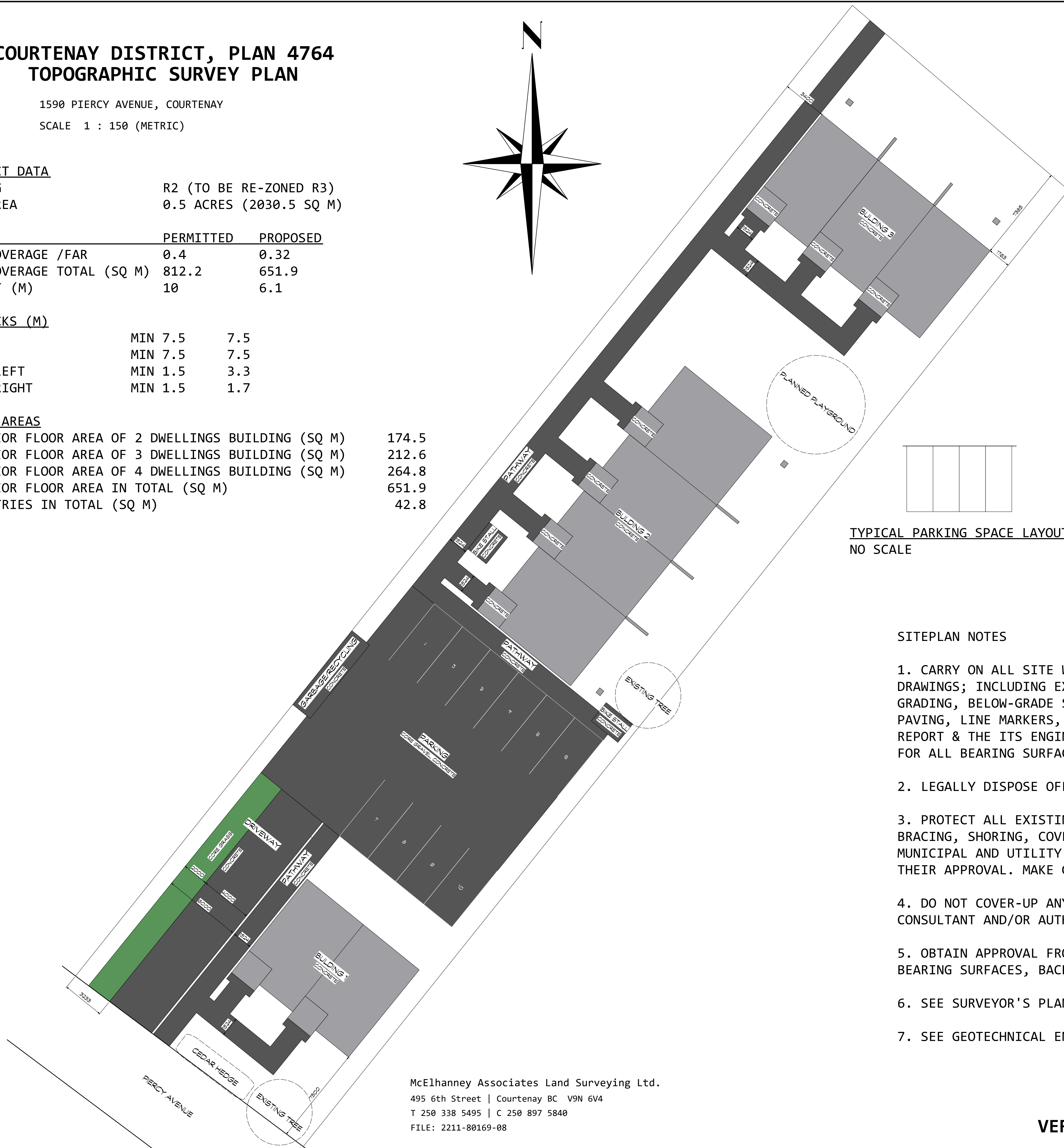
ITEMS	PERMITTED	PROPOSED
LOT COVERAGE /FAR	0.4	0.32
LOT COVERAGE TOTAL (SQ M)	812.2	651.9
HEIGHT (M)	10	6.1

SETBACKS (M)

FRONT	MIN 7.5	7.5
REAR	MIN 7.5	7.5
SIDE LEFT	MIN 1.5	3.3
SIDE RIGHT	MIN 1.5	1.7

FLOOR AREAS

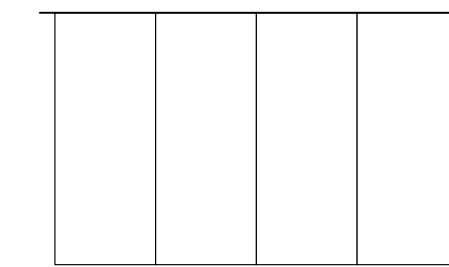
EXTERIOR FLOOR AREA OF 2 DWELLINGS BUILDING (SQ M)	174.5
EXTERIOR FLOOR AREA OF 3 DWELLINGS BUILDING (SQ M)	212.6
EXTERIOR FLOOR AREA OF 4 DWELLINGS BUILDING (SQ M)	264.8
EXTERIOR FLOOR AREA IN TOTAL (SQ M)	651.9
(9)ENTRIES IN TOTAL (SQ M)	42.8



OFF-STREET PARKING SPACES (PER DWELLING INCL.10% VISITOR PARKING)	REQUIRED	PROPOSED
	14 (1.5)	10 (1.1)

BICYCLE PARKING ADDITION

LONG TERM PARKING SPACES	-	30
SHORT TERM PARKING SPACES	-	8



TYPICAL PARKING SPACE LAYOUT
NO SCALE

OTHER PARKING LOT SPECIFICATIONS SHALL FOLLOW DIVISION 7, BYLAW 2500 (2007) REQUIRMENTS

SEE PARKING STUDY ISSUED AND SIGNED BY WATT CONSULTING GROUP, FILE NUMBER 2785.B01; MARCH 30, 2020

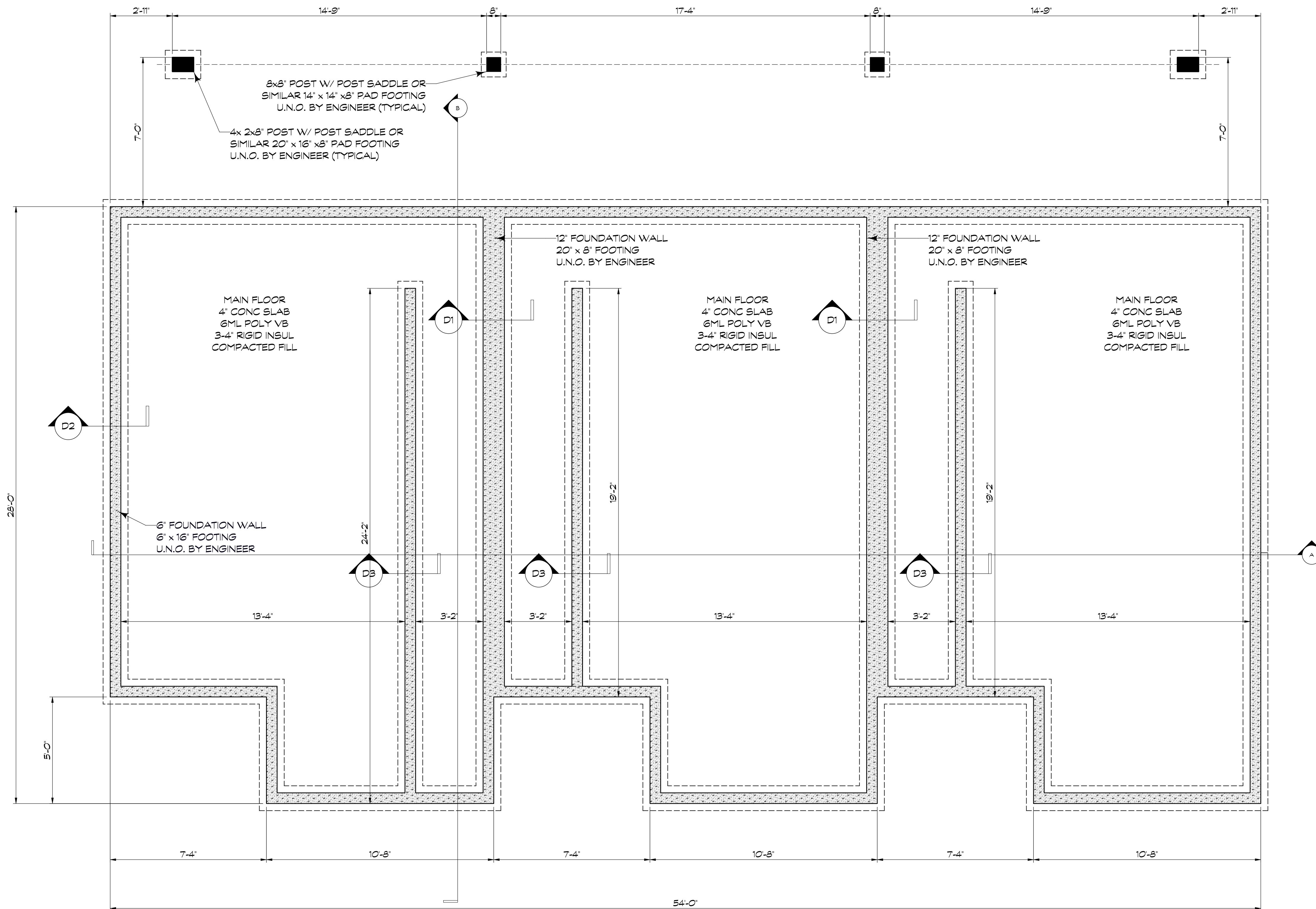
SITEPLAN NOTES

1. CARRY ON ALL SITE WORK SHOWN ON OR READILY INFERRABLE FROM THE PERMIT DRAWINGS; INCLUDING EXCAVATION, DEWATERING, BACKFILLING, ROUGH & FINISHED GRADING, BELOW-GRADE SERVICES, PLANTING, SODDING, FENCING, CURBING, PAVING, LINE MARKERS, SIGNAGE, ETC. CO-ORDINATE ALL WORK WITH THE SOIL REPORT & THE ITS ENGINEER. PROVIDE INSPECTIONS, TEST AND REPORTS, FOR ALL BEARING SURFACES, BACKFILL MATERIALS, AND COMPACTION.
2. LEGALLY DISPOSE OFF-SITE ALL EXCAVATED MATERIAL, & ITEMS REQUIRING REMOVAL.
3. PROTECT ALL EXISTING PUBLIC PROPERTY AND UTILITIES. PROVIDE HOARDINGS, BRACING, SHORING, COVERS, ETC. AS REQUIRED. CONTACT ALL REQUISITE MUNICIPAL AND UTILITY AUTHORITIES FOR LOCATES, AND CARRY OUT WORK TO THEIR APPROVAL. MAKE GOOD ANY DAMAGE.
4. DO NOT COVER-UP ANY WORK UNTIL PROPERLY INSPECTED BY THE PERTINENT CONSULTANT AND/OR AUTHORITY.
5. OBTAIN APPROVAL FROM GEOTECHNICAL / STRUCTURAL ENGINEER FOR ALL BEARING SURFACES, BACKFILL MATERIALS, AND COMPACTION.
6. SEE SURVEYOR'S PLAN FOR PROPERTY LINE DIMENSIONS AND DIRECTION.
7. SEE GEOTECHNICAL ENGINEER'S REPORT FOR SOIL / BEARING CONDITIONS.

McElhanney Associates Land Surveying Ltd.
495 6th Street | Courtenay BC V9N 6V4
T 250 338 5495 | C 250 897 5840
FILE: 2211-80169-08

VERSION FOR RE-ZONING PURPOSES

1.FEB01/21 Concept Design for Review	
Issue Notes	
PERSPECTIVE	Sheet No. 1
Project Location 1590 Piercy Rd., Courtenay, BC	
Project Title NINE PEAKS	
Sheet Title SITE PLAN	



FOUNDATION PLAN
SCALE: 3/8" = 1'-0"
~68 LINEAR FT PER DWELLING

NOTE:
ALL POINT LOADS MUST BE ASSESSED BY AN ENGINEER AND FOOTING SIZES INCREASE AS REQUIRED TO SUPPORT LOADS

NOTES:

GENERAL NOTES

- ALL WORK SHALL CONFORM TO CURRENT BUILDING CODES AND LOCAL BYLAWS
- WRITTEN DIMENSIONS TAKE PRECEDENCE OVER SCALED DRAWINGS

CONCRETE

- ALL CONCRETE TO HAVE MINIMUM COMPRESSIVE STRENGTH OF 20MPA AT 28 DAYS
- FOOTINGS SHOWN ON THESE DRAWINGS HAVE BEEN DESIGNED FOR SOIL BEARING CAPACITY OF 2500 PSF. LOCAL CONDITIONS AND / OR LOCAL PRACTICE MAY NECESSITATE A MORE STRINGENT FOOTING DESIGN WHICH, MAY REQUIRE CONFIRMATION BY A CERTIFIED STRUCTURAL ENGINEER. THIS WILL BE THE RESPONSIBILITY OF THE OWNER / CONTRACTOR TO PROVIDE.
- CONCRETE FOOTINGS MUST BE PLACED ON UNDISTURBED OR COMPACTED SOIL TO AN ELEVATION BELOW FROST PENETRATION OR ALTERNATIVE METHOD SHALL BE USED TO ENSURE INSULATION OF FOUNDATION AS PER LOCAL BUILDING CODE
- ALL CONCRETE AND MASONRY FOUNDATION WALLS EXCEEDING LIMITS STATED IN BUILDING CODES REQUIRE DESIGN BY A REGISTERED STRUCTURAL ENGINEER
- ALL FOOTINGS TO HAVE 2 ROWS OF MIN. NO. 4 REBAR 3" CLEAR FROM SIDE AND BOTTOM
- ALL WOOD CONTACTING CONCRETE TO BE SEPARATED WITH APPROVED MATERIAL

STRUCTURAL

- ALL WOOD FRAMING TO BE #2 OR BETTER DOUGLAS FIR OR SPRUCE
- ALL UNTELS TO BE 2-2X10 #2 DOUGLAS FIR OR BETTER UNLESS OTHERWISE NOTED
- LAMINATE STUDDING UNDER ALL LOAD BEARING POINTS
- JOISTS ARE TO BE DOUBLED UNDER PARTITIONS
- ALL ENGINEERED COMPONENTS TO BE INSTALLED TO MANUFACTURES SPECS.

PLUMBING

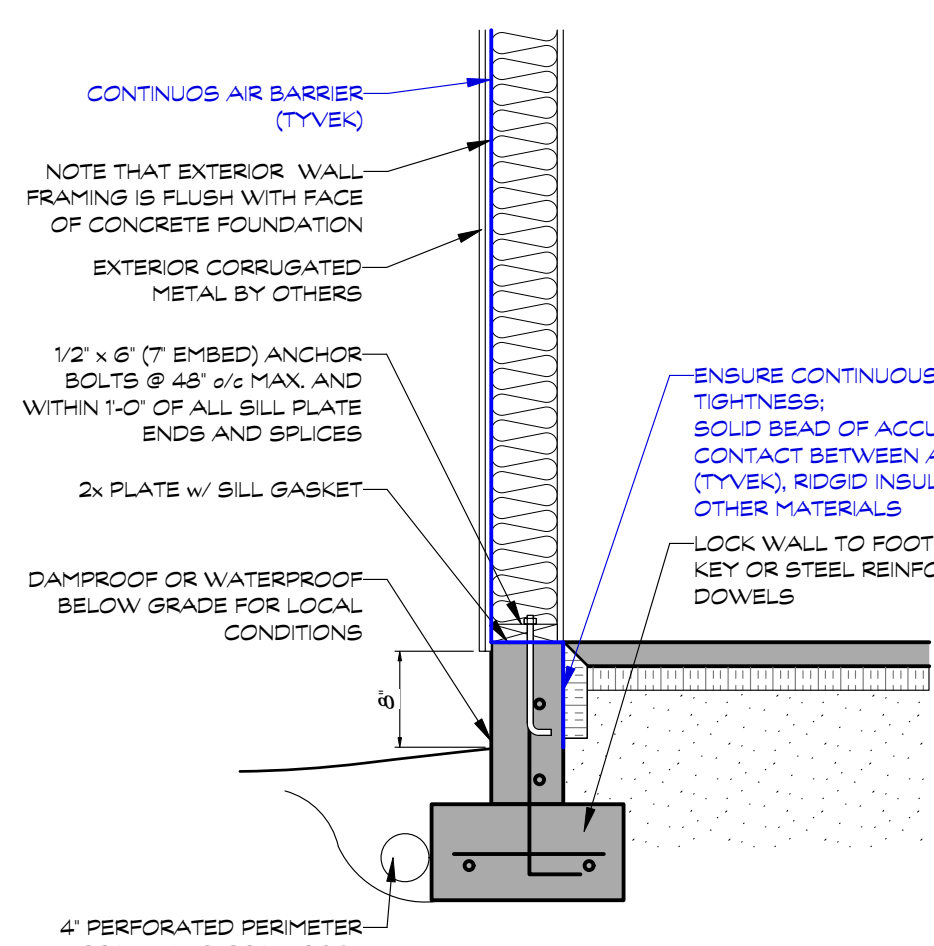
- PLUMBING FIXTURES AND FITTINGS SHALL MEET THE STANDARDS NOTED BELOW
- A) WATER CLOSET = 1.6 GAL PER FLUSH
- B) SHOWER HEAD = 2.5 GPM MAX
- C) LAVATORY FAUCETS = 2.2 GPM MAX
- D) SINK FAUCETS = 2.2 GPM MAX TITLE 24, VCBC, UPC

EGRESS WINDOWS (BEDROOMS)

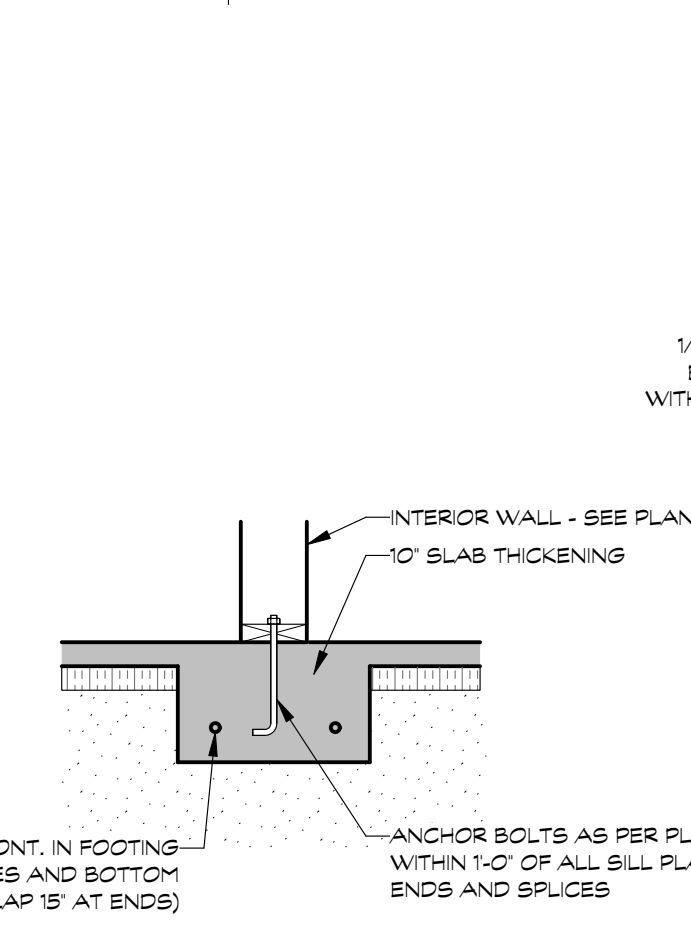
- FINISHED SILL 59" ABOVE FLOOR MAX.
- NET OPENING TO BE 3.77 SQUARE FEET MINIMUM, WITH NO DIMENSION LESS THAN 15"

MECHANICAL AND VENTILATION

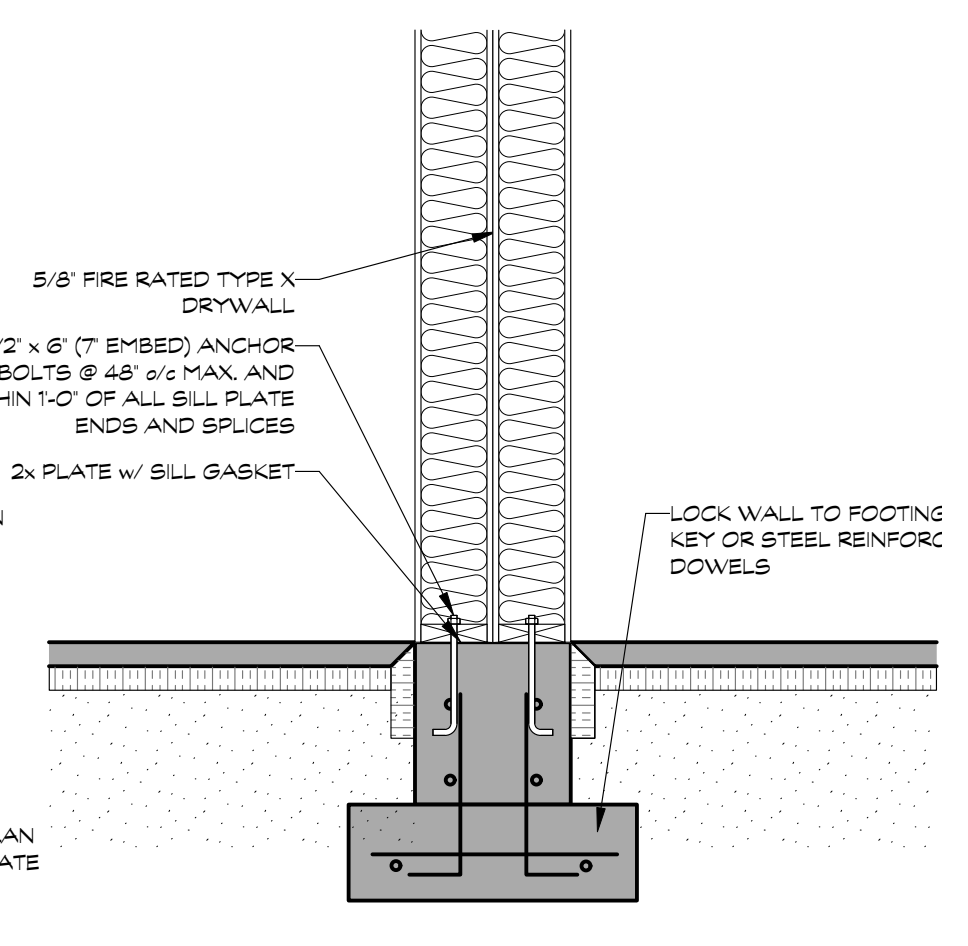
- BUILDING MUST BE PROVIDED WITH MECHANICAL VENTILATION DESIGN BY OTHERS
- ATTIC TO BE VENTED MINIMUM 1:300 OF AREA
- ONE HALF OF ROOF VENTS TO BE LOCATED IN UPPER PORTION OF ATTIC SPACE, AT LEAST 3'-0" HIGHER THAN EAVE OR CORNICE VENTS
- 22' x 30' MIN. ATTIC ACCESS. INSULATE AND WEATHER STRIP DOOR. 30' MIN. HEAD CLEARANCE



CROSS SECTION D2
SCALE: 3/4" = 1'-0"



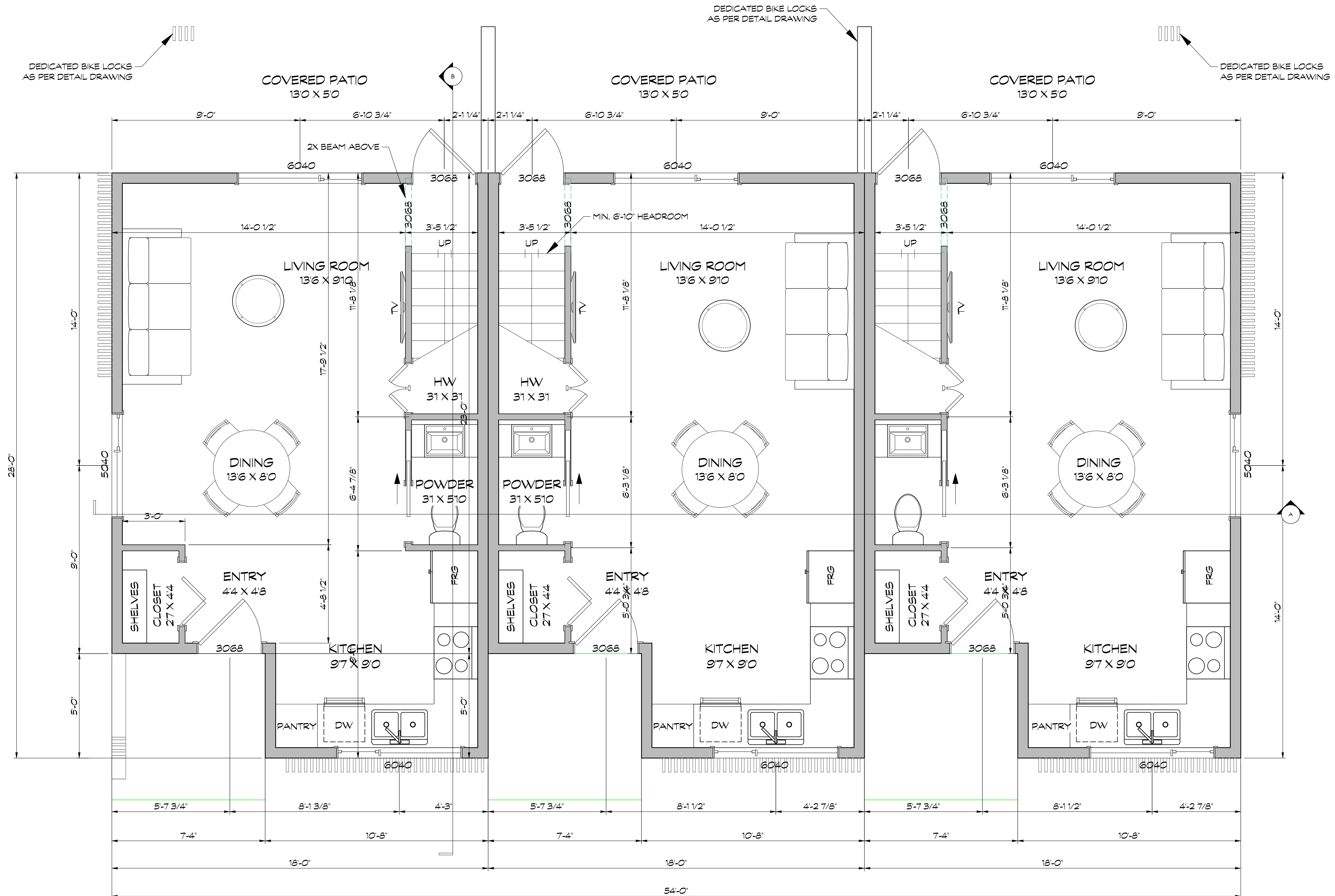
CROSS SECTION D3
SCALE: 3/4" = 1'-0"



CROSS SECTION D1
SCALE: 3/4" = 1'-0"

VERSION FOR RE-ZONING PURPOSES

1.FEB01/21 Concept Design for Review	
ISSUE NOTES	
PERSPECTIVE	Sheet No. 2
Drawn by: MM	
PERSPECTIVE DESIGN BUILD LTD.	
Project Location: 1590 Piercy Rd., Courtenay, BC	
Project Title: NINE PEAKS	
Sheet Title: FOUNDATION PLAN	



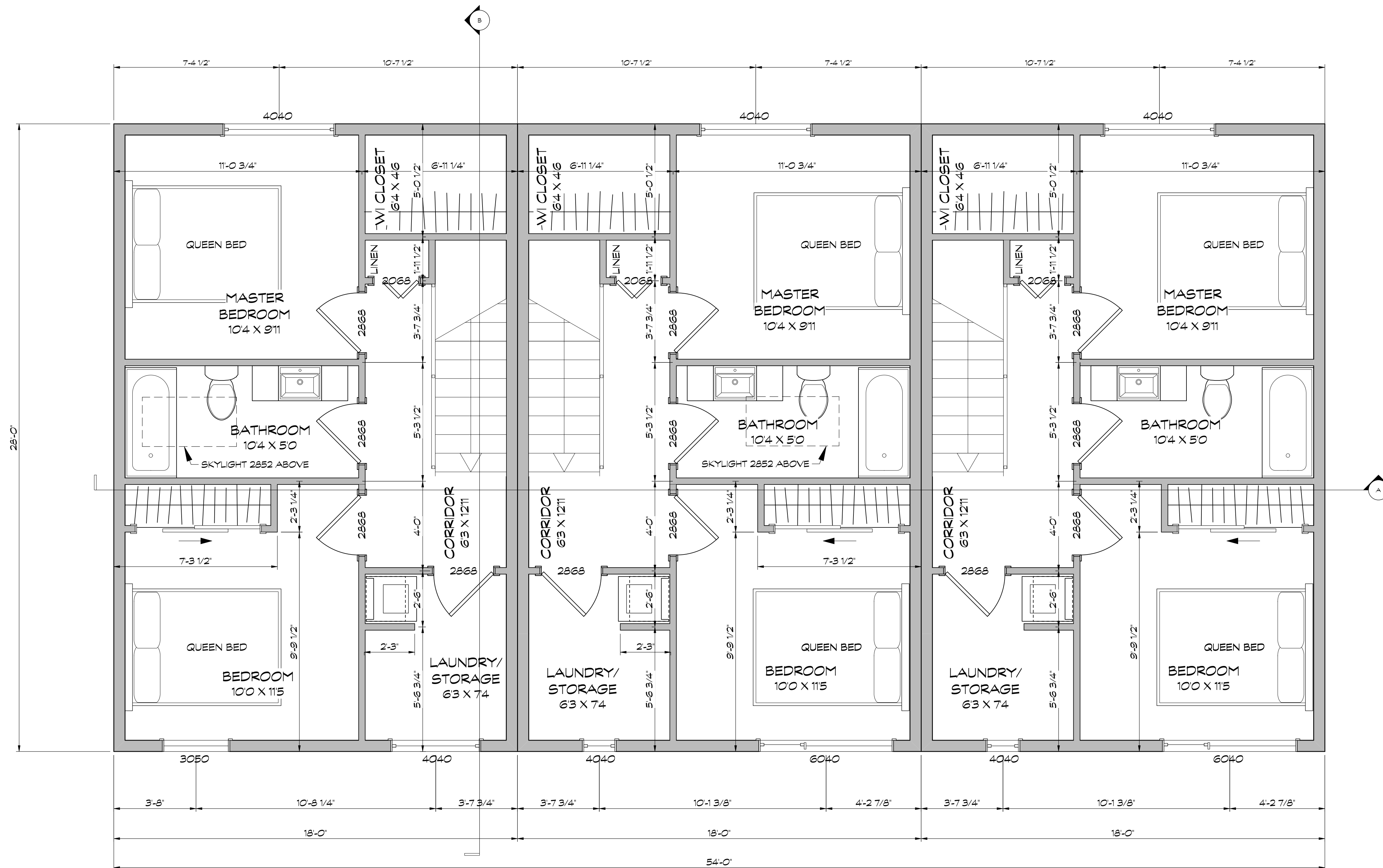
MAIN FLOOR PLAN
SCALE: 3/8" = 1'-0"

ALL 3 DWELLINGS IDENTICAL DIMENSIONS
467 SQ FT (EXTERIOR FLOOR AREA PER DWELLING)
51 SQ FT (ENTRY SLAB PER DWELLING)

1402 SQ FT (EXTERIOR FLOOR AREA IN TOTAL PER BUILDING)
153 SQ FT (ENTRY SLABS IN TOTAL PER BUILDING)

VERSION FOR RE-ZONING PURPOSES

1.FE801/21 Concept Design for Review	
Issue Notes	
PERSPECTIVE	Sheet No. 3
Project Location 1598 Piercy Rd., Courtenay, BC	
Project Title NINE PEAKS	
Sheet Title FLOOR PLANS	



UPPER FLOOR PLAN

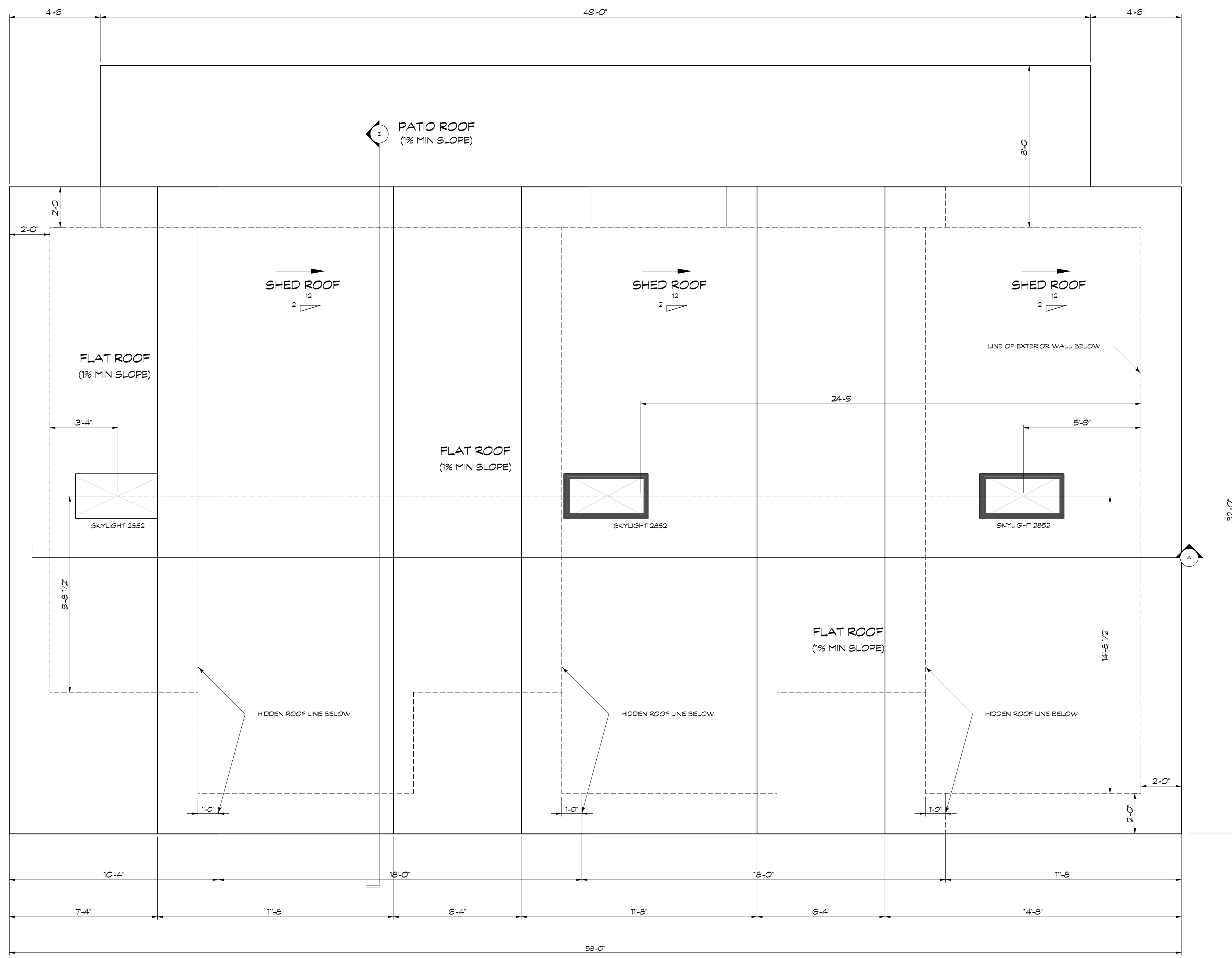
SCALE: 3/8" = 1'-0"

472 SQ FT (EXTERIOR FLOOR AREA IN PER DWELLING)

1445 SQ FT (EXTERIOR FLOOR AREA IN TOTAL PER BUILDING)

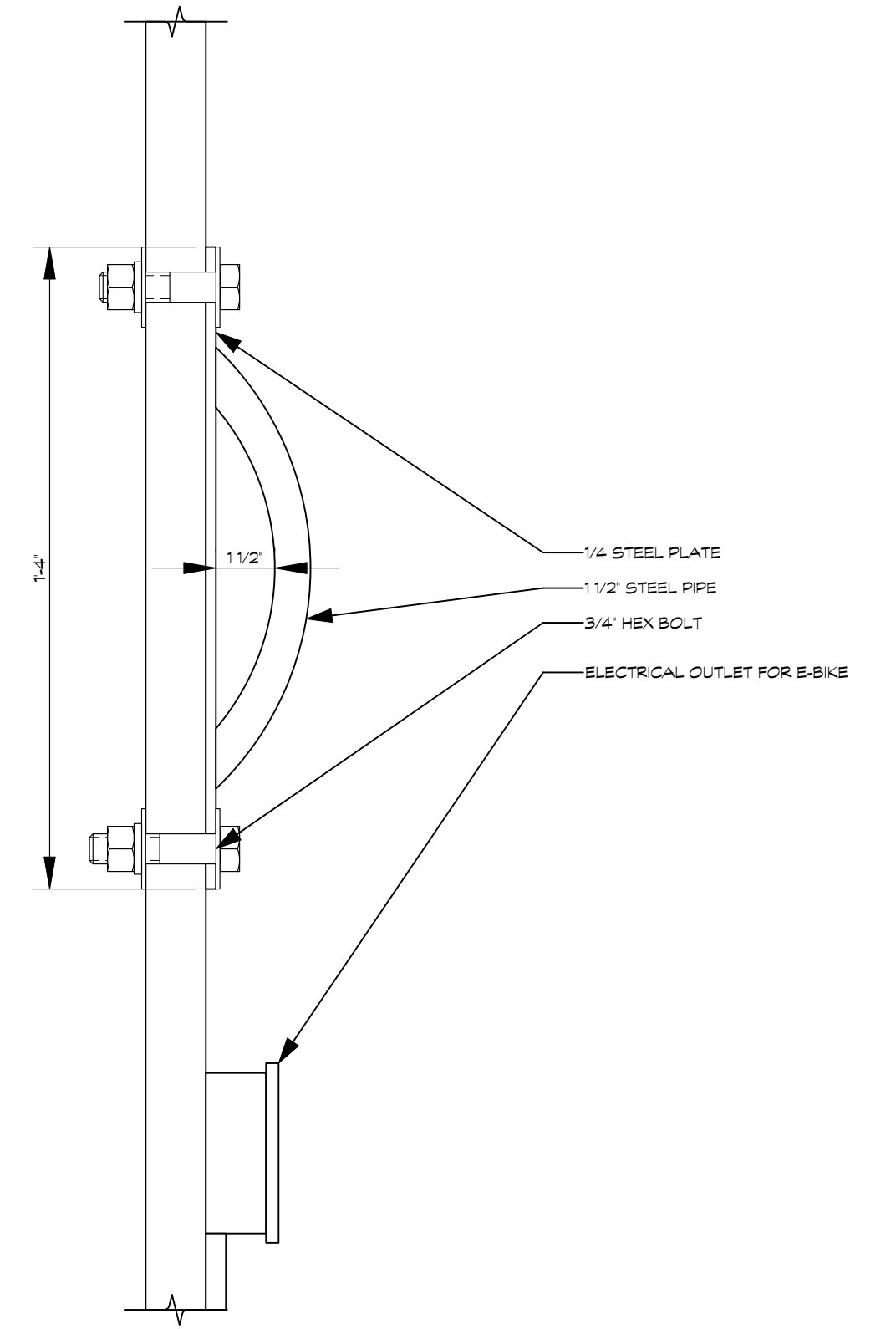
VERSION FOR RE-ZONING PURPOSES

1.FE801/21 Concept Design for Review	
Issue Notes	
PERSPECTIVE	Sheet No. 4
Project Location 1598 Piercy Rd., Courtenay, BC	
Project Title NINE PEAKS	
Sheet Title 2ND FLOOR PLANS	

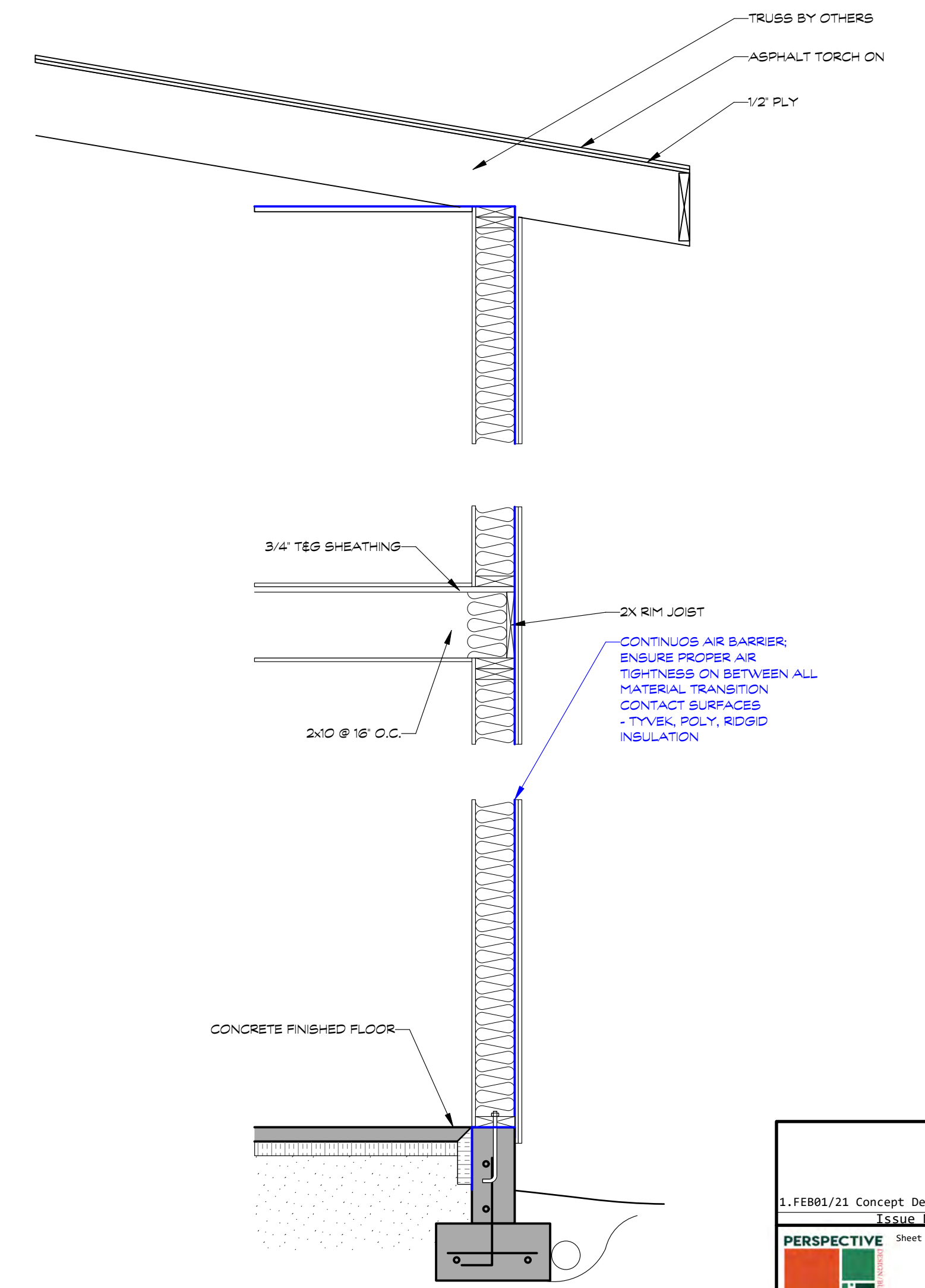


ROOF PLAN
 SCALE: 3/8" = 1'-0"

(1%) COVERED PATIO FLAT ROOF AREA = ~392 SQ FT
 (1%) FLAT ROOF AREA = ~717 SQ FT
 (9.5%) SHED ROOF AREA = ~1302 SQ FT



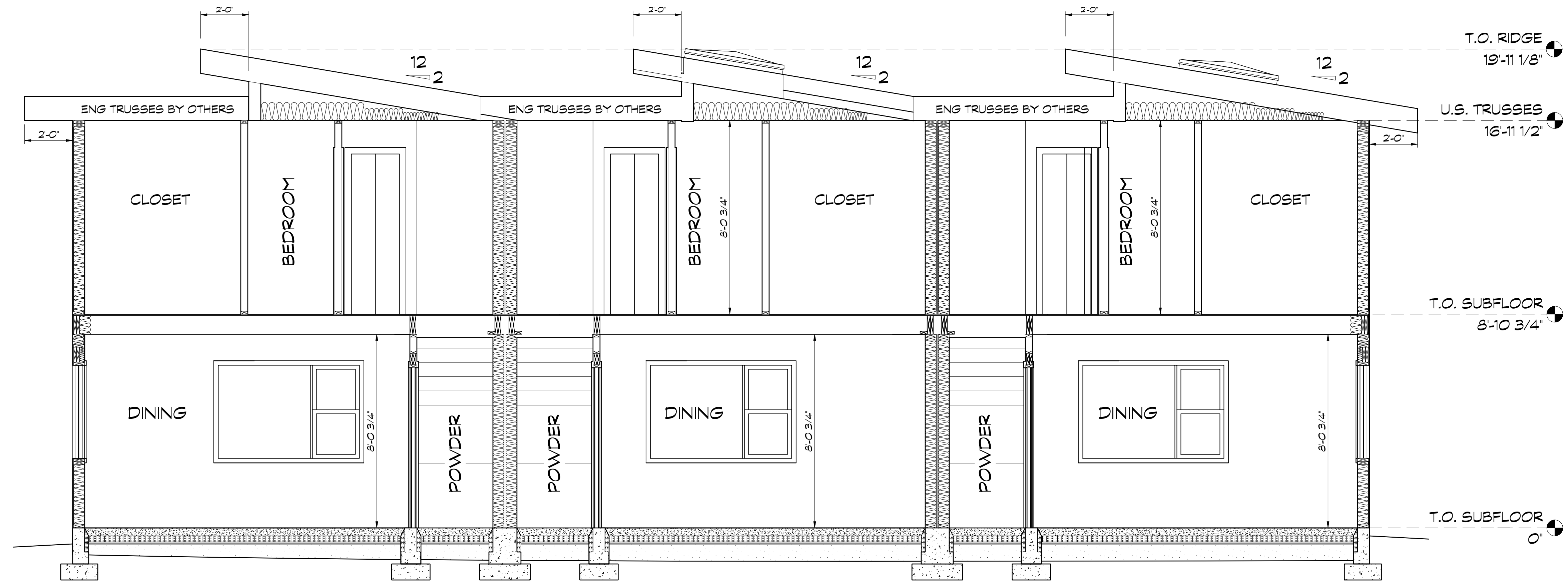
DEDICATED BIKE LOCK DETAIL DRAWING
 SCALE: 3" = 1'-0"



TYPICAL WALL SECTION
 SCALE 3/4" = 1'-0"

1.FE801/21 Concept Design for Review	
ISSUE NOTES	
PERSPECTIVE	Sheet No. 5
Drawn by MM	
PERSPECTIVE DESIGN BUILD LTD.	
Project Location: 1598 Piercy Rd., Courtenay, BC	
Project Title:	NINE PEAKS
Sheet Title:	ROOF PLAN

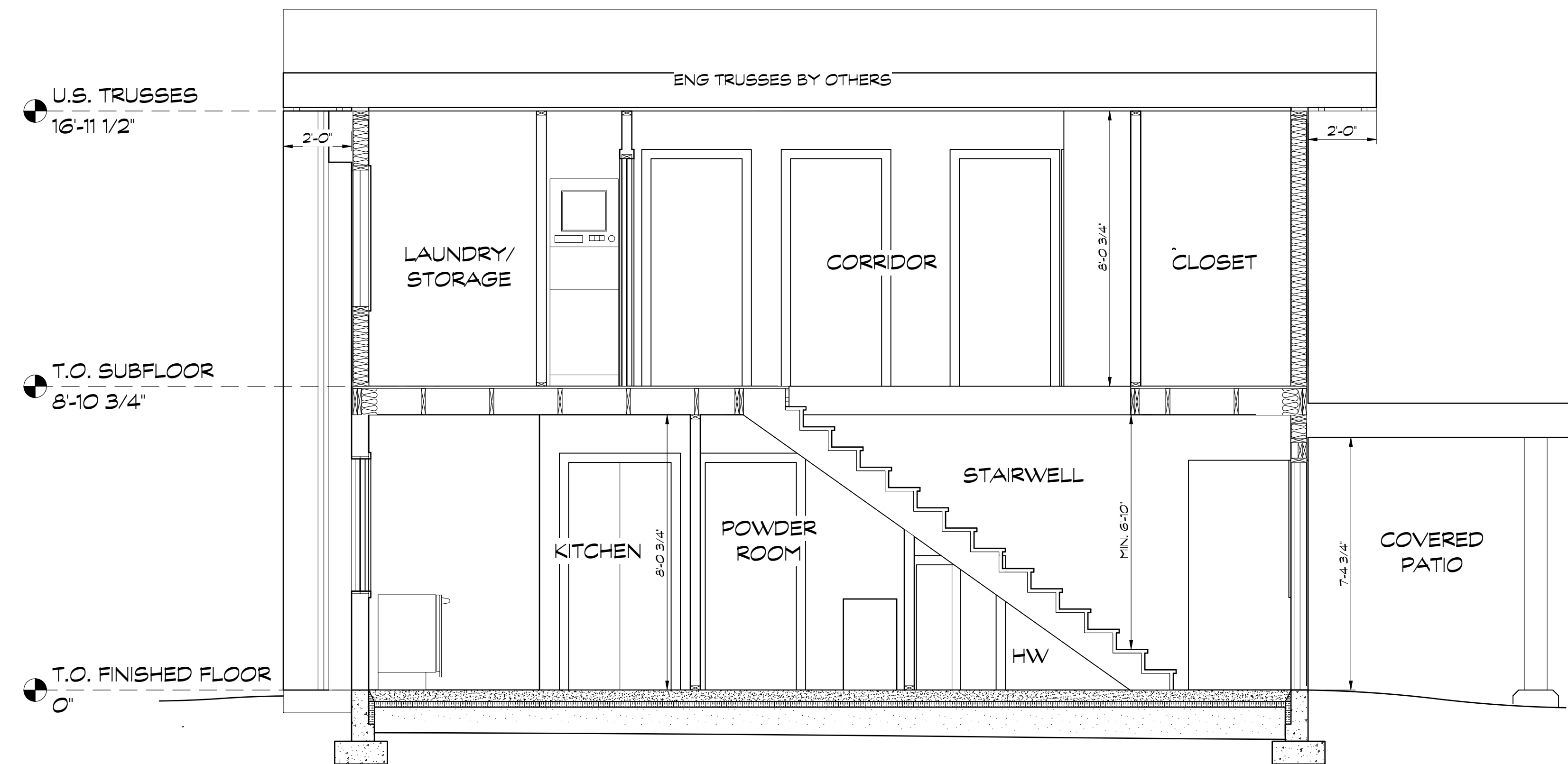
VERSION FOR RE-ZONING PURPOSES



SECTION A
SCALE: 3/8" = 1'-0"

SEALED POLYETHYLENE AIR BARRIER - CODE REFERENCES
9.36.2.9. AIR TIGHTNESS
9.36.2.10. CONSTRUCTION OF AIR BARRIER DETAILS

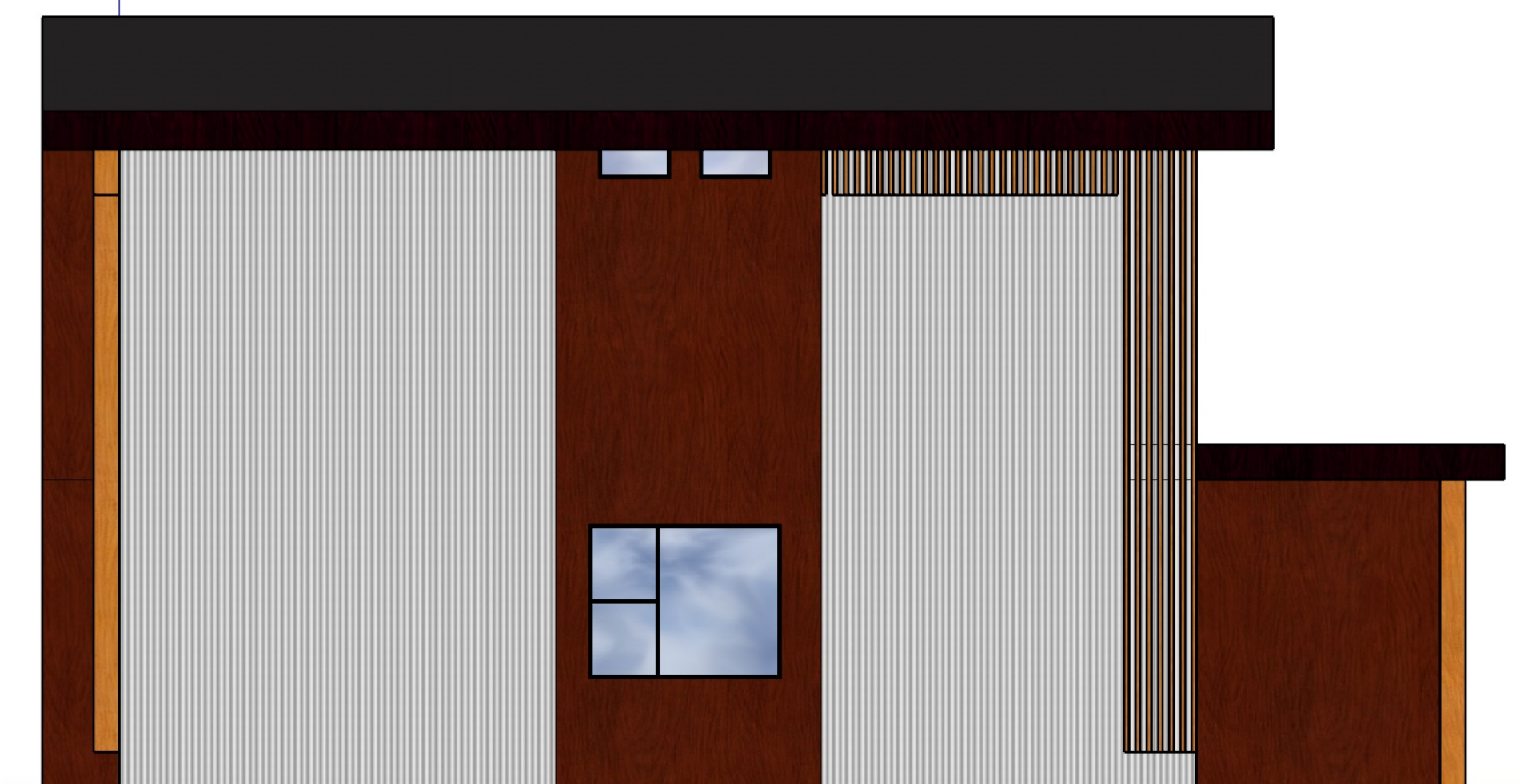
- ENSURE AIR BARRIER IS CONTINUOUS AND AIR TIGHT AT:
- PARTITION WALLS AT EXTERIOR WALLS AND CEILING
 - RECESSED LIGHTS
 - DRYER AND RANGE HOOD EXHAUSTS
 - EXTERIOR DOOR AND WINDOW PENETRATIONS
 - BATHROOM CEILING FANS
 - PLUMBING PENETRATIONS
 - MECHANICAL PENETRATIONS
 - INTERIOR AND EXTERIOR ELECTRICAL PENETRATIONS
 - RIM JOISTS
 - FOUNDATION WALL TRANSITION



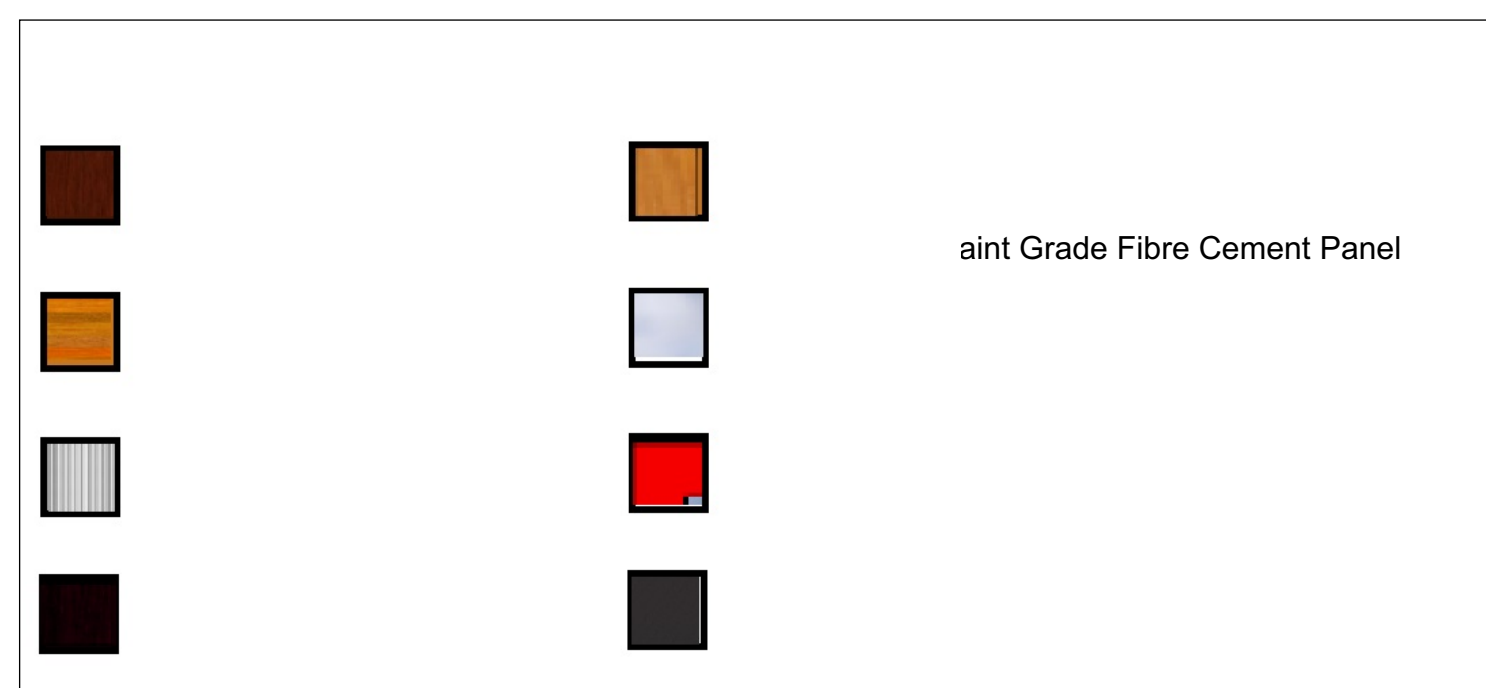
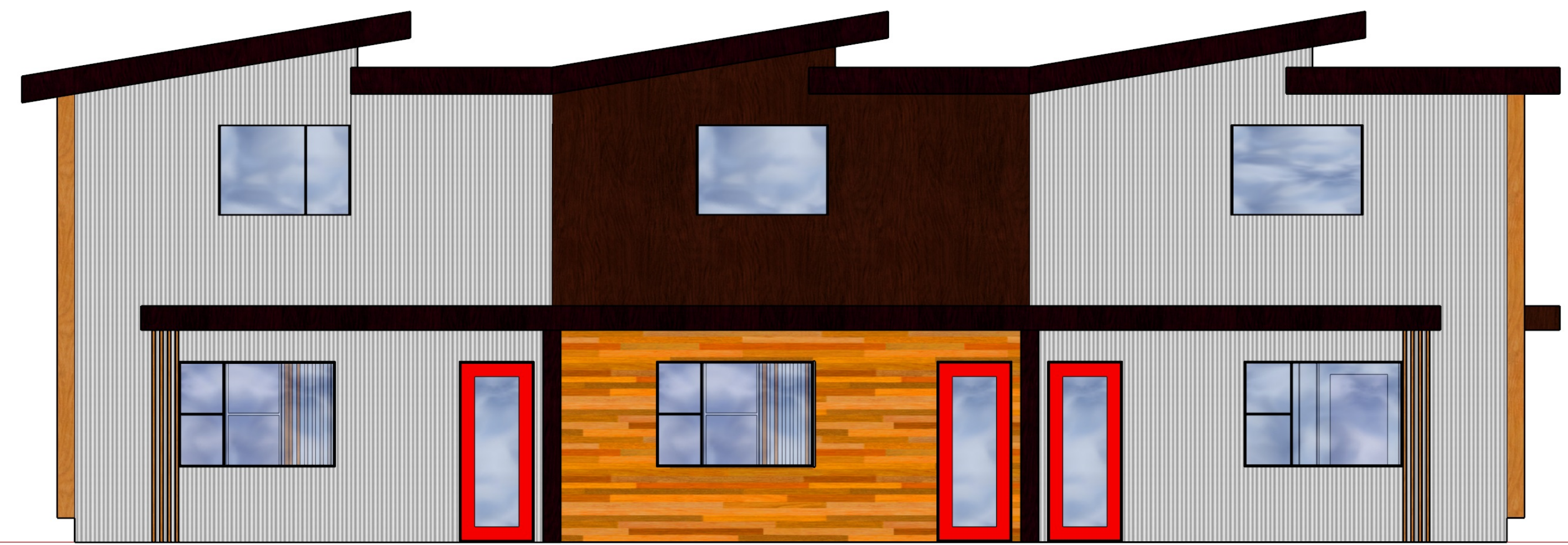
SECTION B
SCALE: 3/8" = 1'-0"

VERSION FOR RE-ZONING PURPOSES

1.FEB01/21 Concept Design for Review	
Issue Notes	
PERSPECTIVE	Sheet No. 6
Project Location 1590 Piercy Rd., Courtenay, BC	
Project Title NINE PEAKS	
Sheet Title SECTIONS	



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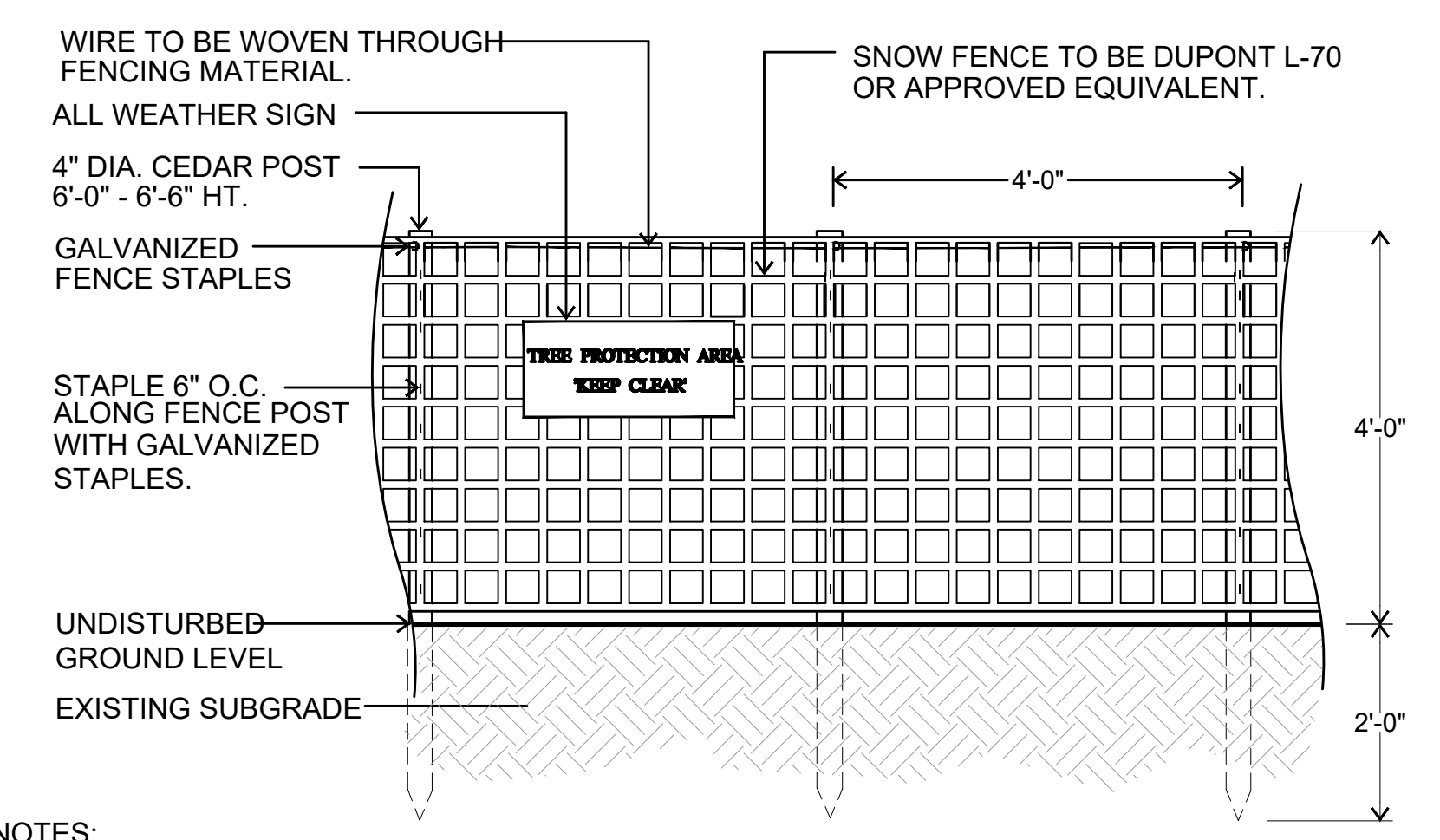
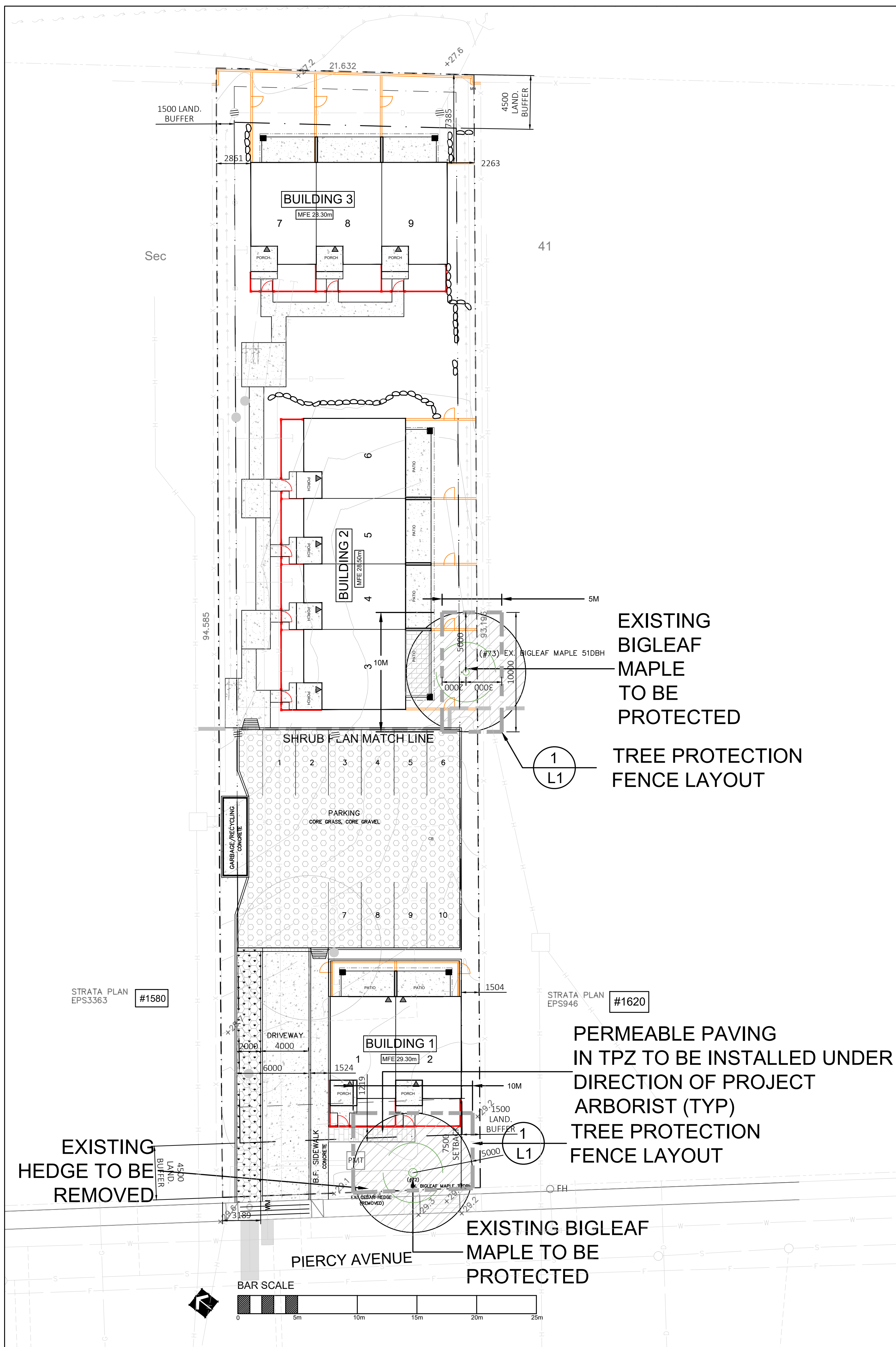


Torch on Roofing Membrane

VERSION FOR RE-ZONING PURPOSES

1.FE001/21 Concept Design for Review	
ISSUE NOTES	
PERSPECTIVE	Sheet No. 7
Project Location 1590 Piercy Rd., Courtenay, BC	
Project Title NINE PEAKS	
Sheet Title ELEVATIONS	

SEAL:



- NOTES:**
1. TREE RETENTION FENCES TO BE IN PLACE PRIOR TO ANY WORK ON SITE.
 2. LANDSCAPE ARCHITECT TO VERIFY PLACEMENT.
 3. FENCE TO ENCLOSE ALL TREE RETENTION ZONES.

1 TREE RETENTION FENCE
SCALE 1:20

REFER TO ARBORIST REPORT FOR FULL TREE PROTECTION REQUIREMENTS:
GROW TREE CARE, 2640 COMOX ROAD, COURTENAY BC (250-702-3453)
WWW.GROWTREECARE.COM

Tree Retention Plan

Tree retention management works seek to protect rooting soils from compaction and contamination, and the tree's stems and branches from mechanical damage, by establishing Root Protection Areas (RPAs) about the subject trees. Additionally, efforts should be made to improve soil conditions within the RPAs to compensate for root loss outside of the RPAs.

- A sturdy wooden post-and-top-rail fence should be installed along the edges of the RPAs before any development works occur.
- The project arborist should inspect the fence location and construction after installation.
- The project arborist should be present when initial excavation works occur adjacent to Tree # 73 to inspect root distribution and to perform any root pruning works at the edge of the RPA.
- An eight (8) inch layer of arborist mulch should be applied to the entire surface of the RPAs to help moderate soil moisture conditions and to improve soil biology.
- Building materials and other should not be stored within the RPAs.
- Periodic deep watering should occur within the RPAs during the hot summer months.
- The project arborist should be contacted first if changes need to be made to the RPAs.

NO.	DATE	REVISION DESCRIPTION	DR.
6	21.JUL.28	REAR BUILDING RELOCATED	BJ
5	22.07.13	TREES ADDED, SETBACK REV., BLDG SHIFTED	BJ
4	21.09.23	LANDSCAPE BUFFER TABLE ADDED	BJ
3	21.APR.21	LANDSCAPE BUFFER DIMS ADDED	BJ
2	21.MAR.01	DP SUBMISSION	BJ
1	21.FEB.16	COORDINATION - IN PROGRESS	BJ

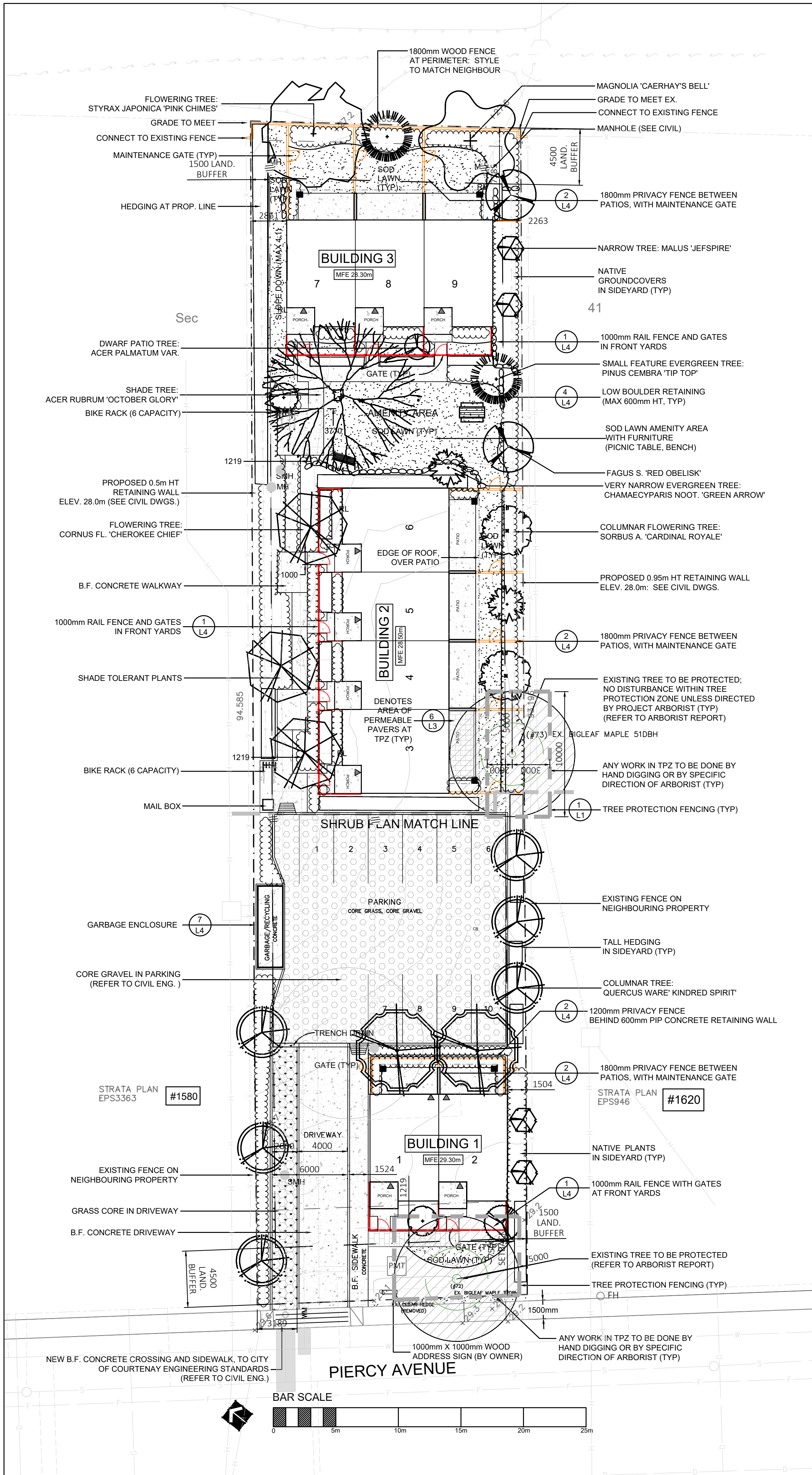
CLIENT:

PROJECT:
NINE PEAKS
1590 PIERCY AVENUE
COURTENAY, BC

DRAWING TITLE:
TREE MANAGEMENT PLAN

DATE: 20.09.20 DRAWING NUMBER:
SCALE: 1:200 **L1**
DRAWN: BJ
DESIGN: PCM
CHK'D: PCM OF 5

PMG PROJECT NUMBER: 20-114



PLANT SCHEDULE				PMG PROJECT NUMBER: 20-114
KEY	QTY	BOTANICAL NAME	COMMON NAME	PLANTED SIZE / REMARKS
TREE	2	ACER GRISEUM	PAPERBARK MAPLE	6CM CAL; 1.8M STD; B&B
	2	ACER PALMATUM 'RED DRAGON'	RED DRAGON JAPANESE MAPLE	1.0M HT; TREE FORM
	1	ACER PALMATUM 'SHIN DESHOJO'	SHIN DESHOJO JAPANESE MAPLE	1.0M HT; TREE FORM
	1	ACER RUBRUM 'OCTOBER GLORY'	OCTOBER GLORY MAPLE	6CM CAL; 2M STD; B&B
	2	CERCIS CANADENSIS 'THE RISING SUN'	THE RISING SUN REDBUD	4CM CAL; MIN. 1.5M STD, B&B
	2	CHAMAECYPARIS 'NOOTKATENSIS' 'GREEN ARROW'	GREEN ARROW FALSECYPRESS	2.0M HT, B&B
	3	CORNUS FLORIDA 'CHEROKEE CHIEF'	RED FLOWERING DOGWOOD	6CM CAL; 1.8M STD; B&B
	2	EXISTING TREE TO BE PROTECTED	SEE ARBORIST PLAN	VARIES
	2	FAGUS SYLVATICA 'RED OBELISK'	RED OBELISK COLUMNAR BEECH	6CM CAL; 1.8m STD, B&B
	1	MAGNOLIA x 'CAERHAY'S BELL'	CAERHAY'S BELL MAGNOLIA	6CM CAL; 1.8M STD, B&B
	4	MALUS 'JEFSPIRE'	PURPLE SPIRE CRABAPPLE	5CM CAL; 1.8M STD; B&B
	2	PINUS CEMBRA 'TIP TOP'	TIP TOP SWISS STONE PINE	2.0M HT; B&B
	6	QUERCUS x WAREI 'KINDRED SPIRIT' ('NADLER')	KINDRED SPIRIT COLUMNAR OAK	6CM CAL; 1.8M STD, B&B
	2	SORBUS AUCUPARIA 'CARDINAL ROYALE'	CARDINAL ROYAL MT. ASH	6CM CAL; 1.8M STD, B&B
	1	STYRAX JAPONICUS 'PINK CHIMES'	PINK CHIMES JAPANESE SNOWBELL	6CM CAL; 1.8M STD; B&B
SHRUB	A2	AZALEA 'WHITE LIGHTS'	DECIDUOUS AZALEA; PINK	#3 POT; 50CM
	A4	AZALEA JAPONICA 'HARDIZER'S BEAUTY'	AZALEA; LIGHT PINK	#2 POT; 25CM
	BF	BUXUS MICROPHYLLA 'FAULKNER'	FAULKNER BOXWOOD	#2 POT
	GTB	BUXUS SEMPERVIRENS 'MONRIE'	GREEN TOWER BOXWOOD	1.0M HT, B&B
	C	CAMELLIA JAPONICA 'DEBUTANTE'	CAMELLIA; LIGHT PINK	#5 POT
	PAB	CAMELLIA SASANQUA 'MONDEL'	PINK-A-BOO CAMELLIA	#5 POT
	EU	EUONYMUS ALATA 'COMPACTUS'	COMPACT WINGED BURNING BUSH	#3 POT; 50CM
	AP	HAMAMELIS X INTER 'ARNOLD'S PROMISE'	ARNOLD'S PROMISE WITCH HAZEL	#10 POT; TREE FORM
	HYB	HYDRANGEA 'BLUSHING BRIDE'	BLUSHING BRIDE HYDRANGEA	#3 POT
	H	HYDRANGEA MACRO. 'ENDLESS SUMMER'	ENDLESS SUMMER BIGLEAF HYDRANGEA	#3 POT; 80CM
	KE	KALMIA LATIFOLIA 'ELF'	ELF MOUNTAIN LAUREL	#2 POT; 30CM
	KA	KALMIA LATIFOLIA 'OLYMPIC FIRE'	OLYMPIC FIRE MOUNTAIN LAUREL	#3 POT; 50CM
	KT	KALMIA LATIFOLIA 'TIDDLYWINKS'	TIDDLYWINKS MOUNTAIN LAUREL	#2 POT; 30CM
	M	MAHONIA AQUIFOLIUM 'COMPACTA'	COMPACT OREGON GRAPE HOLLY	#3 POT; 40CM
	N	NANDINA DOMESTICA 'MONUM'	PLUM PASSION HEAVENLY BAMBOO	#3 POT
	NI	PHYSOCARPUS OPUULIFOLIUS 'LITTLE DEVIL'	LITTLE DEVIL NINEBARK	#3 POT
	P2	PIERIS JAPONICA 'COMPACTA'	DWARF JAPANESE PIERIS	#2 POT; 35CM
	R	RHODODENDRON 'BOW BELLS'	BOW BELLS RHODODENDRON	#2 POT
	RO2	ROSA 'FUSCHIA MEIDLAND'	FUSCHIA MEIDLAND ROSE	#2 POT
	SA	SARCOCOCCA 'HOKOKANA VAR. 'HUMILIS'	HIMALAYAN SWEET BOX	#2 POT; 25CM
	SK	SKIMMIA JAPONICA (10% MALE)	JAPANESE SKIMMIA	#2 POT; 30CM
	YD	TAXUS X MEDIA 'DENSIFORMIS'	DWARF ENGLISH YEW	#3 POT
	Y	TAXUS X MEDIA 'HICKSII'	HICKS YEW	#3 POT; 80CM
	TH	THUJA OCCIDENTALIS 'SMARAGD'	EMERALD GREEN CEDAR	1.5M HT; B&B
	V1	VIBURNUM P.T. 'SUMMER SNOWFLAKE'	SUMMER SNOWFLAKE DOUBLEFILE VIBURNUM	#3 POT; TREE FORM
GRASS	KF	CALAMAGROSTIS 'KARL FOERSTER'	KARL FOERSTER FEATHERREED GRASS	#2 POT
	JA	HAKONECHLOA MACRA 'ALL GOLD'	ALL GOLD JAPANESE FOREST GRASS	#1 POT
VINE	HY	HYDRANGEA ANOMALA V. 'PETIOLARIS'	CLIMBING HYDRANGEA	#2 POT; STAKED
PERENNIAL	WMF	ADIANTUM ALEUTICUM	WESTERN MAIDENHAIR FERN	#1 POT
	CA	CAREX BUCHANANII 'VIRIDIS'	GREEN LEATHERLEAF SEDGE	#1 POT
	CF	CIMICIFUGA R. 'HILLSIDE BLACK BEAUTY'	HILLSIDE BLACK BEAUTY BUGBANE	#2 POT
	HM	HEUCHERA 'MIDNIGHT ROSE'	MIDNIGHT ROSE HEUCHERA	15CM POT
	HE	HEUCHERA 'PARIS'	PARIS CORAL BELLS	15 CM POT
	HEP	HEUCHERA 'PINOT NOIR'	PINOT NOIR CORAL BELLS	15 CM POT
	HF	HOSTA 'FRANCEE'	FRANCEE HOSTA	#1 POT; 1 EYE
	HAL	HOSTA 'HALCYON'	HALCYON HOSTA	#1 POT
	HS	HOSTA 'SUM AND SUBSTANCE'	SUM AND SUBSTANCE HOSTA	#1 POT; 1 EYE
	OF	MATTEUCIA STRUTHIOPERIS	OSTRICH FERN	#1 POT
	F	POLYSTICHUM MUNITUM	WESTERN SWORD FERN	#1 POT
	PU	PULMONARIA 'MILKY WAY'	MILKY WAY LUNGWORT	#1 POT
	S	SALVIA X SUPERBA 'EAST FRIESLAND'	EAST FRIESLAND SAGE	#1 POT
GC	EF	EUONYMUS JAPONICA 'EMERALD 'N GOLD'	EUONYMUS; GOLD VARIEGATED	#1 POT; 25CM

NOTES: * PLANT SIZES IN THIS LIST ARE SPECIFIED ACCORDING TO THE BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD, LATEST EDITION. CONTAINER SIZES SPECIFIED AS PER CNLA STANDARD. BOTH PLANT SIZE AND CONTAINER SIZE ARE THE MINIMUM ACCEPTABLE SIZES. * REFER TO SPECIFICATIONS FOR DEFINED CONTAINER MEASUREMENTS AND OTHER PLANT MATERIAL REQUIREMENTS. * SEARCH AND REVIEW: MAKE PLANT MATERIAL AVAILABLE FOR OPTIONAL REVIEW BY LANDSCAPE ARCHITECT AT SOURCE OF SUPPLY. AREA OF SEARCH TO INCLUDE LOWER MAINLAND AND FRASER VALLEY. * SUBSTITUTIONS: OBTAIN WRITTEN APPROVAL FROM THE LANDSCAPE ARCHITECT PRIOR TO MAKING ANY SUBSTITUTIONS TO THE SPECIFIED MATERIAL. UNAPPROVED SUBSTITUTIONS WILL BE REJECTED. ALLOW A MINIMUM OF FIVE DAYS PRIOR TO DELIVERY FOR REQUEST TO SUBSTITUTE. SUBSTITUTIONS ARE SUBJECT TO BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD. DEFINITION OF CONDITIONS OF AVAILABILITY. ALL LANDSCAPE MATERIAL AND WORKMANSHIP MUST MEET OR EXCEED BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD LATEST EDITION. ALL PLANT MATERIAL MUST BE PROVIDED FROM CERTIFIED DISEASE FREE NURSERY

SITE FURNITURE



BENCH, 1.8M LENGTH
MAGLIN 'ICONIC' COLLECTION
THERMALLY MODIFIED ASH
POWDERCOAT COLOUR TBD
BY OWNER
SURFACE MOUNT TO CONCRETE
FOOTINGS



PICNIC TABLE WITH TWO BENCHES
MAGLIN 'ICONIC' COLLECTION
THERMALLY MODIFIED ASH
POWDERCOAT COLOUR TBD
BY OWNER
SURFACE MOUNT TO CONCRETE
FOOTINGS
(OR SIMILAR)



BIKE RACK
CORA 'EXPO 1500' RACK
STAINLESS STEEL FINISH
SURFACE MOUNT TO CONCRETE
(OR SIMILAR)

GENERAL IRRIGATION NOTES:

- LANDSCAPE CONTRACTOR TO PROVIDE SEPARATE CONTRACT TO LANDSCAPE INSTALLATION.
- ALL SOFT LANDSCAPE TO BE IRRIGATED WITH LOW FLOW IRRIGATION TO PLANTER BEDS, AND IN-GROUND SPRINKLER IRRIGATION TO LAWN AREAS.
- IRRIGATION CONTRACTOR TO PROVIDE AN EFFICIENT DESIGN-BUILD AUTOMATIC SYSTEM TO IABC STANDARDS, WITH MOISTURE SENSORS, AUTOMATIC RAIN GAUGE.
- SYSTEM ACCESSED WITHIN BUILDING, WITH A MAIN CONTROLLER.
- REFER TO MECHANICAL AND ELECTRICAL CONSULTANT DRAWINGS FOR COORDINATION & FOR IRRIGATION STUB-OUT LOCATIONS.
- IRRIGATION PLAN TO MEET OR EXCEED ALL RELEVANT REQUIREMENTS FOR MUNICIPAL APPROVAL.
- IRRIGATION DESIGN TO BE REVIEWED AND APPROVED BY LANDSCAPE ARCHITECT.

GENERAL PLANTING NOTES:

- LANDSCAPE CONTRACTOR TO PROVIDE 300mm MINIMUM GROWING MEDIUM DEPTH IN LAWN AREAS, AND 450mm MINIMUM GROWING MEDIUM DEPTH IN PLANTING BEDS.
- REFER TO SOFT LANDSCAPE SPECIFICATIONS.

Landscape Buffering (m)	Provided
Piercy Avenue (front)	4.5m (except for site access entry)
Side Yard (north)	Ranges from 1.2m to 3.4m
Side Yard (south)	Ranges from 1.5 to 1.8m
Rear	4.5m

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SEAL:

NO.	DATE	REVISION DESCRIPTION	DR.
6	21.JUL.28	REAR BUILDING RELOCATED	BJ
5	22.07.13	TREES ADDED, SETBACK REV., BLDG SHIFTED	BJ
4	21.09.23	LANDSCAPE BUFFER TABLE ADDED	BJ
3	21.APR.21	LANDSCAPE BUFFER DIMS ADDED	BJ
2	21.MAR.01	DP SUBMISSION	BJ
1	21.FEB.16	COORDINATION - IN PROGRESS	BJ

CLIENT:

PROJECT:

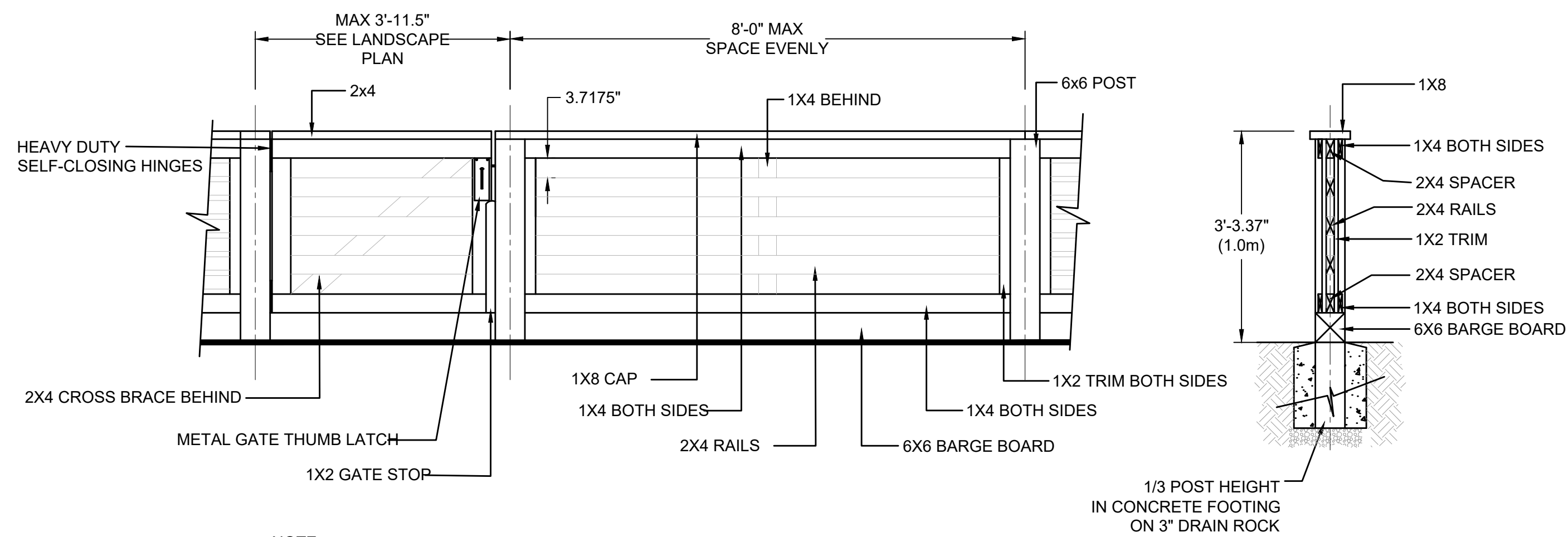
NINE PEAKS
1590 PIERCY AVENUE
COURTENAY, BC

DRAWING TITLE:

LANDSCAPE PLAN

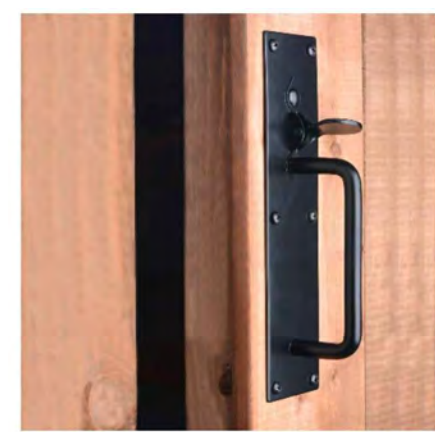
DATE: 20.09.20 DRAWING NUMBER:
SCALE: 1:200
DRAWN: BJ
DESIGN: BJ
CHK'D: PCM OF 5

PMG PROJECT NUMBER: 20-114



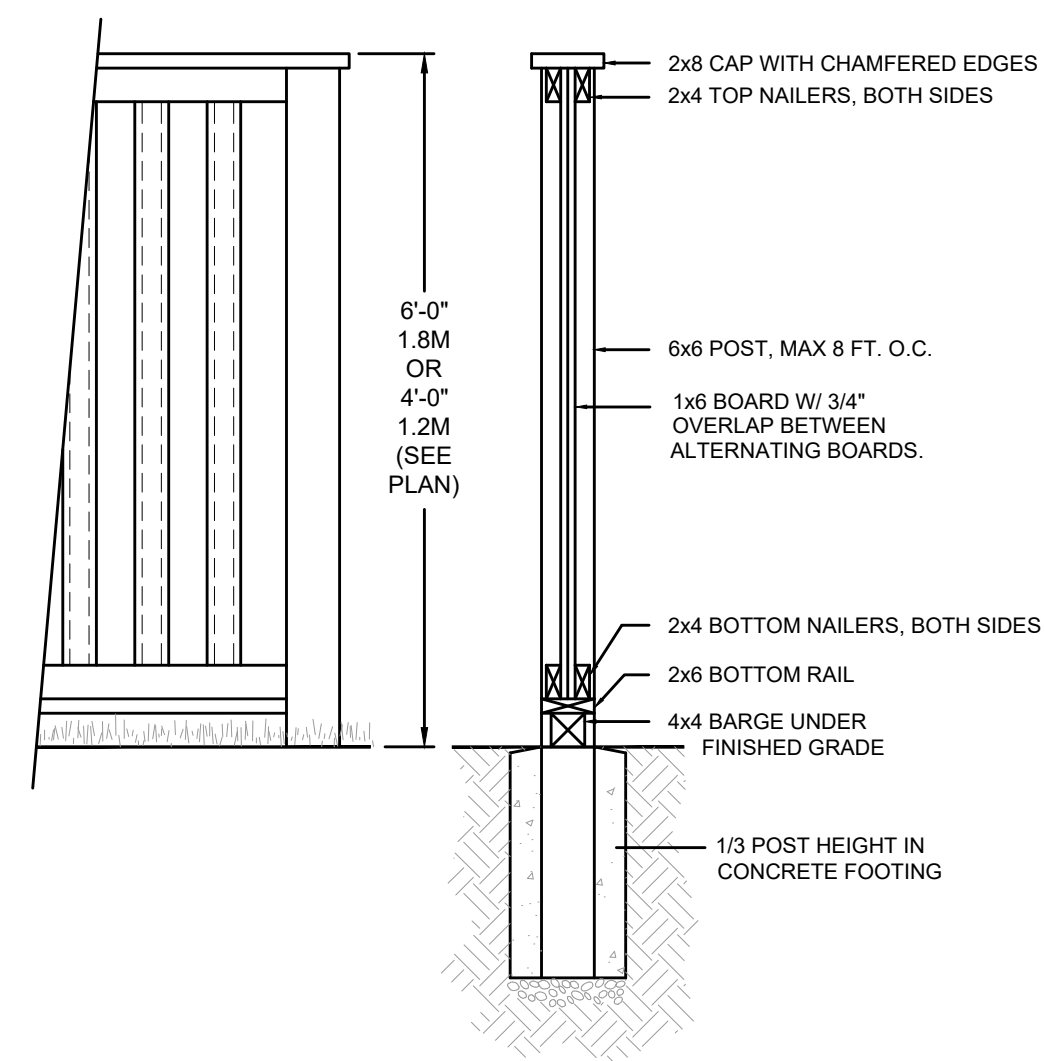
NOTE

1. ALL POSTS PRESSURE TREATED TO CSA STANDARD AND END CUTS TREATED WITH PRESERVATIVE.
2. ALL OTHER MEMBERS TO BE CEDAR #2 (CONSTRUCTION) GRADE MINIMUM.
3. ALL HARDWARE HOT DIPPED GALVANIZED.
4. APPLY 2 COATS EXTERIOR STAIN TO MANUFACTURERS SPECIFICATION. FINISH SELECTION AS APPROVED BY PROJECT ARCHITECT.
5. ALL FENCES TO BE LEVEL. CHANGES IN GRADE TO BE IN 12"-18" STEPS (MAX). GAPS TO GRADE TO FOLLOW FINISH GRADE. GAP TO BE 3-6".
6. LATCH AND HINGES TO BE HEAVY DUTY, P.C. BLACK, TO BE PREAPPROVED BY OWNER - PROVIDE SAMPLE.



EVERBUILT THUMB LATCH (OR SIMILAR)

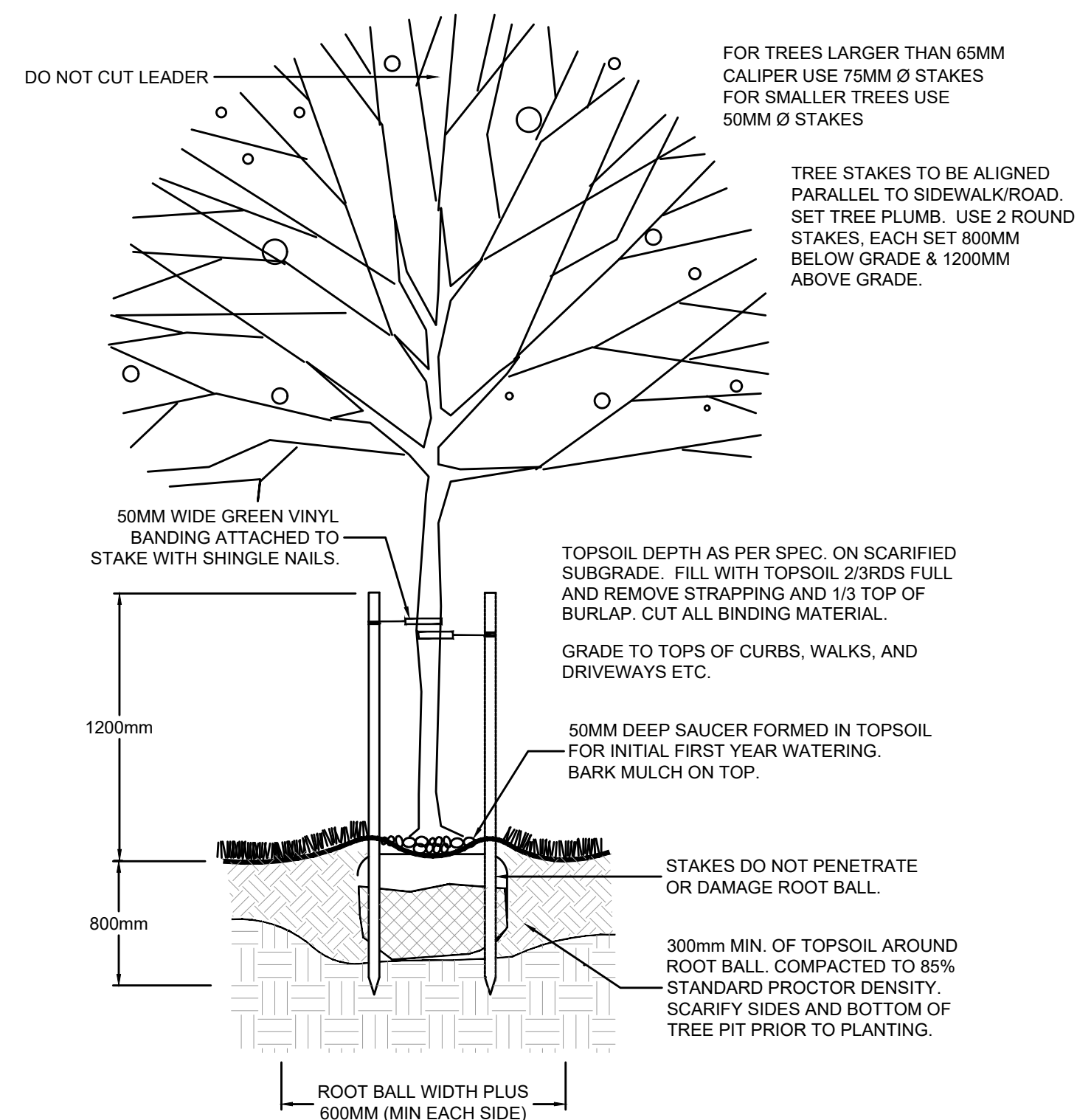
1 RAIL FENCE AND GATE
SCALE 1:20



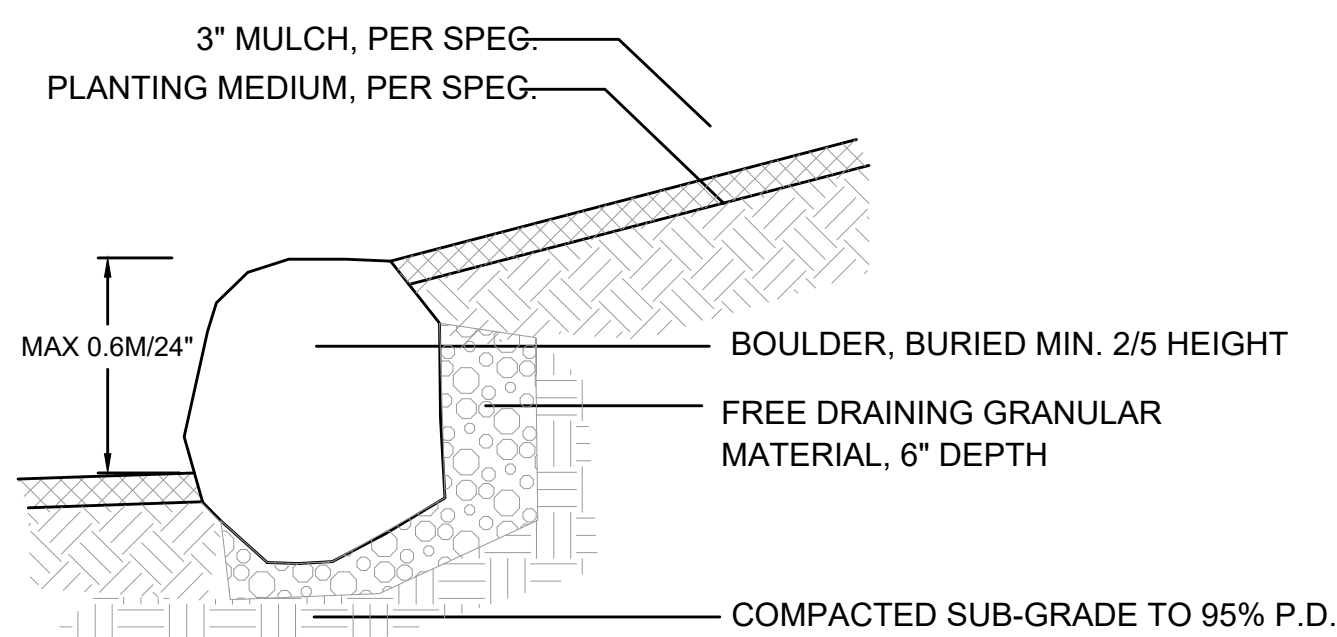
NOTE

1. ALL POSTS PRESSURE TREATED TO CSA STANDARD AND END CUTS TREATED WITH PRESERVATIVE.
2. ALL OTHER MEMBERS TO BE HEM/FIR #2 (CONSTRUCTION) GRADE MINIMUM.
3. ALL HARDWARE HOT DIPPED GALVANIZED.
4. APPLY 2 COATS EXTERIOR STAIN TO MANUFACTURERS SPECIFICATION. FINISH SELECTION AS APPROVED BY PROJECT ARCHITECT.
5. ALL FENCES TO BE LEVEL. CHANGES IN GRADE TO BE IN 12"-18" STEPS (MAX). GAPS TO GRADE TO FOLLOW FINISH GRADE. GAP TO BE 3-6".

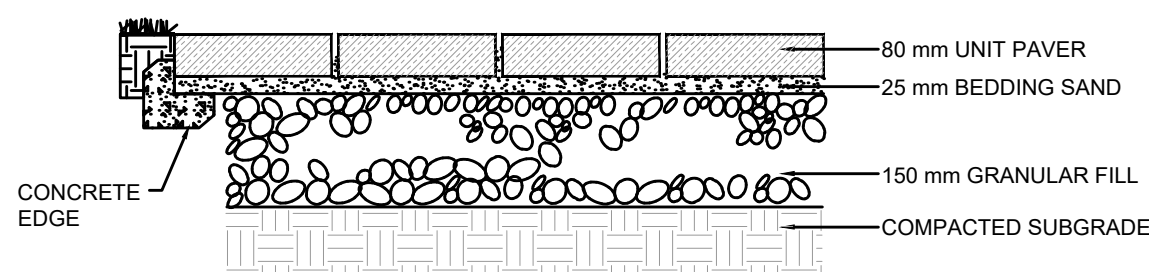
2 PRIVACY FENCE (1.8m/1.2m)
SCALE 1:20



3 TYPICAL TREE PLANTING
SCALE 1:25



4 BOULDER
SCALE 1:20

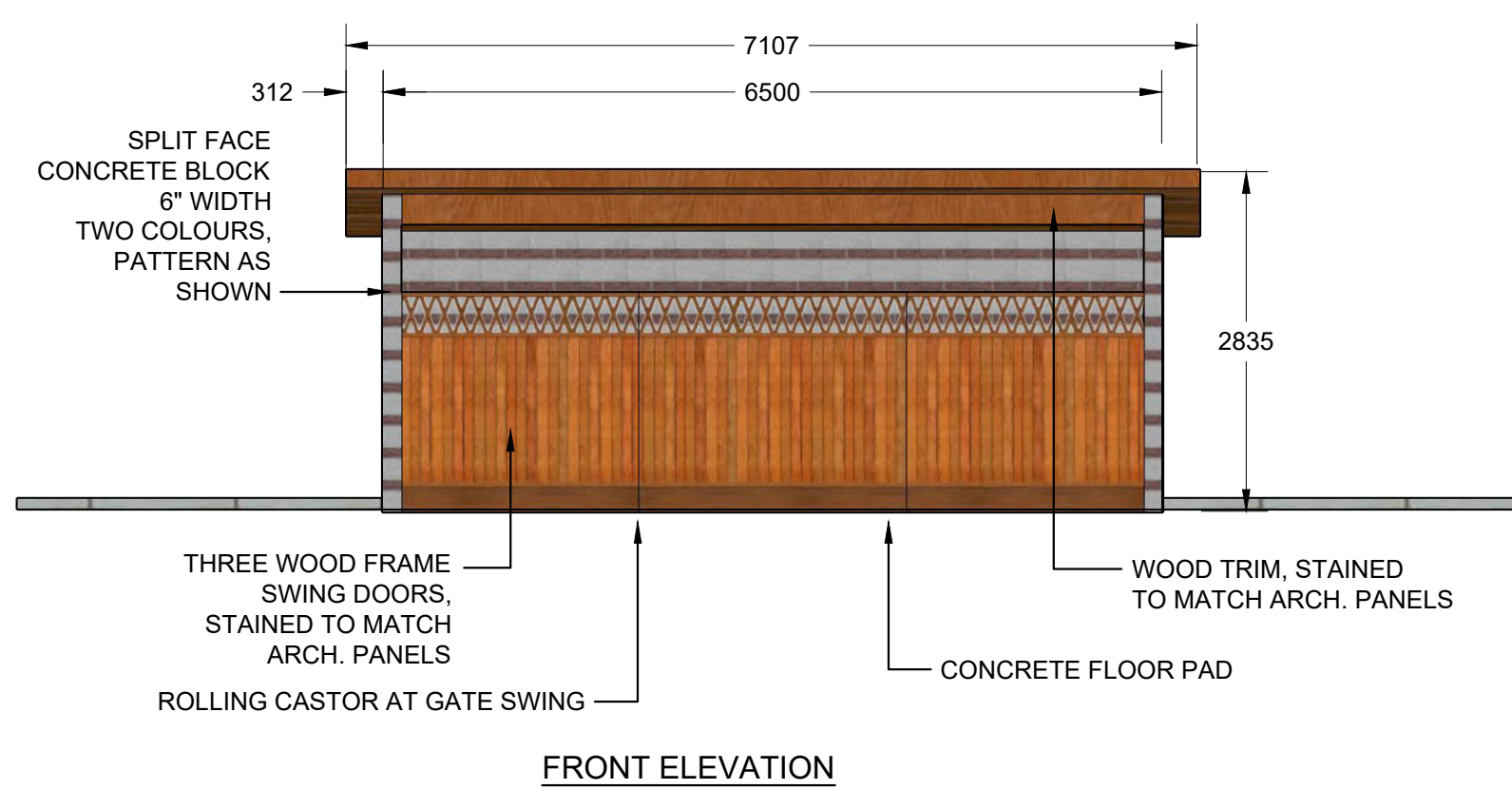


NOTE: PAVERS IN TPZ TO BE INSTALLED UNDER SUPERVISION OF PROJECT ARBORIST.

6 PERMEABLE PAVERS AND EDGE RESTRAINT
SCALE 1:10



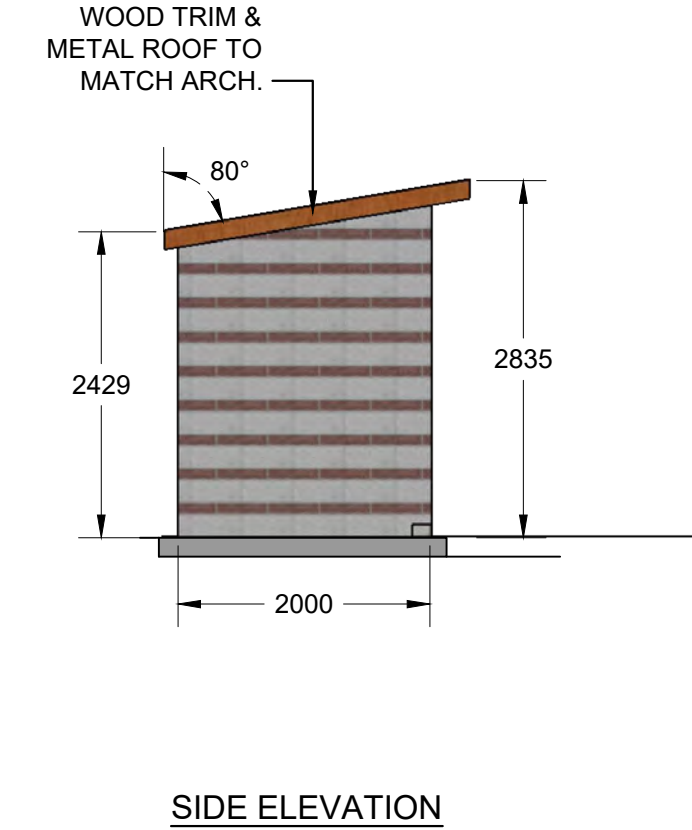
PERMEABLE PAVERS: ECO-PRIORA STANDARD PAVER RUNNING BOND PATTERN; PROVIDE COLOUR SAMPLE, TBD BY OWNER; OR SIMILAR, TO BE PREAPPROVED BY OWNER. INSTALL TO MEET ICPI & MANUFACTURER'S SPECIFICATIONS.



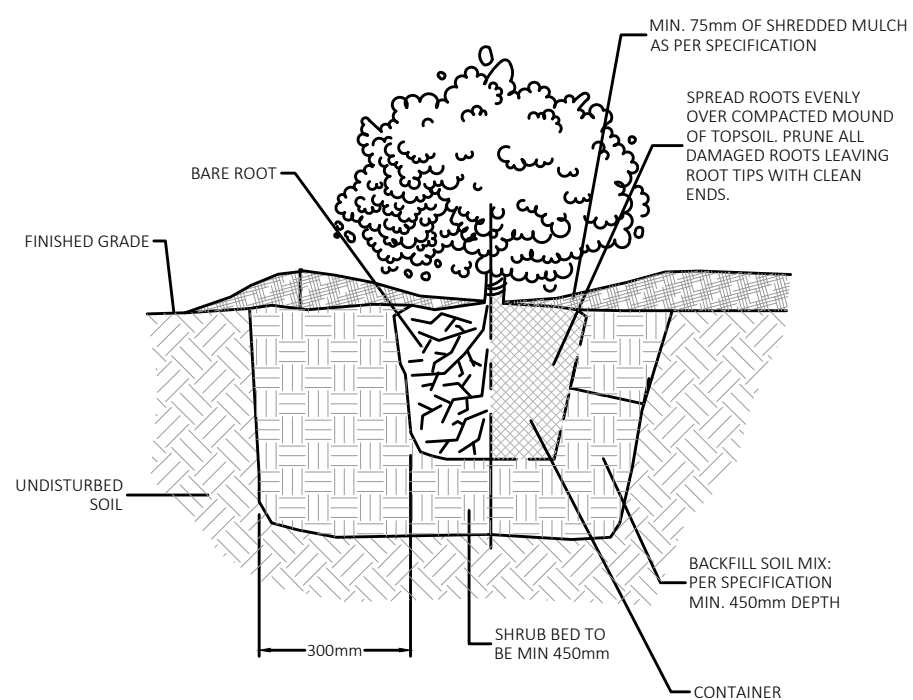
NOTE

1. ALL POSTS PRESSURE TREATED TO CSA STANDARD AND END CUTS TREATED WITH PRESERVATIVE.
2. ALL OTHER MEMBERS TO BE HEM/FIR #2 (CONSTRUCTION) GRADE MINIMUM.
3. ALL HARDWARE HEAVY DUTY, HOT DIPPED GALVANIZED.
4. APPLY 2 COATS EXTERIOR STAIN TO MANUFACTURERS SPECIFICATION. FINISH SELECTION AS APPROVED BY OWNER.
5. ALL PANELS TO BE LEVEL.

7 GARBAGE ENCLOSURE CONCEPT
SCALE 1:60



SIDE ELEVATION



NOTE:
-LOOSEN ROOT MASS PRIOR TO PLANTING.
-CONTAINER IS TO BE CUT CAREFULLY AWAY. SHRUB SHALL NOT BE PULLED FROM CONTAINER.
-THE USE OF POLY EDGING AND FILTER FABRIC IS NOT PERMITTED.

5 TYPICAL SHRUB PLANTING
SCALE 1:15

SEAL:

NO.	DATE	REVISION DESCRIPTION	DR.
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2	21.MAR.01	DP SUBMISSION	BJ
1	21.FEB.16	COORDINATION - IN PROGRESS	BJ

CLIENT:

PROJECT:

NINE PEAKS

**1590 PIERCY AVENUE
COURTENAY, BC**

DRAWING TITLE:

DETAILS

DATE:	20.09.20	DRAWING NUMBER:	
SCALE:	VARIABLES		
DRAWN:	BJ		
DESIGN:			
CHK'D:	PCM		

L4

OF 5



Suite C100 - 4185 Still Creek Drive
Burnaby, British Columbia, V5C 6G9
p: 604 294-0011 | f: 604 294-0022

SEAL:

PART ONE GENERAL REQUIREMENTS

- 11 REFERENCES**
- 1.1 CCOC Doc 2 2008
Comply with all articles in the General Conditions of Contract in conjunction with this section unless superseded by other Contract Documents.
 - 1.2 B.C. Landscape Standard, 2012 edition, prepared by the B.C. Society of Landscape Architects and the B.C. Landscape & Nursery Association, jointly. All work and materials must meet standards as set out in the B.C. Landscape Standard unless superseded by this Landscape Architect with written instruction.
 - 1.3 MASTER MUNICIPAL SPECIFICATIONS & STANDARD DETAILS, 2000 edition, prepared by the Consulting Engineers of British Columbia, Roadbuilders and Heavy Construction Association, and the Municipal Engineers Division
 - 1.4 STANDARD FOR LANDSCAPE IRRIGATION SYSTEM, 2008. Prepared by the Irrigation Industry Association of British Columbia.
 - 1.5 MUNICIPAL BYLAWS AND ENGINEERING SPECIFICATIONS WHERE NOTED.
- 12 TESTING**
- 1.1 A current form more than one month test for all growing medium to be used on this site is required. Provide and pay for testing by an independent testing facility pre-approved by the Landscape Architect. Deliver growing medium test results to Landscape Architect for review and approval prior to placement. Refer to Section 3.4 Growing Medium Testing for procedure.
 - 1.2 Owner reserves the right to re-test materials. Contractor responsible to pay for testing if materials do not meet specification.
- 13 SUBMITTALS**
- 1.1 Any alternate products differing from that contained in the contract documents must be pre-approved by the Landscape Architect.
 - 1.2 Submittals to consist of product sample or manufacturer's product description.
- 14 SITE REVIEW**
- 1.1 Under the Terms of the Landscape Architect's Contract with the Owner and where the Landscape Architect is the designated reviewer, the Landscape Architect will observe construction as necessary in their opinion to confirm conformance to the plans and specifications. Contact Owners Representative to arrange for site observation at the appropriate times. Allow two days notice. Observation schedule may include but will not be limited to the following:
 - 1.1.1 Start Up Site Meeting, General Contract: Prior to any site disturbance, a meeting with the general contractor to review free preservation issues, general landscape issues and municipal requirements.
 - 1.1.2 Start Up Site Meeting, Landscape Contract (if separate): At the start of work with Owner's Representative, Site Superintendent and Landscape Contractor; a meeting is to be held to review expected work and to verify the acceptability of the subgrade and general site conditions to the Landscape Contractor. Provide growing medium test results for this meeting.
 - 1.1.3 Progress Site Visits: To observe materials and workmanship as necessary through the course of the work. Review of different aspects of the work may deal with on any single visit. Such elements may include: Site Layout, Rough Grading, Growing Medium - quality, depths, finish grading, Drainage and Drainage Materials, Lawns or Grass areas; Planting - plant material including negotiations with suppliers, nursery inspections, plant sizes, quality, quantity, planting practice and layout, (tree support), Mulch, Irrigation Systems, Play Equipment, Site Furniture, and other elements of the site development where the Landscape Architect is the designated reviewer such as Pedestrian Paving, Fencing, Non-structural walls and stairs, Util Paving.
 - 1.1.4 Substantial Performance: Review of all work, accounting of all substitutions, deletions; plant counts, preparations of deficiency list, and recommendations for completion.
 - 1.1.5 Certificate of Completion: Upon the declaration of Substantial Performance, a recommendation for the issuance of the Certificate of Completion will be made to the Payment Certifier as defined in the contract.
 - 1.1.6 Deficiency Review: Prior to the completion of the holdback period, check for completion of deficiencies. Once completed, a Schedule "C" will be issued where required.
 - 1.1.7 Warranty Review: Prior to the completion of the warranty period (1-11 months after issuance of the Certificate of Completion), review all warranty material and report recommendations for warranty replacement.
- 15 WORKMANSHIP**
- 1.1 Unless otherwise instructed in the Contract Documents, the preparation of the subgrade shall be the responsibility of the General Contractor. Placement of growing medium constitutes acceptance of the subgrade by the Landscape Contractor. Any subsequent corrections to the subgrade required are the responsibility of the Landscape Contractor.
 - 1.2 All work and superintendence shall be performed by personnel skilled in landscape contracting. In addition, all personnel applying herbicides and/or pesticides shall hold a current license issued by the appropriate authorities.
 - 1.3 A site visit is required to become familiar with site conditions before bidding and before start of work.
 - 1.4 Confirm location of all services before proceeding with any work.
 - 1.5 Notify Landscape Architect of any discrepancies. Obtain approval from Landscape Architect prior to deviating from the plans.
 - 1.6 Take appropriate measures to avoid environmental damage. Do not dump any waste materials into water bodies. Conform with all federal, provincial and local statutes and guidelines.
 - 1.7 Collect and dispose of all debris and/or excess material from landscape operations. Keep paved surfaces clean and repair damage resulting from landscape work. Repairs are to be completed prior to final acceptance.
 - 1.8 Where new work connects with existing, and where existing work is altered, make good to match existing undisturbed condition.
- 16 WARRANTIES**
- 1.1 Guarantee all materials and workmanship for a minimum period of one full year from the date of Certificate of Completion.
 - 1.2 Refer to individual sections for specific warranties.

PART TWO SCOPE OF WORK

- 21 SCOPE OF WORK**
- 1.1 Other conditions of Contract may apply. Confirm Scope of Work at time of tender.
 - 1.2 Work includes supply of all related items and performing all operations necessary to complete the work in accordance with the drawings and specifications and generally consists of the following:
 - 1.1 Retention of Existing Trees where shown on drawings.
 - 1.2 Finish Grading and Landscape Drainage.
 - 1.3 Supply and placement of growing medium.
 - 1.4 Testing of imported growing medium and/or site topsoil.
 - 1.5 Supply and incorporation of additives to meet requirements of soil test and Table One.
 - 1.6 Preparation of planting beds, supply of plant material and planting.
 - 1.7 Preparation of rough grass areas, supply of materials and seeding.
 - 1.8 Preparation of lawn areas, supply of materials and sodding.
 - 1.9 Supply and placement of bark mulch.
 - 1.10 Maintenance of planted and seeded/sodded areas until accepted by Owner.
 - 1.11 SEPARATE PRICE: Establishment Maintenance, Section 3.11.
 - 1.12 Other work: Work other than this list, not specified by Landscape Architect.
- 22 MATERIALS**
- 1.1 Growing Medium: Conform to B.C. Landscape Standard for definitions of imported and on-site topsoil. Refer to Table One below.

TABLE ONE: PROPERTIES OF GROWING MEDIUM FOR LEVEL 2 GROWNED AND LEVEL 1 MODERATE AREAS Canadian System of Soil Classification Textural Class "Loamy Sand" to "Sandy Loam".				
Applications	Low Traffic Areas, Trees and Large Shrubs	High Traffic Areas, Lawn Areas	Planting Areas and Planters	
Growing Medium Types	ZL	ZH	ZP	
Percent Of Dry Weight of Total Growing Medium				
Texture				
Coarse Gravel: larger than 25mm	0 - 1%	0 - 1%	0 - 1%	0 - 1%
All Gravel: larger than 2mm	0 - 5%	0 - 5%	0 - 5%	0 - 5%
Percent Of Dry Weight of Growing Medium Excluding Gravel				
Sand: larger than 0.05mm smaller than 2.0mm	50 - 80%	70 - 90%		40 - 80%
Silt: larger than 0.002mm smaller than 0.05mm	10 - 25%	0 - 15%		10 - 25%
Clay: smaller than 0.002mm	0 - 25%	0 - 15%		0 - 25%
Clay and Silt Combined	maximum 35%	maximum 35%		maximum 35%
Organic Content (overall)	3 - 10%	3 - 5%		10 - 20%
Organic Content (interior)	3 - 5%	3 - 5%		15 - 20%
Acidity (pH)	6.0 - 7.0	6.0 - 7.0		4.5 - 6.5
Drainage	Percolation shall be such that no standing water is visible 60 minutes after at least 10 minutes of moderate to heavy rain or irrigation.			

- 1.1 Fertilizer: An organic and/or inorganic compound containing Nitrogen (N), Phosphate (P), and Potash (Soluble 2) in proportions required by soil test.
- 1.2 Lime: Ground agricultural limestone. Meet requirements of the B.C. Landscape Standard.
- 1.3 Organic Additive: Commercial compost product to the requirements of the B.C. Landscape Standard, 6th edition and pre-approved by the Landscape Architect. Recommended suppliers: The Answer Garden Products, Fraser Richmond Soils & Fibre, Stream Organics Management.
- 1.4 Sand: Clean, washed pump sand to meet requirements of the B.C. Landscape Standard.
- 1.5 Composted Bark Mulch: 10mm (3/8") minus F19 Fleckback bark chips and fines, free of chunks and sticks, dark brown in colour and free of all soil, stones, roots or other extraneous matter. Fresh orange or colour bark will be rejected.
- 1.6 Herbicides and Pesticides: If used, must conform to all federal, provincial and local statutes. Applies must hold current licenses issued by the appropriate authorities in the area.
- 1.7 Filter Fabric: A non biodegradable blanket or other filtering membrane that will allow the passage of water but not fine soil particles. (Such as MIRAFL 140 ML, GEOLON 140 GR ANECO 455S or alternate product pre-approved by the Landscape Architect.)
- 1.8 Drainage Piping if required: Schedule 40 PVC nominal sizes.
- 1.9 Drain Rock: Clean, round, inert, durable, and have a maximum size of 19mm and containing no material smaller than 10mm.
- 1.10 Plant Material: To the requirements of the B.C. Landscape Standard. Refer to 3.9, Plants and Planting. All plant material must be provided from a certified disease free nursery. Provide proof of certification.
- 1.11 Sod: Refer to individual sections in this specification.
- 1.12 Supplier and installers of segmental block walls to provide engineered drawings for all walls: signed and sealed drawings for all walls, individually, in excess of 12m, or combinations of walls collectively in excess of 12m. Installations must be reviewed and signed off by Certified Professional Engineer; include cost of engineering services in tender price.
- 1.13 Miscellaneous: Any other material necessary to complete the project as shown on the drawings and described herein.

PART THREE SOFT LANDSCAPE DEVELOPMENT

- 3.1 RETENTION OF EXISTING TREES**
- 1.1 Prior to any work to be performed individual trees or plant groupings indicated as retained on landscape plans as vegetation retention areas.
 - 1.1.1 In some instances the Landscape Architect will tag trees or areas to remain. Discuss retention areas at a start-up meeting with the Landscape Architect.
 - 1.2 A physical barrier must be installed to delineate clearing boundaries. Refer to physical barrier detail. If detail not provided, comply with local municipal requirements.
 - 1.3 No machine travel through or within vegetation retention areas or under crowns of trees to be retained is allowed.
 - 1.4 Do not stockpile soil, construction materials, or excavated materials within vegetation retention areas.
 - 1.5 Do not park, fuel or service vehicles within vegetation retention areas.
 - 1.6 No debris fires, clearing fires or trash burning shall be permitted within vegetation retention areas.
 - 1.7 No excavations, drain or service trenches nor any other disruption shall be permitted within vegetation retention areas without a review of the proposed encroachment by the Landscape Architect.
 - 1.8 Do not cut branches or roots of retained trees without the approval of the Landscape Architect.
 - 1.9 Any damage to existing vegetation intended for preservation will be subject to evaluation by an I.S.A. Certified Arborist using the "Guide for Plant Appraisal", Eighth Edition, 1992.
 - 1.10 Replacement of equivalent value to the disturbance will be required. The cost of the evaluation and of the replacement planting will be the responsibility of the General Contractor and the person(s) responsible for the disturbance.
 - 1.11 In municipalities with specific tree retention/replacement bylaws ensure compliance to bylaws.
 - 1.12 In situations where required construction may disturb existing vegetation intended for preservation, contact Landscape Architect for review prior to commencing construction.

- 3.2 GRADES**
- 1.1 Ensure subgrade is prepared to conform to depths specified in Section 3.5, Growing Medium Supply, below. Where planting is indicated close to existing trees, prepare suitable planting pockets for material defined on the planting plan. Shape subgrade to eliminate free standing water and conform to the site grading and drainage plan.
 - 1.2 On slopes in excess of 3:1 trench subgrade across slope to 150mm (6") minimum at 1.5m (5 ft) intervals minimum.
 - 1.3 Scarify the entire subgrade immediately prior to planting growing medium. Re-cultivate where vehicular traffic results in compaction during the construction procedures. Ensure that all planting areas are smoothly contoured after light compaction to finished grade.
 - 1.4 Eliminate standing water from all finished grades. Provide a smooth, firm and even surface and conform to grades shown on the Landscape Drawings. Do not exceed maximum and minimum gradients defined by the B.C. Landscape Standard.
 - 1.5 Construct swales to line and grade, smooth and free of sags or high points. Minimum slope 2%, maximum side slopes 10%. Assume positive drainage to collection points.
 - 1.6 Slope not to exceed the following maximums: Rough Grass 3:1, Lawn 4:1, Landscape plantings 2:1
 - 1.7 Finished soil/mulch elevation at building to comply with municipal requirements.
 - 1.8 Inform Landscape Architect of completion of finish grade prior to placement of seed, sod, plants or mulch.

- 3.3 LANDSCAPE DRAINAGE**
- 1.1 Related Work: Growing medium and Finish Grading, Grass areas, Trees Shrubs and Groundcovers, Planters, Crb Walls.
 - 1.2 Work Included: Site finish grading and surface drainage. Installation of any drainage systems detailed on landscape plans. Note Catch basins shown on landscape plans for coordination only, confirm scope of work prior to bid.
 - 1.3 Coordinate all landscape drainage work with rest of site drainage. Refer to engineering drawings and specifications for connections and other drainage work.
 - 1.4 Determine exact location of all existing utilities and structures and underground utilities prior to commencing work, which may not be located on drawings and conduct work so as to prevent interruption of service to the Owner. Protect existing structures and utility services and be responsible for damage caused.
 - 1.5 Planter drains on slab. Refer to Section 3.10, Installing Landscapes on Structures.
 - 1.6 Execution
 - 1.1 Do trenching and backfilling in accordance with engineering details and specifications.
 - 1.2 Lay drains as prepared bed, true to line and grade with invert's smooth and free of sags or high points. Ensure barrel of each pipe is in contact with bed throughout full length.
 - 1.3 Commence laying pipe at outlet and proceed in upstream direction.
 - 1.4 Lay perforated pipes with perforations at 90 and 180 positions.
 - 1.5 Make joints tight in accordance with manufacturer's directions.
 - 1.6 Do not allow water to flow through the pipes during construction except as approved by Engineer.
 - 1.7 Make watertight connections to existing drains, new or existing manholes or catchbasins where indicated or as directed by Landscape Architect.
 - 1.8 Plug upstream ends of pipe with watertight clean out caps.
 - 1.9 Surround and cover pipe with drain rock in uniform 50mm layers to various depths as shown in details, minimum 100mm.
 - 1.10 Cover drain rock with non-woven filter cloth lap all edges and seams minimum 50mm.
 - 1.11 Assume positive drainage.
 - 1.12 Back fill remainder of trench as indicated.
 - 1.13 Protect subdrains from floatation during installation.

- 3.4 GROWING MEDIUM TESTING**
- 1.1 Submit representative sample of growing medium proposed for use on this project to an independent laboratory. Provide test results to Landscape Architect prior to planting. Test results to include:
 - 1.1 Physical properties, % content of gravel, sand, silt, clay and organics.
 - 1.2 Acidity (pH) and quantities of lime or sulphur required to bring within specified range.
 - 1.3 Nutrient levels of principle and trace elements and recommendations for required soil amendments.
 - 1.4 Carbon/Nitrogen level.

- 3.5 GROWING MEDIUM SUPPLY AND PLACEMENT**
- 1.1 Supply all growing medium required for the performance of the Contract. Do not load, transport or spread growing medium when it is so wet that its structure is likely to be damaged.
 - 1.2 Supply all growing medium amendments as required by the soil test. Amended growing medium must meet the specification for growing medium as defined in Table One for the various areas.
 - 1.3 Thoroughly mix required amendments into the full depth of the growing medium.
 - 1.2 Special mixes may be required for various situations. Refer to drawing notes for instructions.
 - 1.3 Place the amended growing medium in all grass and planting areas. Spread growing medium in uniform layers not exceeding 6" (150mm), over unfrozen subgrade free of standing water.
 - 1.1 Minimum depths of growing medium placed and compacted to 80%:
 - 4.1 On-grade:
 - 4.1.1 Seeded and sodded lawn: 12" (300mm)
 - 4.1.2 Grass planted shrubs & groundcovers: 18" (450mm)
 - 4.1.3 Groundcover only areas, if defined on plan: 9" (225mm)
 - 4.1.4 Tree & large shrub pits: _____ depth to conform to depth of rootball - width shall be at least twice the width of the root ball with saucer shaped edges.
 - 4.2 4-8" Sub:
 - 4.2.1 Graded lawn: 9" (225mm)
 - 4.2.2 Groundcover areas: 12" (300mm)
 - 4.3 Lawn without automatic irrigation: 12" (300mm)
 - 4.4 Shrub & groundcover areas: 18" (450mm)
 - 4.5 Trees and specimen shrubs: 30" (762mm) over columns and/or edge of slab (verify column locations on-site for tree locations)
 - 4.6 Depth noted includes 1" to 2" (25-50mm) sand over filter fabric.
 - 4.7 Maximum 18" depth growing medium except where rounded for trees over column pits.
 - 1.4 Manually spread growing medium/planting soil around existing trees, shrubs and obstacles.
 - 1.5 In perimeter seeded grass areas, feather growing medium out to nothing at edges and blend into existing grades.
 - 1.7 Finished grades shall conform to the elevations shown on landscape and site plans.

- 3.6 ROUGH GRASS AREA - SEEDING**
- 1.1 Design: Rough grass areas are noted on the drawings as "Rough Grass". Treat all areas defined as rough grass between all property lines of the project including all boulevards, edge of roads and lanes.
 - 1.2 Preparation of Surface: To B.C. Landscape Standard Class 3 Areas (Rough grass) Section 3.11.3
 - 1.2 Clean existing soil by mechanical means or do nothing at edges and blend into existing grades.
 - 1.2 Roughly grade surfaces to allow for maintenance specified and for positive drainage.
 - 1.3 Time of Seeding: Seed from early spring (generally April 1st) to late fall (September 15th) of each year. Further extensions may be obtained on concurrence of the Landscape Architect.

- 1.4 Seed Supply & Testing: All seed must be obtained from a recognized seed supplier and shall be No. 1 grass mixture delivered in containers bearing the following information:
 - 4.1 Analysis of the seed mixture
 - 4.2 Percentage of each seed type
- 1.5 Seed Mixture: All varieties shall be rated as strong performers in the Pacific Northwest and be subject to client approval.
 - 100% Creeping Red Fescue
 - 20% Annual Ryegrass
 - 5% Saturo Perennial Ryegrass
 - 5% Kentucky Bluegrass
 For Meadows Areas use a mixture of Wildflowers with Hard Fescues (Ternstroem Coastal Wildflowers) with Hard Fescue of pre-approved alternate.
- 1.6 Fertilizer: Mechanical seeding: Apply a complete synthetic slow-release fertilizer with maximum 35% water soluble nitrogen and a formulation ratio of 18-18-18 - 50% sulphur urea coated, 102 kg/ha(1000kg/acre) using a mechanical spreader.
- 1.7 Seeding: Apply seed at a rate of 102kg/ha (1000kg/acre) with a mechanical spreader. Incorporate seed into the top 1 1/4" (30mm) of soil and lightly compact.
- 1.8 Acceptance: Provide adequate protection of the seeded areas until conditions of acceptance have been met. Comply with Section 3.7 Hydroseeding.

- 3.7 HYDROSEEDING**
- 1.1 May be used as an alternate to mechanical seeding in rough grass areas.
 - 1.2 May not be used in areas of Lawn unless pre-approved by the Landscape Architect prior to bidding.
 - 1.3 Preparation and Growing Medium
 - 1.1 In areas of Rough Grass Comply with Section 3.6 Rough Grass.
 - 1.2 Where approved for use in areas of lawn, comply with Section 3.8 Lawn Areas Seeding.
 - 1.4 Protection: Ensure that fertilizer in solution does not come in contact with the foliage of any trees, shrubs, or other susceptible vegetation. Do not spray seed or mix on objects not expected to grow grass. Protect existing site equipment, roadways, landscaping, reference points, monuments, markers and structures from damage. Where contamination occurs, remove seeding slurry to satisfaction of and by means approved by the Landscape Architect.
 - 1.5 Mulch shall consist of virgin wood fibre or recycled paper fibre designed for hydrolic seeding and give the appearance of mulch application. If using recycled paper material for wood fibre substitute use 95% by weight. Conform to B.C. Landscape Standard for mulch requirements.
 - 1.6 Water: Shall be free of any impurities that may have an injurious effect on the success of seeding or may be harmful to the environment.
 - 1.7 Equipment: Use industry standard hydrolic seeder/mulcher equipment with the tank volume certified by an identification plate or sticker affixed in plain view on the equipment. The hydrolic seeder/mulcher shall be capable of sufficient agitation to mix the mulch into a homogeneous slurry and to maintain the slurry in a homogeneous state until it is applied. The discharge pump and gun nozzles shall be capable of applying the materials uniformly over the designated area.

PART THREE SOFT LANDSCAPE DEVELOPMENT - CONT

- 3.8 LAWN AREAS - SODDING**
- 1.1 General: Treat all areas defined as lawn areas on the landscape plan between all property lines of the project including all boulevards to edge of roads and lanes.
 - 1.2 Growing Medium: Comply with Section 2.2.1, Growing Medium. Prior to sodding, request an inspection of the finished grade, and depth and condition of growing medium by the Landscape Architect.
 - 1.3 Time of Sodding: Sod from April 1st to October 31st. Further extensions may be obtained on concurrence of the Landscape Architect.
 - 1.4 Sod Supply: Conform to all conditions of B.C. Landscape Standard, Section 8, B.C. Standard for Turfgrass Sod.
 - 1.5 Specified Turfgrass by Area: Refer to Table 2 below.
- | TABLE 2 SPECIFIED TURFGRASS BY AREA | | | |
|-------------------------------------|--|------------------|--|
| Area | Description | Quality Grade | Major Species |
| CLASS 1 | Lawn, all areas noted on drawings as lawn in urban development sites including boulevard grass | No. 1 Premium | Kentucky Blue for sun, Fescues for shade |
| CLASS 2 | Grass - public parks, industrial and institutional sites | No. 2 Standard | same |
| CLASS 3 | Rough Grass | see hydroseeding | |
| SPECIAL | | | |
- 1.6 Line: The line shall be as defined in Section 2.2.3, Materials. Apply at rates recommended in required soil test. Refer to Section 3.4 for method.
 - 1.7 Fertilizer: Refer to Section 2.2.2, Materials. Apply specified fertilizer at rates shown in the required soil test. Apply with a mechanical spreader. Cultivate into growing medium 48 hours prior to sodding. Apply separately from lime.
 - 1.8 Sodding: Prepare a smooth, firm, even surface for laying sod. Lay sod staggered with sections closely butted, without overlapping or gaps, smooth and even with adjoining areas and rock lightly. Water to obtain moisture penetration of 3" to 4" (75 - 100mm). Comply with requirements of B.C. Landscape Standard Section 8, B.C. Standard for Turfgrass Sod.
 - 1.9 Maintenance: Begin maintenance immediately after sodding and continue for 60 days after Substantial Completion and until accepted by the Owner. Protect sodded areas from damage with temporary wire or luvine fences complete with signage until lawn is taken over by the Owner. Water to obtain moisture penetration of 3" to 4" (75-100mm) at intervals necessary to maintain sufficient growth. Keep grass cut at height of between 1-1/2" (40mm) and 2" (50mm). Provide adequate protection of sodded areas against damage until the turf has been taken over by Owner. Repair any damaged areas, re-grade as necessary. Aeration may be required if in the Landscape Architect's option, until the sod base medium is repaired.
 - 1.10 Acceptance of Lawn Areas: The turf shall be reasonably well established, with no apparent dead spots or bare spots and shall be reasonably free of weeds (to B.C. Landscape Standard, Section 13 Maintenance Level 2 Appearance). Use herbicides if necessary for weed removal unless other conditions of contract forbid their use. After the lawn has been cut at least twice, areas meeting the conditions above will be taken over by the Owner.

- 3.9 PLANTS AND PLANTING**
- 1.1 Conform to planting layout as shown on Landscape Plans.
 - 1.2 Obtain approval of Landscape Architect for layout and preparation of planting prior to commencement of planting operations.
 - 1.3 Make edge of beds with smooth clean defined lines.
 - 1.4 Time of Planting:
 - 1.1 Plant trees, shrubs and groundcovers only during periods that are normal for such work as determined by local weather conditions when seasonal conditions are likely to ensure successful adaptation of plants to their new location.
 - 1.5 Standards:
 - 1.1 All plant material shall conform to the requirements of the B.C. Landscape Standard, 7th edition 2008, unless exceeded by drawing Plant Schedule or this specification.
 - 1.1.1 Refer to B.C. Landscape Standard, Section 9, Plants and Planting and to Section 12, BC/LNA Standard for Container Grown Plants for minimum standards.
 - 1.1.2 Refer to Plant Schedule for specific plant and container sizes and comply with requirements.
 - 1.1.2 Plant material obtained from areas with less severe climatic conditions shall be grown to withstand the site climate.
 - 1.6 Review:
 - 1.1 Review at the source of supply and/or collection point does not prevent subsequent rejection of any or all planting stock at the site.
 - 1.1.1 Area of search includes the Lower Mainland and Fraser Valley; refer to Plant Schedule for any extension of area.
 - 1.1.2 Supply proof of the availability of the specified plant material within 30 days of the award of the Contract.
 - 1.7 Substitution:
 - 1.1 Obtain written approval of the Landscape Architect prior to making any substitutions to the specified material. Non-approved substitutions will be rejected.
 - 1.2 Allow a minimum of 5 days prior to delivery for request to substitute.
 - 1.3 Substitutions are subject to B.C. Landscape Standard - definition of Conditions of Availability.
 - 1.8 Plant Species Location:
 - 1.1 Plants shall be true to name and of the height, caliper and size of root ball as shown on the landscape/site plant schedule. Caliper of trees is to be taken 6" (15cm) above grade.
 - 1.2 Plant all specified species in the location as shown on the landscape drawings. Notify Landscape Architect if conflicting rock or underground/overhead services are encountered.
 - 1.3 Deviation of given planting location will only be allowed after review of the proposed deviation by the Landscape Architect.
 - 1.9 Excavation:
 - 1.1 Trees and large shrubs: Excavate a saucer shaped tree pit to the depth of the rootball and to at least twice the width of the rootball. Assume that finished grade is at the original grade the tree was grown at.
 - 1.10 Drainage of Planting Holes:
 - 1.1 Provide drainage of planting pits where required, ie on sloped conditions, break out the side of the planting pit to allow drainage down slope, and in flat conditions, mound to raise the rootball above impervious layer. Notify the Landscape Architect where the drainage of planting holes is limited.
 - 1.11 Planting and Fertilizing Procedures:
 - 1.1.1 Plant all trees and shrubs with the roots placed in their natural growing position. If burtopped, loosen around the top of the ball and cut away or fold under. Do not pull burtop from under the ball. Carefully remove containers without injuring the rootball. After settled in place, cut twine. For wire baskets, clip and remove top three rows of wire.
 - 1.1.2 Fill the planting holes by gently firming the growing medium around the root system in 6" (15cm) layers. Settle the soil with water. Add soil as required to meet finish grade. Leave no air voids. When 2/3 of the topsoil has been placed, apply fertilizer as recommended by the required soil test at the specified rates.
 - 1.1.3 Where planting is indicated adjacent to existing trees, use special care to avoid disturbance of the root system or natural grades of such trees.
 - 1.1.4 Where trees are in lawn areas, provide a clean cut mulched 900mm (3 ft) diameter circle centered on the tree.
 - 1.12 Staking of Trees:
 - 1.1.1 Use two 2"x2 1/2" stakes, unless superseded by municipal requirements. Set stakes minimum 2 ft. in soil. Do not drive stake through rootball.
 - 1.1.2 Leave the tree carefully vertical.
 - 1.1.3 Tie with pre-approved material, flat woven polypropylene fabric belt, minimum width 30mm (3/4"). Approved product: Arbor Tie - available from DeepRoot.
 - 1.1.4 Confine trees over 4 ft. high: Guy with three 3/8" strand wires (11 gauge). Drive three stakes equidistant around the tree completely below grade.
 - 1.1.5 Trees 6 ft. + on Wood or Concrete Decks: Guy as above using three deadend (4m, 2"x2"x4") buried to the maximum possible depth instead of stakes.
 - 1.1.6 Mark all guy wires with visible flagging material.
 - 1.13 Pruning:
 - 1.1.1 Limit pruning to the minimum necessary to remove dead or injured branches. Preserve the natural character of the plants, do not cut the leader. Use only clean, sharp tools. Make all cuts clean and cut to the branch collar leaving no stubs. Shape affected areas so as not to retain water. Remove damaged material.
 - 1.14 Mulching:
 - 1.1.1 Match all planting areas with an even layer of mulch to 2-1/2" - 3" (65 - 75mm) depth. Confirm placement of mulch in areas labeled "Groundcover Area" on drawings. Match 3 ft. (900mm) diameter around trees in lawn areas, leave a clean edge.
 - 1.15 Acceptance:
 - 1.1 The establishment of all plant material is the responsibility of the Landscape Architect.
 - 1.16 Plant Material Maintenance:
 - 1.1.1 Maintain all plant material for 60 days after landscape work has received a Certificate of Completion.
 - 1.1.2 Watering: Conform to B.C. Landscape Standard, Section 13.3.2 - Watering and generally as follows:
 - 1.1.2.1 Water to supplement natural rainfall such that the soil moisture content is kept to 50% to 100% of field capacity. Water to the full depth of the root zone each time. The Owner is responsible to supply water to extra cost to the Contractor.
 - 1.1.2.2 Confirm source of water prior to beginning work.
 - 1.1.2.3 Use appropriate measures to combat pests or diseases damaging plant material. Comply with all local governing statutes and guidelines for chemical control.
 - 1.1.3 Plant material which fails to survive shall be replanted in the next appropriate season as determined by the Landscape Architect.
 - 1.1.4 Repair tree guards, stakes, and guy wires, when necessary.
 - 1.1.5 Maintain areas relatively weed free. (Appearance level 2, B.C. Landscape Standard, Chapter 13).
 - 1.1.6 Maintain mulch to specified depths.
 - 1.17 Plant Warranty:
 - 1.1.1 Replace all unsatisfactory plant material except those designated "Specimen" for a period of one (1) year after the Certificate of Completion. Replace all unsatisfactory plant material designated "Specimen" for a period of two (2) years after the Certificate of Completion. Replace all unsatisfactory trees and shrubs and continue to replace these until the specified number is replaced to the satisfaction of the Landscape Architect. Such replacement shall be subject to the notification, inspection and approval as specified for the original planting, and shall not constitute an extra to the Contract.
 - 1.1.2 Those Plants, identified as hardy within one zone of the Canada Department of Agriculture hardiness zone for the area, specified by the Landscape Architect and installed by the Landscape Contractor which are killed through normal temperatures below the average of the extreme minimum temperatures officially recorded in the area concerned, in the last 10 years, will not be replaced without cost of replacement borne by the Owner.
 - 1.1.3 A review may be requested during the latter part of the warranty growing season. All plant material showing well developed foliage, healthy growth and bud forming, will then be taken over.

PART THREE SOFT LANDSCAPE DEVELOPMENT - CONT

- 1.18 For all plant material, the Landscape Architect reserves the right to extend the Contractor's responsibility for another growing season if, in his opinion, leaf development and growth is not sufficient to ensure future satisfactory growth.
 - 1.19 Where the Owner is responsible for plant maintenance and has not provided adequate maintenance, the plant replacement section of the contract may be declared void. The Landscape Architect shall determine whether maintenance has been satisfactory using the B.C. Landscape Standard, Section 13, Maintenance as the guide. The required maintenance standard is a minimum of Level Three - Medium. Refer to Section 3.11, Establishment Maintenance.
 - 1.20 The Landscape Contractor is responsible to replace any plant material or repair any construction included in the Contract that is damaged or stolen until the issuance of the Certificate of Completion.
 - 1.21 Deviation from the specifications may require extension of the Warranty Period as determined by the Landscape Architect.
- 3.10 INSTALLING LANDSCAPE ON STRUCTURES**
- 1.1 Verify that drainage and protection material is completely installed and acceptable before beginning work. Contact Landscape Architect for instructions if not in place.
 - 1.2 Coordinate work with construction of planters and planter drainage.
 - 1.3 Verify that planter drains are in place and provide drainage to roof drains is present prior to beginning work or slab rock or soil.
 - 1.4 Provide clean out at all through-slab drain locations. Use 300mm min. dia. PVC Pipe filled with drain rock unless specific drawing detail shown.
 - 1.5 Install drain rock evenly to a minimum depth of 4" (100mm) or alternate sheet drain if specified. Install sheet drain as per manufacturer's recommendations.
 - 1.6 Cover drain rock (or alternate sheet drain if specified on drawing details) with filter fabric (appx 6" (150mm) at all edges. Obtain approval of drainage system prior to placing growing medium.
 - 1.7 Place an even layer of 25 - 50mm clean washed pump sand over filter fabric.
 - 1.8 Place growing medium to depths specified in Section 3.5 above for various surface treatments. Refer to Drawing details for any light weight filler required after grade. Use Styrofoam block on drain rock shaped to provide smooth surface transition at edges. Butt each piece tightly together and cover with filter fabric to prevent soil from migrating down.

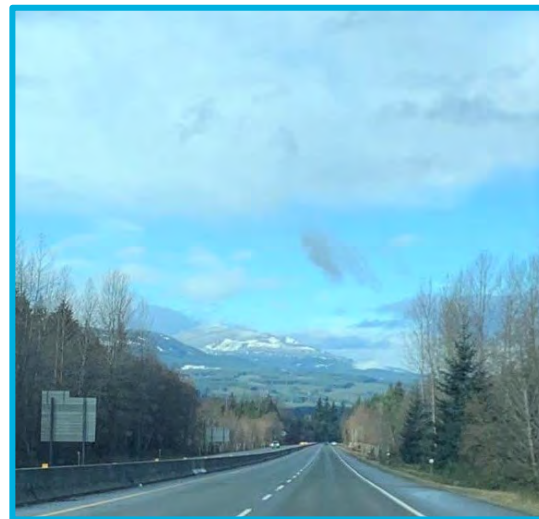
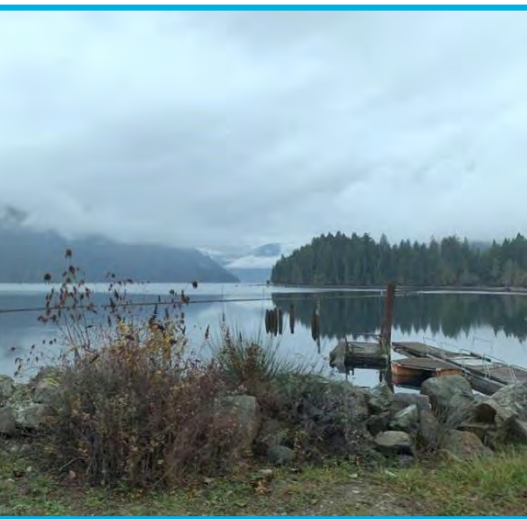
- 3.11 ESTABLISHMENT MAINTENANCE (Provide a separate price for this section)**
- 1.1 General: Treat all areas defined as lawn areas on the landscape plan between all property lines of the project including all boulevards to edge of roads and lanes.
 - 1.2 Intention: The intent of "establishment" maintenance is to provide sufficient care to newly installed plant material for a relatively short period of time to ensure or increase the long term success of the planting. The objective is the adaptation of plants to a new site in order to obtain the desired effect from the planting while reducing the rate of failure and unnecessary work associated with improper establishment. Establishment of maintenance procedures apply to all new and retained vegetation including cultivated turfgrass areas and new trees and shrubs.
 - 1.3 Maintenance Period: Provide maintenance of installed landscaping for 12 months following substantial completion.
 - 1.4 Related Standards and Legislation: B.C. Landscape Standard, latest edition; Fertilizer Code, B.C. Pesticide Control Act.
 - 1.5 Site Review: In addition to the inspections at substantial completion, at final program close application, and at the end of the guarantee period, there should be three other reviews during the 12 months attended by the Contractor and a designated representative of the Owner. Maintain a logbook and reporting procedures and submit to the designated representative.
 - 1.6 Scheduling: Prepare a schedule of anticipated visits and submit to designated representative at start-up. Maintenance operations shall be carried out predominantly during the growing season between March 1st and November 30th, however visits at other times of the year may be required.
 - 1.7 Maintenance Level: Comply with B.C. Landscape Standard, Section 14, Table 14.2, Maintenance Level 2 "Greened".
 - 1.8 Materials: Comply with Part Two of this specification.
 - 1.1 Fertilizers: To the requirements of the B.C. Landscape Standard. Formulations and rates as required by soil testing.
 - 1.9 Plant Material Establishment:
 - 1.1.1 Watering: During the first growing season, water new plants at least every ten (10) days between April 1st and July 31st, and every twenty (20) days between August 1st and September 15th. Minimum 25 gallons per tree per application. During the second growing season, water new plants at least every twenty days between April 1st and July 31st and once between August 1st and September 30th. Apply water at a rate and duration such that the water content reaches field capacity to the full depth of the growing medium. Apply water again when the water content reaches 25% of field capacity. Provide and irrigate with water in the event that any automatic irrigation system malfunctions or has not been completely installed. Scheduled applications of water shall be missed only when rainfall has penetrated the soil fully as required.
 - 1.1.2 Mutch: Maintain mulches in the original areas and to the original depths.
 - 1.1.3 Weed Control: Remove all weeds from all areas at least once per month during the growing season by hoeing or cultivation to a maximum depth of 80mm, hand-pulling, or, if necessary, by the use of herbicides.
 - 1.1.4 Pest and Disease Control: Inspect all planted areas for pests and diseases periodically and at least every two months during the growing season by an experienced person. Carry out treatment for pests or diseases promptly and consistently for maximum effectiveness. Comply with all B.C. Pesticide Control Act and municipal requirements.
 - 1.1.5 Tree Support: Maintain stakes, guy wires and ties one full growing season. Check ties at least every two



1590 PIERCY AVENUE

Parking Study

Build With Perspective Ltd.



WATT CONSULTING GROUP

2022 July 04

WATT VICTORIA
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1590 PIERCY AVENUE

Parking Study

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Date: 2022 July 04
Our File No: 3328.B01

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1.0 INTRODUCTION

Watt Consulting Group (WATT) was retained by Build with Perspective Ltd. to conduct a parking study for the proposed townhouse development at 1590 Piercy Avenue in the City of Courtenay, BC. The purpose of this study is to determine the total parking demand for the subject site..

1.1 SUBJECT SITE

The proposed development is located at 1590 Piercy Avenue in the City of Courtenay (See **Figure 1**).



Figure 1. Subject Site



1.2 SITE CHARACTERISTICS & POLICY CONSIDERATIONS

The following provides information regarding services and transportation options in proximity to the subject site (see **Figure 2**). In addition, the City of Courtenay's Official Community Plan (OCP) and other community policies pertaining to sustainable transportation and parking management are summarised.

COMMUNITY POLICIES



The City of Courtenay updated its Official Community Plan in June of 2022.¹ The OCP contains policy direction on a number of topics pertaining to parking and including the distinct goal of "Functional Transportation Choices". This goal directs the City to rebalance its transportation system to provide a more functional spectrum of options that prioritises walking, cycling, and transit. This in turn will support active living and transportation, neighbourliness, economic vitality, affordable transportation, and lower carbon footprints.

Part C: Streets and Transportation of the OCP, outlines several objectives and policies directly relevant to this study including:

- Objective 2: Transportation investments prioritise walking, cycling, and transit
- Objective 6: The amount of land dedicated to parking is minimized
- Objective 7: Parking standards reflect electric vehicle and cycling needs



SERVICES

The site is located approximately 1.1 kilometres from commercial / retail amenities including restaurants, a grocery store (Thrifty Foods) and cafés. There are also a number of personal and professional services within this distance including medical services, accounting, and wealth management

¹ City of Courtenay. (2022). Bylaw 3070: Official Community Plan, Available online at: <https://pub-courtenay.escribemeetings.com/filestream.ashx?DocumentId=2454>



offices. Courtenay Elementary School is 600m from the site (7-minute walk).



TRANSIT

There are two bus stops at the intersection of Cumberland Road and Piercy Avenue that are within a 300m (6-minute walk) of the subject site. The bus stops are served by the 8 (Downtown / Anfield Centre), which provides service to a number of destinations in the community including downtown Courtenay, Driftwood Mall, Anfield Centre, and schools such as Puntledge Park Elementary School and Lake Trail Secondary School.

The site is also located about 900m (11-minute walk) from a bus stop at Fitzgerald Avenue and 16th Street, which is served by the 1 (Comox Mall / Anfield Centre). The 1 operates as the Frequent Transit Network (FTN) route offering 20-minute service at peak hours between the municipalities of Courtenay and Comox. It provides service to a number of destinations including downtown Courtenay, North Island College, North Island Hospital Comox Valley, and downtown Comox, among other destinations.



WALKING

According to Walk Score, the subject site* can be described as “somewhat walkable” with a walk score of 54, suggesting that some errands may be accomplished on foot.² Sidewalks are located on both sides of Piercy Avenue and serve to connect residents to nearby transit stops on Cumberland Road.

² More information about the site’s walk score is available online at: <https://www.walkscore.com/score/1580-piercy-ave-courtenay-bc-canada>

*1580 Piercy Avenue as used in place of 1590 Piercy Avenue as 1580 Piercy Avenue is more up to date with current amenities and conditions on Walkscore.com



The Rotary Trail is a multi-use pathway northeast of the subject site that is part of the City’s long-term pedestrian network. This pathway travels northwest and southeast, terminating at 5th Street and 29th Street respectively and increases active transportation (AT) connectivity throughout the city.



CYCLING

The subject site has access to cycling infrastructure Cumberland Road and 17th street travelling Northeast into town, and southwest towards Cumberland; additionally, the Rotary Trail – at the north eastern border of the subject site - provides additional access to multiple areas of the city. According to the City’s Transportation Master Plan, a buffered / painted bicycle lane on 17th Street is proposed for the medium-term **Figure 2**. This facility will allow for greater connectivity - and safety - for residents of the subject site travelling to destinations such as downtown Courtenay and further onto Comox or North Island College. As of June 2022, construction has begun on the 17th Street bicycle facility upgrades.

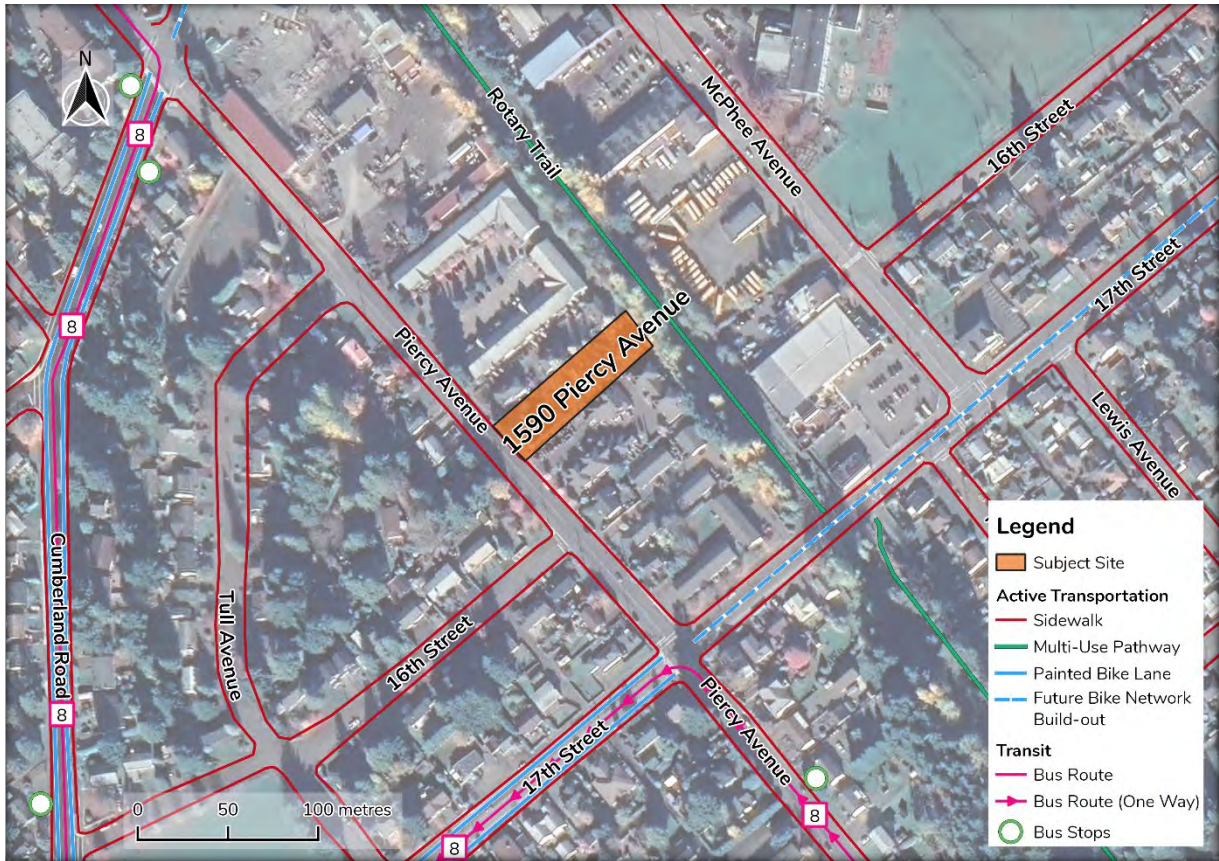


Figure 2. Transportation Options in Proximity to Site

1.3 CURRENT LAND USE

The site is currently zoned R-2 (Residential Two Zone) - which allows for single residential dwelling duplex, and accessory building uses – and is currently occupied by a single detached house.

2.0 PROPOSED DEVELOPMENT

2.1 LAND USE

The proposed development is for a nine-unit townhouse complex comprising 6 two-bedroom units and 3 three-bedroom units.



2.2 PROPOSED PARKING SUPPLY

2.2.1 RESIDENTIAL PARKING

A total of 10 parking spaces are proposed - a rate of 1.11 spaces per unit.

2.2.2 BICYCLE PARKING

The applicant is proposing one electrified long-term bicycle parking space per unit, in addition to two short term guest bicycle parking spaces. Section 6.0 includes specific bicycle parking recommendations for the applicant's consideration.

3.0 PARKING REQUIREMENT

3.1 VEHICLE PARKING

The City of Courtenay Bylaw No. 2500 (2007) determines the minimum parking supply requirement. Per the Bylaw, the site would be subject to the "Multi residential dwellings use" and be required to provide 1.5 parking spaces per residential unit with 10% of the required spaces being provided and retained for visitor parking.

With nine proposed units, this results in a requirement of 14 parking spaces (13.5 rounded), delineated as 13 spaces residential parking spaces and 1 as visitor parking spaces. This is four spaces greater than the proposed supply of 10 spaces parking spaces.

4.0 EXPECTED PARKING DEMAND

Expected parking demand for the site is estimated in the following sections to determine if the proposed supply will adequately accommodate demand. Expected parking demand is based on observations from other townhouse sites in Courtenay and Comox, as well as research from past parking studies.



4.1 RESIDENTIAL PARKING DEMAND

4.1.1 REPRESENTATIVE SITES

Observations were conducted at 8 townhouse sites in the City of Courtenay and Town of Comox, representing a total of 206 units. A breakdown of each site and how it corresponds to the site location can be found in **Table 1**.

- **Geographic Location |** All of the representative sites are located within the City of Courtenay and Town of Comox. This is based on walkability, access to transit, and access to commercial / retail amenities and is intended to encapsulate the unique socio-geographical features of the neighbourhood of the proposed development.
- **Walk Score |** This is a tool that ranks the walkability of a location based on its proximity to seven types of amenities: Dining and drinking, groceries, shopping, errands, parks, schools/education, and culture and entertainment. It is a useful tool for determining if a trip will require a vehicle, and may inform parking needs. The Walk Score of this development is 58, whereas the average Walk Score of the chosen representative sites is 30 (29.88, Rounded). This means that the proposed development will have greater walking access to amenities than most of the comparable townhouse developments in the region; thus, representative sites offer a more conservative estimate of parking demand.
- **Countable Parking Spaces.** The sites needed to have parking spaces that were visible and therefore countable. Many townhouse sites in the Comox Valley have enclosed garages or gated underground parking, making counting difficult.



TABLE 1. SUMMARY OF REPRESENTATIVE SITES

Address	Number of Units	Walk Score	Municipality
1500 Cumberland Ave	20	30	Courtenay
2061 Lake Trail Road // 2016 13th Street	33	31	Courtenay
2077 20th Street	40	19	Courtenay
1111 Edgett Road	24	37	Courtenay
1095 Edgett Road	21	29	Courtenay
1537 Noel Ave	36	14	Courtenay
1180 Braidwood	26	25	Comox
1580 Piercy Avenue	6	54	Courtenay

4.1.1 OBSERVATIONS

Observations were conducted during the following periods:

- Wednesday, 14 June 2022, from 12:00am to 1:00am
- Thursday, 15 June 2022, from 12:00am to 1:00am

Observations of parking utilisation were conducted at representative sites during the peak period for residential land uses (typically weekday evenings after 11:00pm). The peak observation for each site over the two observation periods was selected to calculate the parking demand (see **Table 2**). Parking demand ranged from 0.56 vehicles per unit to 1.25 vehicles per unit with an average parking demand of 0.96 vehicles per unit.



TABLE 2. OBSERVATIONS OF REPRESENTATIVE SITES

Site	Units	Observed Vehicles	Parking Demand (vehicles / unit)
1500 Cumberland Ave	20	25	1.25
2061 Lake Trail Road // 2016 13th Street	33	36	1.09
2077 20th Street	40	30	0.75
1111 Edgett Road	24	21	0.88
1095 Edgett Road	21	22	1.05
1537 Noel Ave	36	34	0.94
1180 Braidwood	26	24	0.92
1580 Piercy Avenue	6	7	1.17
		Average	1.01

4.2 VISITOR PARKING

A study conducted by Metro Vancouver concluded that visitor parking typically has a demand of less than 0.1 vehicles per unit.³ This is corroborated by findings of multiple studies conducted by WATT Consulting Group throughout Vancouver Island, suggesting that visitor parking is not strongly linked to location. With a total of 9 units, and applying a visitor parking demand rate of 0.1, the recommended visitor parking is **1 space** (0.9 spaces, rounded). Section 4.3 addresses on-street parking conditions. Based on the on-street parking assessment, it is anticipated that visitor vehicles will park on-street as there is available supply during the peak time.

4.3 ON-STREET PARKING ASSESSMENT

On-street parking conditions were observed to determine parking availability adjacent to the subject site. Observations were completed on Piercy Avenue, between Cumberland Road and 17th Street; on Tull Avenue, between Piercy Avenue between

³ Metro Vancouver. (2012). The Metro Vancouver Apartment Parking Study, Technical Report. Available online at: http://www.metrovancouver.org/services/regionalplanning/PlanningPublications/Apartment_Parking_Study_TechnicalReport.pdf



Piercy Avenue and 16th Street; and on 16th Street, between Tull Avenue and Piercy Avenue. Observations were conducted during the following periods:

- Tuesday, 13 June 2022, from 11:00pm to 12:00am
- Wednesday, 14 June 2022, from 11:00pm to 12:00am

Utilisation of the on-street parking spaces ranged from 20% to 23% with peak utilisation being observed during the 11:00pm to 11:30pm observation period on 13 June 2022 (see **Table 3**). This indicates that a few residents and/or visitors in the area are utilising the available and unrestricted on-street parking. A total of 110 spaces were vacant within this period indicating that parking is generally available during peak times when residents are expected to be home, and that visitor parking may be accommodated on-street.

TABLE 3. ON-STREET OBSERVATIONS DURING PEAK OCCUPANCY PERIOD

Street	Segment	Side	Available Spaces	Observed	% Occupied
Piercy Avenue	Cumberland Road – Tull Avenue	NE	17	0	0%
		SW	17	0	0%
	Tull Avenue – 16 th Street	NE	20	6	30%
		SW	18	2	11%
	16 th Street – 17 th Street	NE	7	1	14%
		SW	7	2	29%
Tull Avenue	Piercy Avenue – Schjelderup Place	E	3	3	100%
		W	4	4	100%
	Schjelderup Place – 16 th Street	E	2	2	100%
		W	3	3	100%
16th Street	Tull Avenue - Alleyway	NW	17	6	35%
		SE	17	3	18%



Street	Segment	Side	Available Spaces	Observed	% Occupied
	Alleyway – Piercy Avenue	NW	5	0	0%
		SE	5	0	0%
Totals			142	32	23%

4.4 SUMMARY OF EXPECTED PARKING DEMAND

The total expected parking demand for the proposed development is **10 vehicle parking spaces** (9.95 spaces, rounded), nine residential parking spaces (9.05 spaces, rounded), and one visitor parking space (0.9 spaces, rounded). This results in a total expected demand of 10 spaces, which is equal to the proposed supply.

5.0 CONCLUSIONS

The proposed development at 1590 Piercy Avenue is a 9-unit multi-family townhouse development that is providing 10 vehicle parking spaces (1.11 spaces per unit), which is four vehicle parking spaces fewer than the requirement of 14 parking spaces (13.5 rounded) from the City of Courtenay Zoning Bylaw. Furthermore, the proposed development is proposing 9 electrified long-term bicycle parking spaces.

Expected parking demand for this development was estimated based on observational data collected from representative townhouse sites in Courtenay and Comox. The expected parking demand is 10 spaces (nine residential spaces and one visitor space), which is equal to the proposed spaces. Finally, observations of on-street parking in the area indicate that there is sufficient capacity to accommodate visitor parking.

6.0 RECOMMENDATIONS

Based on the conclusions of this study, the proposed parking for the development is supported, with the following optional recommendations for consideration:

1. Increase the number of long-term secured bicycle parking spaces to a rate of 1.5 spaces per unit each with 110V charging outlets.



2. Increase the size of bicycle parking spaces to accommodate cargo bikes and other non-standard bicycles. The spaces should be designed to be a minimum of 3.0 metres in length and 0.9 metres wide. They should also be provided as ground anchored racks.
3. Provide addition to end of trip facilities such as a bicycle maintenance area to further support active transportation.



1590 PIERCY AVENUE

Parking Study

A handwritten signature in black ink, appearing to read "Matthew Lilly".

Author: Matthew Lilly

A handwritten signature in black ink, appearing to read "Tim Shah".

Reviewer: Tim Shah, RPP, MCIP

Prepared for: Build With Perspective Ltd.

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1.0 INTRODUCTION

Watt Consulting Group (WATT) was retained by Build with Perspective Ltd. to conduct a parking study for the proposed townhouse development at 1590 Piercy Avenue in the City of Courtenay, BC. The purpose of this study is to determine the parking demand for the site and identify transportation demand management strategies to help the applicant reduce the expected parking demand.

1.1 SUBJECT SITE

The proposed site is located at 1590 Piercy Avenue in the City of Courtenay. See **Figure 1**.

FIGURE 1. SUBJECT SITE



1.2 SITE CHARACTERISTICS & POLICY CONSIDERATIONS

The following provides information regarding relevant community policies, services and transportation options in proximity to the subject site.



COMMUNITY POLICIES

The City of Courtenay is in the process of updating its Official Community Plan (OCP), which will contain policy direction on a number of topics pertaining to parking including transportation and mobility, land use, and design of the built environment, among others. The City's existing OCP includes a number of goals and policies pertaining to transportation including goal #2, which is stated as follows:

“Development of a transportation system that provides choices for different modes of travel including vehicle, transit, pedestrian, cycling and people with mobility impairments”.

The City OCP contains a number of other policies that relate to sustainable transportation and transportation demand management as outlined in multiple sections including: 4.4.3, 4.6.5, 5.2, and 10.3.

4.4.3. Land Use Designations (Residential Policies):

- Increasing densities can reduce urban sprawl and benefit the environment and transportation system, while promoting healthy community and fiscal responsibility through the provision of services. This may be achieved by creating neighbourhoods that offer a variety of transportation choices.¹

4.6.5. Parks and Open Space (Greenway Strategy):

- The city requires safe, continuous, and convenient pedestrian routes from residences to public walkways, transit, and facilities.²

5.2 Transportation (Goals):

- Integrating land use changes with transportation planning to coordinate changes and increases in traffic patterns.
- Developing transportation systems that provide choices for different modes of travel including vehicle, transit, pedestrian, cycling, and people with mobility impairments.
- Supporting integration of transportation systems that reduce travel distances and congestion.³

¹ City of Courtenay (2016), Official Community Plan, Section 4.4.3: Land Use Designations Available online at: https://www.courtenay.ca/assets/Departments/Development~Services/Bylaw_2387_OCP.pdf.pdf

² Ibid, Section 4.6.5: Parks and Open Space.

³ Ibid, Section 5.2: Transportation.

10.3. Planning for Climate Change (Objectives and Policies):

- Aligning developments within the following transportation mode hierarchy:
 1. Walking
 2. Cycling
 3. Transit
 4. Commercial delivery of goods and services
 5. SOV
- Encouraging and supporting initiatives that reduce the number of SOV trips throughout the community.⁴



SERVICES

The site is located approximately 1.1 kilometres from commercial / retail amenities including restaurants, a grocery store (Thrifty Foods) and cafés. There are also a number of personal and professional services within this distance including medical services, accounting, and wealth management offices. Courtenay Elementary School is 600m from the site (7-minute walk).



TRANSIT

There are two bus stops at the intersection of Cumberland Road and Piercy Avenue that are within a 300m (6-minute walk) of the subject site. The bus stops are served by the 8 (Downtown / Anfield Centre), which provides service to a number of destinations in the community including downtown Courtenay, Driftwood Mall, Anfield Centre, and schools such as Puntledge Park Elementary School and Lake Trail Secondary School.

The site is also located about 900m (11-minute walk) from a bus stop at Fitzgerald Avenue and 16th Street, which is served by the 1 (Comox Mall / Anfield Centre). The 1 operates as the Frequent Transit Network (FTN) route offering 20-minute service at peak hours between the municipalities of Courtenay and Comox. It provides service to a number of commuting destinations including downtown Courtenay, North Island College, North Island Hospital Comox Valley, and downtown Comox, among other destinations.



WALKING

The subject site can be described as somewhat walkable with a walk score of 56, suggesting that some errands can be accomplished on foot.⁵ Sidewalks are located on both sides of Piercy Avenue and serve to connect residents to nearby transit stops on Cumberland Road.

⁴ City of Courtenay (2016), Official Community Plan, Section 10.3: Planning for Climate Change. Available online at: https://www.courtenay.ca/assets/Departments/Development~Services/Bylaw_2387_OCP.pdf.pdf

⁵ More information about the site's walk score is available online at: <https://www.walkscore.com/score/1590-piercy-ave-courtenay-bc-canada>

The recommended pedestrian network plan identified in the City's Transportation Master Plan shows a proposed multi-use pathway on Piercy Avenue between 29th Street and 5th Street.⁶ This specific multi-use pathway is part of the City's long-term pedestrian network and is intended to increase connectivity and comfort for pedestrians, cyclists, and other active travel users when travelling along Piercy Avenue.



CYCLING

Piercy Avenue is currently designated as a 'Signed Bicycle Route'. According to the City's Transportation Master Plan and the Cycling Network Plan, there are no plans to upgrade Piercy Avenue to a better bicycle facility for all ages and abilities. However, the long-term pedestrian plan does identify a multi-use pathway on Piercy Avenue between 29th Street and 5th Street, which could facilitate trips for cyclists.

The Transportation Master Plan identifies a proposed buffered / painted bicycle lane on 17th Street in the medium-term.⁷ See **Figure 2**. This facility would make it easier for residents of the subject site to travel more directly to destinations such as downtown Courtenay and to further destinations such as North Island College using other proposed bike facilities along Fitzgerald Avenue and Old Island Highway. A bike facility is also planned for Piercy Avenue in the long-term; however, the specific facility type is not known at this time.

⁶ City of Courtenay & Urban Systems Ltd. (2019). Connecting Courtenay: Transportation Master Plan. Figure 5-3 Recommended Pedestrian Network Plan, pg. 43, available online at: <https://www.courtenay.ca/assets/City~Hall/Project~Gallery/2018~Master~Transportation~Plan/2019-09-30%20Connecting%20Courtenay%20-%20Transportation%20Master%20Plan%20FINAL.pdf>

⁷ Ibid, Figure 9-3, pg. 77.

1.3 CURRENT LAND USE

The site is currently zoned R-2 (Residential Two Zone), which allows for single residential dwelling duplex, and accessory building uses.

2.0 PROPOSED DEVELOPMENT

2.1 LAND USE

The proposed development is for a nine unit townhouse complex comprising of 6 two-bedroom units and 3 three-bedroom units.

2.2 PARKING SUPPLY

2.2.1 VEHICLE PARKING

A total of 10 parking spaces are proposed—a rate of 1.11 spaces per unit.

2.2.2 BICYCLE PARKING

The development will be providing 30 long-term and 6 short-term bicycle spaces. The long-term bicycle parking ratio results in 3.3 spaces per unit.

3.0 PARKING REQUIREMENT

The City of Courtenay Bylaw No. 2500 (2007) determines the minimum parking supply requirement. Per the Bylaw, the site would be subject to the multi residential dwellings use and be required to provide 1.5 parking spaces per residential unit with 10% of the required spaces being provided and retained for visitor parking. With nine proposed units, this results in a requirement of 14 residential spaces, of which 13 spaces are for residential and 1 as visitor parking. This is four spaces greater than the proposed supply (10 spaces).

4.0 EXPECTED PARKING DEMAND

Expected parking demand for the site is estimated in the following sections to determine if the proposed supply will adequately accommodate demand. Expected parking demand is based on [a] observations of other townhouse sites in Courtenay and Comox [b] research from past parking studies and [c] data from the Institute of Transportation Engineers Parking Generation Manual.

4.1 RESIDENT PARKING DEMAND

Observations were conducted at 8 townhouse sites in the City of Courtenay and Town of Comox, representing a total of 156 units. A breakdown of each site and how it corresponds to the site location can be found in **Table 1**.

Townhouse sites were selected based on two criteria, in the following priority order:

1. Countable Parking Spaces. The sites needed to have parking spaces that were visible and therefore countable. Many townhouse sites in the Comox Valley have enclosed garages or gated underground parking, making counting difficult.
2. Location. Where possible, townhouse sites were selected based on their location outside of downtown Courtenay and Comox. This was completed to ensure that the sites had comparable walkability and access to transportation options as the subject site.

TABLE 1. SUMMARY OF REPRESENTATIVE SITES

Site	Municipality	Units
1755 Willemar Avenue	Courtenay	31
1500 Cumberland Avenue	Courtenay	20
2061 Lake Trail Road // 2061 13 th Street	Courtenay	33
2077 20 th Street	Courtenay	40
1111 Edgett Road	Courtenay	24
1095 Edgett Road	Courtenay	21
1537 Noel Avenue	Comox	36
1180 Braidwood Road	Courtenay	26

4.1.1 OBSERVATIONS

Observations were conducted during the following periods:

- Wednesday, January 29, 2020, from 9:00pm to 11:30pm
- Thursday, January 30, 2020, from 9:00pm to 11:30pm

Observations of parking utilization were conducted at representative sites during the peak period for residential land uses (typically weekday evenings). The peak observation for each site over the two observation periods was selected to calculate the parking demand (see **Table 2**). Parking demand ranged from 0.53 vehicles per unit to 1.35 vehicles per unit with an average parking demand of 0.85 vehicles per unit.

TABLE 2. OBSERVATIONS OF REPRESENTATIVE SITES

Site	Units	Observed Vehicles	Parking Demand (vehicles / unit)
1755 Willemar Ave	31	9	0.75*
1500 Cumberland Ave	20	27	1.35
2061 Lake Trail Road // 2016 13 th Street	33	19	0.58
2077 20 th Street	40	33	0.83
1111 Edgett Road	24	21	0.88
1095 Edgett Road	21	21	1.00
1537 Noel Ave	36	19	0.53
1180 Braidwood	26	19	0.73
Average			0.85

*1855 Willemar Avenue has a total of 31 units, of which 19 have garages and 12 do not. Observations taken at this site reflect the 12 non-garaged units and demand is calculated based on the countable spaces (12).

4.1.2 ADJUSTMENT FACTORS

Observations are a useful method of assessing parking demand rates; however, there are limitations to this method. One of these limitations is that a resident(s) may not be present at the time of observation.

To mitigate this factor, observations were conducted after 9:00pm. However, there is still a chance that some residents may not be home at the time of observation due a multitude of reasons. As such, it can be expected that their vehicle would not be present at the time of observation. This problem was addressed by a study commissioned by Metro Vancouver recommended an adjustment factor of 10% should be applied when parking observations are conducted between after 9:00pm.⁹ This adjustment factor was applied to create an adjusted demand rate of 0.95. See **Table 3**.

⁹ Metro Vancouver. (2012). The Metro Vancouver Apartment Parking Study, Technical Report. Available online at: http://www.metrovancouver.org/services/regional-planning/PlanningPublications/Apartment_Parking_Study_TechnicalReport.pdf

TABLE 3. ADJUSTED PARKING DEMAND AT REPRESENTATIVE SITES

Site	Units	Parking Demand Rate (vehicles / unit)	Adjusted Parking Demand Rate (vehicles / unit)
1755 Willemar Ave	31	0.75	0.80
1500 Cumberland Ave	20	1.35	1.49
2061 Lake Trail Road // 2016 13 th Street	33	0.58	0.64
2077 20 th Street	40	0.83	0.91
1111 Edgett Road	24	0.88	0.97
1095 Edgett Road	21	1.00	1.10
1537 Noel Ave	36	0.53	0.58
1180 Braidwood	26	0.73	0.80
Average		0.85	0.95

4.1.3 INSTITUTE OF TRANSPORTATION ENGINEERS

As stated earlier, the majority of townhouse developments in Courtenay and Comox have garages and vehicle parking demand is therefore difficult to ascertain. Only a few townhouse developments have carports, which enabled the consultant team to conduct observations of sites that were deemed representative of the subject site.

Due to the limited sample, the Institute of Transportation Engineers (ITE) Parking Generation Manual was used to support and validate the findings of the data collection. The manual’s section on low-rise multifamily housing (Land Use 220) includes apartments, townhouses, and condominiums of at least three dwellings with one or two floors of residence; however, it does not include affordable housing developments. ITE reports the average parking demand is 1.21 vehicles per unit across 119 surveyed low-rise multi-family developments, with a lower and upper range of 0.58 vehicles per unit and 2.50 vehicles per unit, respectively.¹⁰

Using the ITE rate of 1.21 vehicles per unit, and the observed parking demand rate of 0.95 vehicles per unit, the two rates were averaged to increase the validity of the data collection findings. This results in a rate of 1.1 vehicles (spaces) per unit, which is the recommended rate for the resident parking demand for the site.

¹⁰ Institute of Transportation Engineers. (2020). ITEParkGen Web-based App, (220 – Multifamily Housing (Low-Rise)). Available online at: <https://iteparkgen.org/ParkGenQuery>

4.2 VISITOR PARKING

Observations of visitor parking were conducted at each of the representative sites and the average rate was 0.09 vehicles per unit. A study by Metro Vancouver concluded that visitor parking typically has a demand of less than 0.1 vehicles per unit.¹¹ Findings from similar studies conducted by WATT in the City of Langford and the City of Victoria support these findings, and suggest that visitor parking is not strongly linked to location.

Based on the available research and observational data, a rate of 0.1 is recommended for the subject site. With nine units and applying a visitor demand rate of 0.1, the recommended visitor parking is 1 space (0.9, rounded).

4.3 SUMMARY OF EXPECTED PARKING DEMAND

Based on adjusted observed parking demand results (0.95 vehicles per unit) as well as the ITE parking generation for low-rise multifamily dwellings (1.21 vehicles per unit), resident parking demand will be approximately 1.1 vehicles per unit. Visitor parking demand was calculated and determined as 0.1 vehicles per unit. This results in a residential requirement of 10 parking spaces and one (1) visitor space. This brings the total to 11 parking spaces, which is one greater than the proposed supply.

5.0 ON-STREET PARKING

On-street parking conditions were observed to determine parking availability around the subject site. Observations were completed on Piercy Avenue between Cumberland Road and 17th Street, and 16th Street. Counts were conducted on the following dates:

- Wednesday, January 29, 2020, at 9:00pm
- Thursday, January 30, 2020, at 9:00pm

These two count times were intended to capture the on-street conditions when local residents would have the highest likelihood of being home and/or when visitors might be visiting the neighbourhood.

Peak utilization was observed on Wednesday January 29, 2020 with 14 parked vehicles observed out of 157 total spaces, an occupancy rate of 9%. This indicates that very few residents and/or visitors in the area are utilizing the available unrestricted on-street parking. A total of 143 spaces were unoccupied, which means that a majority of parking is available during the peak time when residents are expected to be home and/or when visitors may be in the neighbourhood.

¹¹ Metro Vancouver. (2012). The Metro Vancouver Apartment Parking Study, Technical Report. Available online at: http://www.metrovancouver.org/services/regional-planning/PlanningPublications/Apartment_Parking_Study_TechnicalReport.pdf

6.0 TRANSPORTATION DEMAND MANAGEMENT

Transportation Demand Management (TDM) refers to policies, programs, and services that are designed to reduce reliance on single occupancy vehicles (SOVs) and parking demand by encouraging sustainable travel such as active transportation, public transit, carpools, and ride-share services.¹²

As a part of the proposed development, the applicant will be providing 30 long-term and six short-term bicycle spaces to manage vehicle parking demand and support sustainable transportation options at the site. This would align with policy direction in the OCP and TMP that broadly support increasing cycling mode share and providing residents the option of multiple transportation modes. In addition to bicycle parking, it is recommended that the applicant implement the following TDM measures to further justify the reduction in parking supply.

6.1 SHARED ELECTRIC BICYCLE PROGRAM

Electric bicycles (e-bikes) are an emerging transportation mode that provide convenient, zero emission transportation. With Courtenay's proposed Long Term Cycling Network connecting Piercy Avenue to downtown, destination nodes, and local schools, e-bikes would be a highly viable substitute or replacement for motorized vehicles reducing congestion and greenhouse gas emissions.

As an emerging transportation form, there is limited e-bike ownership data available in Courtenay. Six bicycle retailers operating in the Comox Valley were surveyed to assess current interest and sales levels among Courtenay residents. All six retailers reported electric bikes in their inventory and have shown a year-over-year increase in sales with a range of demographics purchasing and/or expressing interest. Some stores such as Black's Cycle and Trails Bicycles reported that e-bikes now represent approximately 25-30% of their total bike sales. These data indicate that e-bikes are growing in popularity in the region.

It is recommended that the applicant provide a shared e-bike program in the proposed development to make cycling a more attractive mode of transportation. The minimum recommended size of the fleet is two e-bikes, which could be purchased locally. As mentioned above, a number of bicycle stores in the Comox Valley sell e-bikes ranging in price with the urban and commuter bikes typically in the range of \$3,000-\$5,000. The operation parameters of the shared e-bike program would need to be determined through direct outreach with future residents and the strata.

With the commitment to 30 long-term bicycle spaces, it is recommended that 10% of these spaces be equipped with 110V outlets to allow e-bike users easy access to battery charging while parked. If used in conjunction with the shared e-bike program outlined above, five 110V outlets would be provided—three for residents and two for the e-bike share program.

¹² Definition based on Transport Canada, TDM for Canadian Communities, March 2011

Lastly, cargo e-bikes are elongated bicycles (~ 2.5m versus ~1.8m) that allow transportation of children, groceries, or cargo. This increased capacity would allow significantly more trips to be accomplished by bicycle or e-bike, especially for young families; however, they require a longer bike rack to account for the additional length. Accounting for this, it is recommended that the bike racks outfitted with the 110V outlet also be designed to accommodate longer cargo e-bikes.

6.2 IMPACTS OF ELECTRIC BICYCLES

E-bikes are still an emerging form of mobility and there is limited research that has quantified the impact they have on vehicle ownership/parking demand; however, it is anticipated that they will have a positive impact on reducing vehicle ownership at this proposed development. A recent survey of North American e-bike owners reported the capacity of e-bikes to replace various modes of transportation commonly used for both recreational and utilitarian trips such as SOVs, public transit, and regular bicycles.

The study found that 62% of e-bike trips replaced car based transportation. Of that 62%, 45.8% were commuting to and from work or school, 44.7% were for entertainment, errands, and cordial visits, and 9.4% were for exercise or recreation. The average distance for each of these trips was about 15 kilometres.¹³ Additionally, other studies have shown that 39 kilometres of car based transportation was displaced by utilization of an e-bike.¹⁴

6.3 TDM SUMMARY

Overall findings in the available research confirms that e-bikes do replace trips that would otherwise use a car or gas powered vehicle. With the provision of a shared e-bike program and 110V outlets supplied to 10% of the long-term bicycle spaces, a 15% reduction in resident parking demand is supported. This would reduce the number of resident vehicle parking spaces by two and bring the total site demand to nine spaces (eight resident and one visitor), which is one space lower than the parking supply (10 spaces).

7.0 CONCLUSIONS

The proposed development at 1590 Piercy Avenue is a nine unit townhouse complex with nine residential and one visitor parking spaces (one parking space per unit). In addition to this the applicant is also proposing to include 30 long-term and six short-term bicycle parking spaces (3.3 long-term spaces per unit).

Parking demand for this development was estimated based on observational data collected from representative townhouse sites in Courtenay and Comox, and was informed from previously conducted studies. To improve the rigor of the analysis, the observation data was also adjusted based on data from the Institute of Transportation Engineers Parking Generation

¹³ MacArthur, J., Harpool, M., & D. Scheppke. (2018). A North American Survey of Electric Bicycle Owners. National Institute for Transportation and Communities, NITC-RR-1041.

¹⁴ Bigazzi, A & E Berjisian. (2019). Electric Bicycles: Can they reduce driving and emissions in Canada. Plan Canada Fall 2019.

Manual. Based on these observations the peak parking demand rate is 11 parking spaces (ten residential and one visitor spaces), one greater than the proposed supply of 10 (nine residential and one visitor spaces). To accommodate this discrepancy TDM measures were recommended with the goal of reducing parking demand by up to 15%.

7.1 RECOMMENDATIONS

The provision of nine resident and one visitor parking spaces is supported if the applicant commits to adopting the following TDM recommendations:

- Provision of a resident shared e-bike program.
- Supplying 10% of long-term bicycle spaces with 110V outlets to enhance e-bike viability for the development.
- Designing 10% of long term bicycle parking spaces to accommodate cargo bicycles.

ATTACHMENT A

Part 66 – Comprehensive Development 39 Zone (CD-39) (1590 Piercy Avenue)

8.66.1 Intent

The CD-39 Zone is intended to accommodate a strata development of three buildings with 9 units on the property legally described as Lot 3, Section 41, Comox District, Plan 4764, Shown Outlined in Red on Plan 1402-R. The property shall be developed substantially in accordance with Schedules A and B which form part of this zone.

8.66.2 Permitted Uses

The following uses are permitted and all other uses are prohibited except as otherwise noted in this bylaw:

- (1) *Dwelling, duplex*
- (2) *Dwelling, townhouse*
- (3) *Accessory buildings and structures*
- (4) *Home occupation*

8.66.3 Minimum Lot Size

A lot shall have an area of not less than 2,030 m².

8.66.4 Floor Area Ratio

Providing all other applicable size, shape and siting conditions are met, the maximum *floor area ratio* shall not exceed 0.40.

8.66.5 Setbacks

Except where otherwise specified in this bylaw the following minimum *building setbacks* shall apply for the principle buildings:

- (1) *Front Yard* 7.5 m
- (2) *Rear Yard* 7.3 m
- (3) *Side Yard* - shall total 4.0 m, with a minimum *side yard setback* on one side of 1.5 m.

8.66.6 Height of Building

Maximum *building height* shall be 7.0 m.

8.66.7 Useable Open Space

Useable open space must be provided on a lot in the amount of 30.0 m² for each one-bedroom *dwelling unit* and 50.0 m² for each *dwelling unit* with two or more bedrooms.

8.66.8 Accessory Buildings and Structures

Shall not be permitted except for a waste and recycling enclosure and exterior bicycle storage areas:

- (1) The side yard setback for the waste and recycling enclosure shall be 0.0 m.

8.66.9 Off-Street Parking and Loading

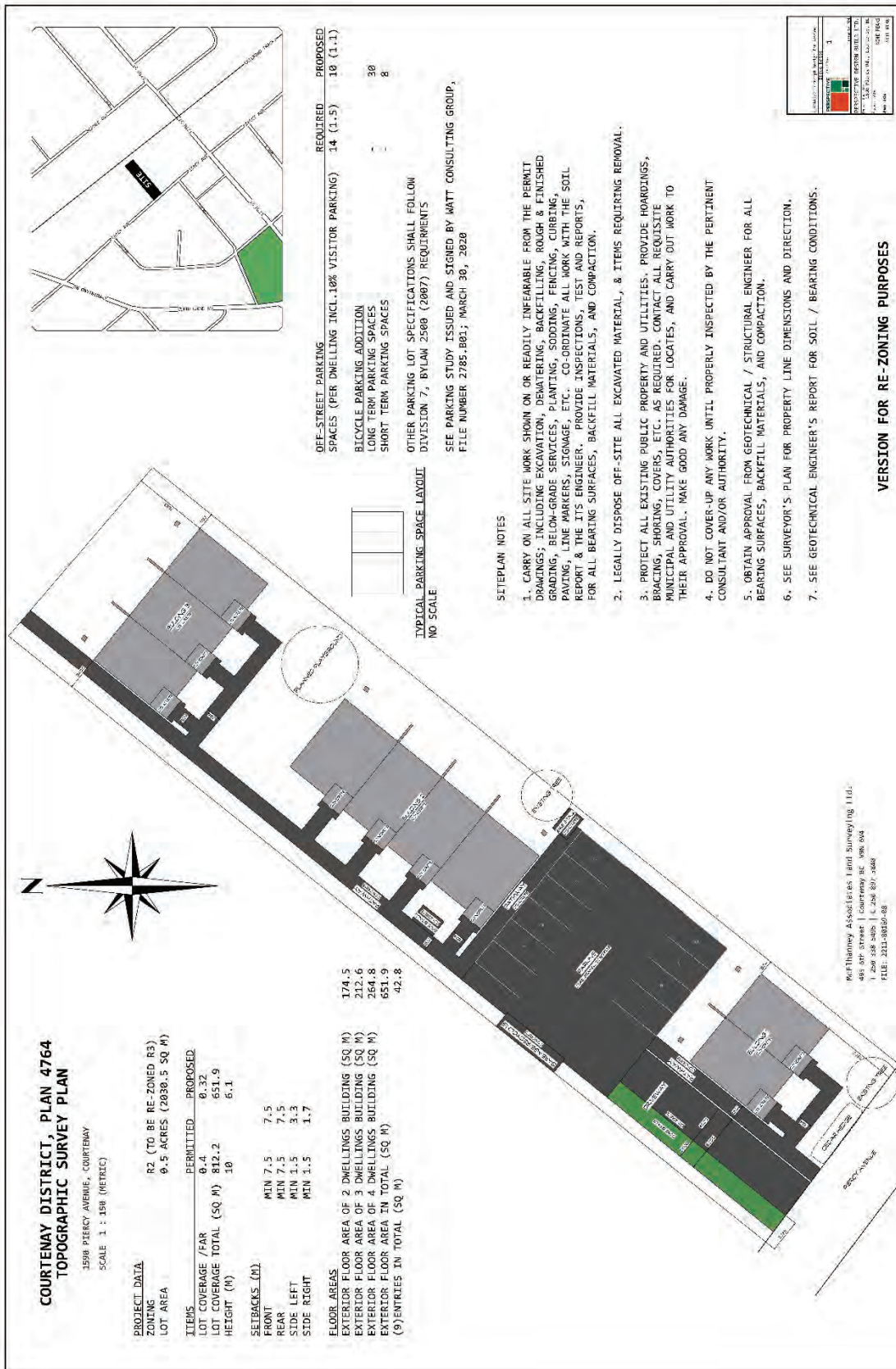
- (1) Off-street parking shall be provided and maintained in accordance with the requirements of Division 7 of this bylaw except in this zone, parking shall be provided at a rate of 1.11 parking spaces per dwelling unit inclusive of visitor parking;
- (2) Bicycle parking shall be provided and maintained in accordance with the requirements of Division 7, Part 3 of this bylaw.

8.66.10 Landscaping and Screening

In addition to the Landscape Requirements in Part 14 of this bylaw, the following landscape requirements shall be met:

- (1) Landscaping shall be used to ensure privacy and to complement the overall development.
- (2) A 4.5 m landscape buffer in the rear and front yard, except for the driveway.
- (3) A 1.5 m landscape buffer for both side yards, except for the driveway and garbage enclosure.

SCHEDULE A



COURTENAY DISTRICT, PLAN 4764 TOPOGRAPHIC SURVEY PLAN

15th PIERCE AVENUE, COURTENAY
SCALE 1 : 125 (METRIC)

PROJECT DATA	PERMITTED	PROPOSED
ZONING	R2 (TO BE RE-ZONED R3)	
LOT AREA	0.5 ACRES (2030.5 SQ M)	
ITEMS		
LOT COVERAGE / FAR	0.4	0.32
LOT COVERAGE TOTAL (SQ M)	812.2	651.9
HEIGHT (M)	10	6.1

SETBACKS (M)	MIN	MAX
FRONT	7.5	7.5
REAR	7.5	7.5
SIDE LEFT	1.5	3.3
SIDE RIGHT	1.5	1.7

FLOOR AREAS	(9) ENTRIES IN TOTAL (SQ M)
EXTERIOR FLOOR AREA OF 2 DWELLINGS BUILDING (SQ M)	174.5
EXTERIOR FLOOR AREA OF 3 DWELLINGS BUILDING (SQ M)	212.6
EXTERIOR FLOOR AREA OF 4 DWELLINGS BUILDING (SQ M)	264.8
EXTERIOR FLOOR AREA IN TOTAL (SQ M)	651.9
(9) ENTRIES IN TOTAL (SQ M)	42.8

REQUIRED	PROPOSED	
OFF-STREET PARKING SPACES (PER DWELLING INCL. 10% VISITOR PARKING)	14 (1-5)	10 (1-1)
BICYCLE PARKING ADDITION (LONG TERM PARKING SPACES)		30
SHORT TERM PARKING SPACES		8

TYPICAL PARKING SPACE LAYOUT
NO SCALE

OTHER PARKING LOT SPECIFICATIONS SHALL FOLLOW DIVISION 7, BYLAW 2500 (2007) REQUIREMENTS
SEE PARKING STUDY ISSUED AND SIGNED BY WATT CONSULTING GROUP, FILE NUMBER 2785.802; MARCH 30, 2020

SITEPLAN NOTES

- CARRY ON ALL SITE SHOWN ON OR READILY INFERRABLE FROM THE PERMIT DRAWINGS; INCLUDING EXCAVATION, DEMATERING, BACKFILLING, ROUGH & FINISHED GRADING, BELOW-GRADE SERVICES, PLANTING, SODDING, FENCING, CURBING, PAVING, LINE MARKERS, SIGNAGE, ETC. CO-ORDINATE ALL WORK WITH THE SOIL REPORT & THE ITS ENGINEER. PROVIDE INSPECTIONS, TEST AND REPORTS, FOR ALL BEARING SURFACES, BACKFILL MATERIALS, AND COMPACTION.
- LEGALLY DISPOSE OFF-SITE ALL EXCAVATED MATERIAL, & ITEMS REQUIRING REMOVAL.
- PROTECT ALL EXISTING PUBLIC PROPERTY AND UTILITIES. PROVIDE HOARDINGS, BRACING, SHORING, COVERS, ETC. AS REQUIRED. CONTACT ALL REQUISITE MUNICIPAL AND UTILITY AUTHORITIES FOR LOCATES, AND CARRY OUT WORK TO THEIR APPROVAL. MAKE GOOD ANY DAMAGE.
- DO NOT COVER-UP ANY WORK UNTIL PROPERLY INSPECTED BY THE PERTINENT CONSULTANT AND/OR AUTHORITY.
- OBTAIN APPROVAL FROM GEOTECHNICAL / STRUCTURAL ENGINEER FOR ALL BEARING SURFACES, BACKFILL MATERIALS, AND COMPACTION.
- SEE SURVEYOR'S PLAN FOR PROPERTY LINE DIMENSIONS AND DIRECTION.
- SEE GEOTECHNICAL ENGINEER'S REPORT FOR SOIL / BEARING CONDITIONS.

Prepared by: Associates Ltd. (Landscape Architect)
1-250-897-8444 | 1-250-897-8444
FILE: 2211-0010-08

PROJECT NO.	2211-0010-08
DATE	2024.04.10
SCALE	1:125
PROJECT NAME	COURTENAY DISTRICT, PLAN 4764
CLIENT	WATT CONSULTING GROUP
DATE PLOTTED	2024.04.10

VERSION FOR RE-ZONING PURPOSES





Rear Elevation- Not to scale



Right Elevation - Not to scale



Left Elevation - Not to scale



Front Elevation - Not to scale

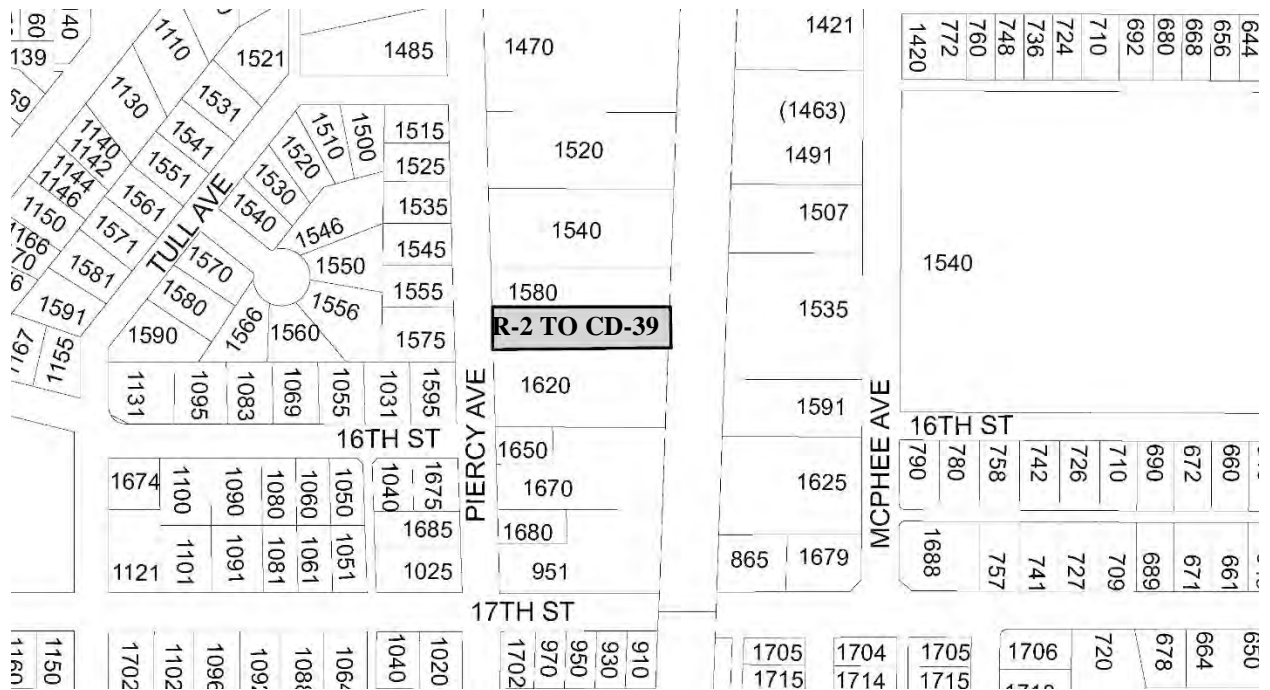
Materials Legend

Wood Tone Fibre Cement Panel	For Timber Framing and Ornamental Wooden Sides
Dark Siding	Glassing
Light Siding	Accent Panel
Paint Grade Fibre Cement Panel	Touch on Roofing Membrane

 PLANNING BOARD 7 11100 10th Ave Everett, WA 98203	 CITY OF EVERETT 1000 1st Ave Everett, WA 98201
--	--

VERSION FOR RE-ZONING PURPOSES

ATTACHMENT B



THE CITY OF COURTENAY
Attachment "B"
 Part of Bylaw No. 3037, 2023
 Amendment to the
 Zoning Bylaw No. 2500, 2007



1160 – 4th Street
Courtenay, BC V9N 1H8
Phone: (778) 647-2266
Email: office@buildwithperspective.com

May 2, 2022

City of Courtenay – City Council

Re: Rezoning Application for 1590 Piercy Avenue (Nine Peaks project)

Dear Council,

Perspective Design Build Ltd. commends Council for their interest in promoting affordable housing within our community and your understanding of the urgency of the matter.

We also share this interest and have created our plans for Nine Peaks with this in mind. By taking property that once only had a small, single-family home on it and in its place adding a multi-family complex with 9 livable units, we are giving people the opportunity to enter homeownership at a more affordable price point than for a single-family home. Our target market is working singles and families or retirees with a median income. The future potential of renting these units out opens the door for those that do not qualify for homeownership.

The units will be offered for sale at a fair sale price. However, we understand the severe need for affordable housing. We realize that this is an ongoing issue with no quick resolution. To that end, Perspective Design Build Ltd. would like to offer the City of Courtenay two options to show our commitment to affordable housing.

Option 1 – A One Time contribution of \$40,000.00 to the Affordable Housing Fund.

This option would allow the funds to be directed where most needed by the Fund. Be it in support of emergency or supportive housing services, the development of new affordable housing projects or used as leverage in negotiations for additional non-market affordable housing units.

Option 2 – A Discount of \$40,000.00 on one of our strata units.

This option could be further discussed and may or may not include covenants whereby potential homeowners would be subject to qualification. Things like income qualification and covenants on title restricting resale price can be sorted out to ensure that this opportunity is held in perpetuity to any future owners of the discounted strata unit.

We look forward to hearing from City Council.

Yours Truly,

George Slomp and Peter Bain
Owners of Perspective Design Build Ltd.



1590 Piercy Avenue



May 28, 2021

To Whom It May Concern:

Re: 1590 Piercy Avenue Rezoning Application (RZ000058)

Perspective Design Build LTD is applying to change the Zoning of 1590 Piercy Avenue, Courtenay, BC V9N 3E9 from R-2 to a Comprehensive Development Zone for the purpose of constructing nine townhouse units. Seven of these townhouses will be two-bedroom units and two will be three-bedroom units. They will be separated into three buildings: a two-unit building facing Piercy Avenue, a four unit building in the heart of the property and a three unit building along the rear.

The lot will be developed to have a driveway, onsite parking, a garbage enclosure, bicycle parking facilities, green space, an amenity area, pathways, and a variety of native and ornamental plants and trees. Two large maple trees on the property will be retained.



View relevant documents on The City of Courtenay website www.courtenay.ca/devapptacker (search by file number or address)

If you have any comments regarding this application, please return your comments to the City of Courtenay by one of the following methods by Jun 11, 2021:

Drop your comment sheet off in the drop box located at the front entrance of the City of Courtenay or

mail: *City of Courtenay, Planning Services Department, 830 Cliffe Avenue, Courtenay BC V9N 2J7*

Email your comments to planning@courtenay.ca Fax your comments to 250-334-4241

Regards,

George Slomp and Peter Bain, Owners

Perspective Design Build Ltd.



Email: george@buildwithperspective.com Phone: (778) 647-2266 or (250) 218-6939

Schedule No. 7

Comments received for Zoning Amendment 1590 Piercy Ave
and Staff Responses

COMMENT

Hello,

I am hoping there is a chance that the garbage bin area for the proposed 1590 Piercy Ave rezoning application (RZ000058) can be moved to the other side of the parking lot. I live at 1580 Piercy (so will be direct neighbors of the new complex) and the garbage bin will be right beside my small patio and house. The housing complex on the other side (1620 Piercy) have their garbage bin on the side against the new build so it would make sense to have them both together. Thank you for considering my request.

Jessica Cote (email 9/1/2021)

RESPONSE

The garbage can't be moved as the suggested location is not accessible for garbage/recycling collection. The garbage enclosure for 1580 Piercy is located along the fenceline that is adjacent to a rear patio and dwelling unit. This is a similar location to what is proposed for this application.

COMMENT

I live at 1620 Piercy so would be the most impacted by this development. Our living room and back patio is 4 ft from the fence line. Our concern is how close the building will be to the fence line? I can't decipher from the sketch plan. If there is no space, we will lose privacy. For the 3 units most affected, developers should be made to put bigger trees as a buffer, not the little ones proposed. We planted 5 big maples on our side of the fence in the front portion, which will benefit their development. I think they should do the same on their side. In general, like the looks of the project. In our strata we have parking for all units plus 4 visitor spots. Will they be parking on the street? Please make our concerns known to council.

Thank you...Doug Vollet (email 6/24/2021)

RESPONSE

The proposed duplex dwelling units will be located between 1.5 m (4.95 ft) and the rear triplex 2.2 m (7.3 ft) from the property lines. These are both side elevations and have limited windows. The units are proposed to be located a similar distance to the unit sitting on the neighbouring property. A large maple is being retained in the rear of the triplex building and a landscape buffer is proposed to be planted around the perimeter of the property.

COMMENT

[REDACTED]
Courtenay, B.C
V9N 3E9
June 7th, 2021

RE: 1590 Piercy Avenue Nine Peaks Development

Dear Sir/Madam,

I am writing with respect to the application to rezone the above listed property (RZ000058). I am a resident of Railside Lane Townhouse Complex at 1620 Piercy Avenue which borders the intended property for redevelopment.

I do support the development of this property, however, after having read through the documents to accompany this application I have a number of concerns.

- 1) Geotech Assessment Document: The date on this document is from June of 2017 with the sampling having been done in April of 2017. This document is 4 years old, and I ask **is there a time limit on when a report as important as this for recommending what may need to be done to prepare for foundations?** Further to this, the report states multiple times that plans for what may be built have yet to be decided. As such they can only make possible recommendations based on what they have found at the test sites. I ask, **does this testing need to be carried out again prior to approval of the proposed 9 units?** Lastly, within this document there is reference to Piercy Avenue as Piercy Creek Road. These are two different locations.
- 2) Landscape Plans Document: The one question that I have with respect to these plans is **how does the plan support the biodiversity within the local area, particularly along the back portion of the property nearest to the rail line?** I can see that there are plans to keep a minimal number of the mature foliage in the area. However, a number of animals use these areas on a regular basis: raccoons, rabbits, deer (on the odd occasion), rats, a number of different bird species (including and not limited to Hummingbirds, Steller's Jay, Pileated Woodpeckers, Robins, Starlings, Great Horned Owls, Barred Owl, Northern Flicker). So I ask again, **how is the landscaping going to support this biodiversity?**
- 3) Parking Study Document: I can see that this document is taking into consideration use of the space for vehicle parking as well as alternative conventional bike and electric bike storage. As we push forwards into the future to go more green, the 30 bike storage units is definitely a great way forward. However, I can also see that the current Courtenay bylaw states that there should be a minimum of 1.5 parking spaces per unit and should include visitor parking. Present plans of 9 resident parking spaces and one visitor parking space does not meet this requirement. The study conducted on January 29th and 30th, 2021 at 9pm on 8 townhouse sites within the City of Courtenay and Comox has several flaws. 1) This data was collected during a pandemic in which residents may have been impacted economically and as such have had to reduce their household vehicle numbers. 2) The data was collected during a particularly cold and rainy part of the year and in the evening in which residents and visitors alike may not be actively out. 3) Statistically, data should be collected over multiple time periods, under multiple weather conditions in order to be valid. 4) **Why were the adjacent units/complexes on Piercy Avenue not included in this study? Would it not make more sense to see how those individuals already residing in the area use the road and complexes?** 5) The study does not include information about a Family Day Care that resides almost directly across the road from the proposed development. **If there are more residents having to park on the road then how are**

Parents/Guardians going to find safe spaces to drop off and pick up their children? 6) Of the sites that were observed, **what was the socio-economic status of these properties? How many belong to individuals that are retired? Single parent families?** 7) Surely, properties with garages should also be included in the report. I understand that the proposed re-development does not include garages for the residents, however, this still supports the need for parking. 8) The observation of On-Street Parking is also flawed. It states that there is a possible 157 parking spaces available. However, there is not mention of the end of Piercy Road closest to Cumberland Road being primarily commercial property and a Licensed Day Care Centre. Again, safe access to drop off and pick up children is vital and if more road parking is used this becomes a concern. 9) Another proposed development is in the works by Habitat for Humanity at the end of Piercy Road near Cumberland Road. **How much on-street parking will result from this development?** Again, this has an impact on the 157 'available spaces'. 10) Even though there is an increase in the Comox Valley for demand on E-Bikes, **how many of those individuals within the age bracket that will either rent or reside in the proposed properties? How many e-bikes were observed on January 29th and 30th? Where do the individuals who are currently purchasing these products residing within the Valley?** Just because the bike shops are seeing an increase within the Comox Valley does not mean that individuals who choose to reside locally will be using them instead of a car. And let's be honest, my husband and I have conventional bikes and are in our mid-forties and have not used our bikes in over two years for a variety of reasons. 11) The proposed development is for multi-family use. The majority of families in today's current climate have a minimum of 2 vehicles. This is in part due to the fact that both parents have to work in order to support the family. Even with individuals wanting to be more 'green' they tend to choose a hybrid or electric vehicle over an electric bike.

- 4) Plans and Elevation Duplex Document: The parking plan document references 6 short term bike spaces, however, the plans and elevation duplex document references 8. **Could someone please clarify which one it is?** This information is repeated on the Plans and Elevation Triplex Document.
- 5) Plans and Elevation Complex Document: **Is there information missing from this?** I cannot see the detailed drawings similar to those for the Duplex and Triplex portion. I am assuming that Units 3 and 6 will be the 3-bedroom, but as there is no drawing to show this, it is unclear. It is unclear where the placement of windows along the back side are and how this may affect the properties possibly being overlooked located in the Railside Lane Townhouse Complex.
- 6) Project Summary Document: This only refers to 30 bike spaces and references 10% having e-bike accommodation. This needs to be clarified as the previous documentation states different information.
- 7) 9 Units: I also have concerns as to the number of properties that are being proposed to occupy this space. The Railside Lane Townhouse Complex is twice the size of the proposed 1590 property and consists of 10 units. The Habitat for Humanity Complex which borders the proposed 1590 property on the other side is a similar size and only contains 6 units. The front and back proposed properties do not appear to have windows that would be facing the other properties. However, the middle complex of four properties do have windows that face into the Railside Lane Complex. Further, to the above concern about the need for more parking the middle complex should be reduced in size to accommodate it and possibly the direction in which it faces should be changed to ensure that the windows do not overlook other properties privacy.

I look forward to your response to the above. Please feel free to contact me by email at

Kind Regards

Christina Kurshumliu

Resident of Railside Lane Townhouse Complex and Strata Treasurer - letter in email 6/7/2021

RESPONSE

1. Geotech Assessment Document
There are no steep slopes on this property. The geotechnical engineer on record has confirmed the site is safe for the intended use.
The building department may require a revised geotechnical report at the time of building permit application if that is deemed necessary.
2. Landscape Plan and biodiversity
There are no environmentally sensitive areas identified on DP Guidelines Map 5, Terrestrial Environmentally Sensitive Areas. A 4.5 m wide landscape buffer will be planted in the rear and lesser buffers around the perimeter of the proposed development.
3. Parking study
This comment resulted in the applicant providing an updated parking study. The two studies are attached to the staff report. The consultants concluded that the proposed number of on-site parking stalls is suitable for the proposed use and that there is adequate on street parking for the residential use.
The specific questions asked about purchasers of EV bicycles are beyond the scope for evaluating the suitability of the proposed use. This is a requirement in the Zoning Bylaw.
4. Elevation drawings
Revised elevation drawings were provided by the applicant. Windows are proposed on the rear elevation on the first and second floors. Overlooking onto adjacent properties is avoidable with infill and more dense developments. Landscaping will provide some screening.
The landscape plan shows and the applicant has confirmed that the required number of bicycle parking stalls, both class I and II, are being provided.
5. Numbers of units
The proposed density is a .4 floor area ratio and is a consistent density to the existing R-3 zone and other multi-unit developments.

COMMENT


I've received an information sheet regarding the 'Nine Peaks' proposed development and have the following concerns:

1. **Traffic** - This development, along with the one at 1435 Piercy, will add significant traffic along Piercy. The intersection at 17th St. and Piercy is currently a problem and there needs to be a traffic light installed to facilitate vehicles turning off of Piercy onto 17th.
I have raised this before but I have been ignored and it is an accident waiting to happen. Given that Council has approved bike Lanes along 17th, it would make sense to rectify the situation at the same time. As well, the intersection at Piercy and Cumberland will be affected and the traffic signals should be adjusted.

2. **Consultation area** - As indicated previously, the 100 meter community consultation perimeter is inadequate. In this case, it will not even include both intersections and the impact of the development will even go beyond this.
3. **Business plan** - The City does not require a copy of the business plan (the banks/funders do). There is no indication as to the proposed sale or rental revenue anticipated and how affordable or unaffordable these units will be. Will it alleviate any of the local housing pressure?; or is it a money grab to attract new residents from areas such as the Lower Mainland.
4. **Infrastructure** - *We are already impacted by water restrictions several times a year. How will this development exacerbate this? As well, several trees will require removal and how will the city demand compliance with the recent policies?*

Sincerely,

Fred Muzin (email 6/7/2021)


Courtenay V9N 1X8

RESPONSE

1. Traffic light – a traffic light has been determined to not be warranted in
The City's Transportation Plan (2019) does not identify this intersection as one where a high number of collisions take place as a result this intersection is not identified for future improvements for vehicular traffic. Nor does the plan does not propose pedestrian or cycling related improvements for this intersection.
Piercy itself is a recognized cycling route in the City's Cycling Network Plan. Cycling improvements at the intersection of Piercy and 17th are budgeted in the 2023 work plan.
2. Consultation area – the 100 m notification area is established in the Development Application Procedure Bylaw No. 2790. This bylaw is under review as part of the Bill 26 review and this includes notice distances.
3. Business Plan – the City does not require a business plan as part of an application. This matter is not part of evaluating the suitability of the proposed land use.
4. Infrastructure – Residents in this development will also be subject to water restrictions. The arborist report notes that trees located in the perimeter are cottonwood or red-alder and are not suitable for the landscape plan. And other trees have structural characteristics which preclude them from maturing into healthy, defect free trees.
The landscape plan shows protection of two mature maple trees and tree replacement for those that are being removed (32 new trees).

THE CORPORATION OF THE CITY OF COURTENAY

BYLAW NO. 3037

A bylaw to amend Zoning Bylaw No. 2500, 2007

The Council of the Corporation of the City of Courtenay in open meeting assembled enacts as follows:

1. This bylaw may be cited for all purposes as **“Zoning Amendment Bylaw No. 3037, 2023”**.

2. That “Zoning Bylaw No. 2500, 2007” be hereby amended as follows:

(a) Amending Division 8 – Classification of Zones through the addition of:

Part 66 – Comprehensive Development 39 Zone (CD-39) 1590 Piercy Avenue as attached in **Attachment A**.

(b) By rezoning That part of Lot 3, Section 41, Comox District, Plan 4764, Shown Outlined in Red on Plan 1402-R (1590 Piercy Avenue) as shown in bold outlined on **Attachment B** which is attached hereto and forms part of this bylaw, from Residential 2 to Comprehensive Development Zone 39 (CD-39).

(c) That Schedule No. 8. Zoning Map be amended accordingly.

3. This bylaw shall come into effect upon final adoption hereof.

Read a first time this 14th day of June , 2023

Read a second time this 14th day of June , 2023

Considered at a Public Hearing this 19th day of July , 2023

Read a third time this day of , 2023

Finally passed and adopted this day of , 2023

Mayor

Director of Legislative Services

Approved under S.52(3)(a) of the *Transportation Act*

Tallina McRae, Development Services Officer
Ministry of Transportation and Infrastructure
Vancouver Island District

ATTACHMENT A

Part 66 – Comprehensive Development 39 Zone (CD-39) (1590 Piercy Avenue)

8.66.1 Intent

The CD-39 Zone is intended to accommodate a strata development of three buildings with 9 units on the property legally described as Lot 3, Section 41, Comox District, Plan 4764, Shown Outlined in Red on Plan 1402-R. The property shall be developed substantially in accordance with Schedules A and B which form part of this zone.

8.66.2 Permitted Uses

The following uses are permitted and all other uses are prohibited except as otherwise noted in this bylaw:

- (1) *Dwelling, duplex*
- (2) *Dwelling, townhouse*
- (3) *Accessory buildings and structures*
- (4) *Home occupation*

8.66.3 Minimum Lot Size

A lot shall have an area of not less than 2,030 m².

8.66.4 Floor Area Ratio

Providing all other applicable size, shape and siting conditions are met, the maximum *floor area ratio* shall not exceed 0.40.

8.66.5 Setbacks

Except where otherwise specified in this bylaw the following minimum *building setbacks* shall apply for the principle buildings:

- (1) *Front Yard* 7.5 m
- (2) *Rear Yard* 7.3 m
- (3) *Side Yard* - shall total 4.0 m, with a minimum *side yard setback* on one side of 1.5 m.

8.66.6 Height of Building

Maximum *building height* shall be 7.0 m.

8.66.7 Useable Open Space

Useable open space must be provided on a lot in the amount of 30.0 m² for each one-bedroom *dwelling unit* and 50.0 m² for each *dwelling unit* with two or more bedrooms.

8.66.8 Accessory Buildings and Structures

Shall not be permitted except for a waste and recycling enclosure and exterior bicycle storage areas:

- (1) The side yard setback for the waste and recycling enclosure shall be 0.0 m.

8.66.9 Off-Street Parking and Loading

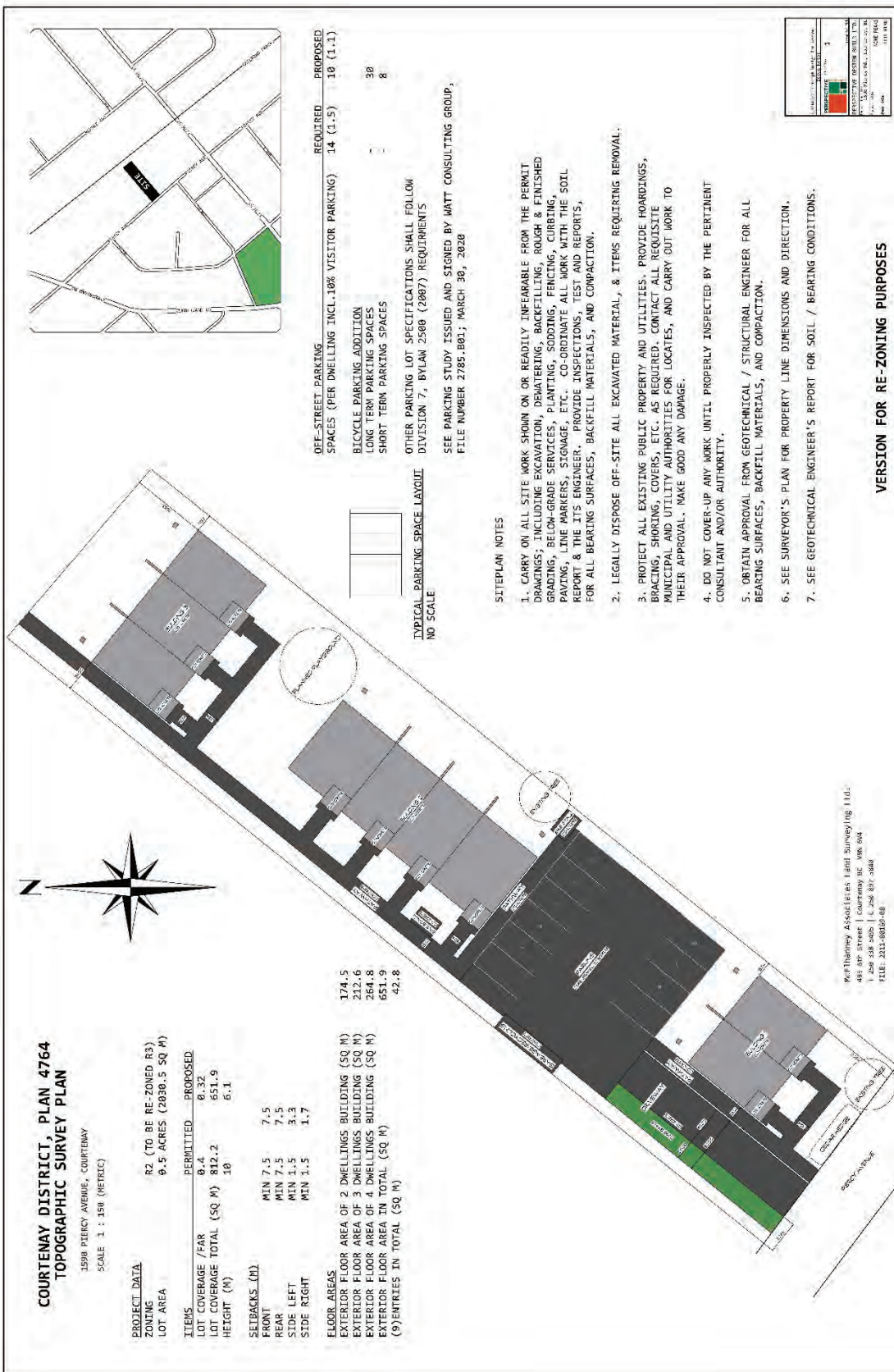
- (1) Off-street parking shall be provided and maintained in accordance with the requirements of Division 7 of this bylaw except in this zone, parking shall be provided at a rate of 1.11 parking spaces per dwelling unit inclusive of visitor parking;
- (2) Bicycle parking shall be provided and maintained in accordance with the requirements of Division 7, Part 3 of this bylaw.

8.66.10 Landscaping and Screening

In addition to the Landscape Requirements in Part 14 of this bylaw, the following landscape requirements shall be met:

- (1) Landscaping shall be used to ensure privacy and to complement the overall development.
- (2) A 4.5 m landscape buffer in the rear and front yard, except for the driveway.
- (3) A 1.5 m landscape buffer for both side yards, except for the driveway and garbage enclosure.

SCHEDULE A



COURTENAY DISTRICT, PLAN 4764 TOPOGRAPHIC SURVEY PLAN

1550 PIERCE AVENUE, COURTENAY
SCALE 1 : 150 (METRIC)

PROJECT DATA	PERMITTED	PROPOSED
ZONING	R2 (TO BE RE-ZONED R3)	
LOT AREA	0.5 ACRES (2030.5 SQ M)	
ITEMS		
LOT COVERAGE / FAR	0.4	0.32
LOT COVERAGE TOTAL (SQ M)	812.2	651.9
HEIGHT (M)	10	6.1

SETBACKS (M)	MIN	MAX
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REAR	7.5	7.5
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EXTERIOR FLOOR AREA OF 3 DWELLINGS BUILDING (SQ M)	212.6
EXTERIOR FLOOR AREA OF 4 DWELLINGS BUILDING (SQ M)	264.8
EXTERIOR FLOOR AREA IN TOTAL (SQ M)	651.9
(9)ENTRIES IN TOTAL (SQ M)	42.8

OFF-STREET PARKING SPACES (PER DWELLING INCL. 10% VISITOR PARKING) 14 (1-5) 10 (1-1)

BICYCLE PARKING ADDITION LONG TERM PARKING SPACES 30
SHORT TERM PARKING SPACES 8

OTHER PARKING LOT SPECIFICATIONS SHALL FOLLOW DIVISION 7, BYLAW 2500 (2007) REQUIREMENTS

SEE PARKING STUDY ISSUED AND SIGNED BY WATT CONSULTING GROUP, FILE NUMBER 2785.802; MARCH 30, 2020

TYPICAL PARKING SPACE LAYOUT
NO SCALE

SITEPLAN NOTES

- CARRY ON ALL SITE SHOWN ON OR READILY INFERRABLE FROM THE PERMIT DRAWINGS; INCLUDING EXCAVATION, DEMATERING, BACKFILLING, ROUGH & FINISHED GRADING, BELOW-GRADE SERVICES, PLANTING, SODDING, FENCING, CURBING, PAVING, LINE MARKERS, SIGNAGE, ETC. CO-ORDINATE ALL WORK WITH THE SOIL REPORT & THE ITS ENGINEER. PROVIDE INSPECTIONS, TEST AND REPORTS, FOR ALL BEARING SURFACES, BACKFILL MATERIALS, AND COMPACTION.
- LEGALLY DISPOSE OFF-SITE ALL EXCAVATED MATERIAL, & ITEMS REQUIRING REMOVAL.
- PROTECT ALL EXISTING PUBLIC PROPERTY AND UTILITIES. PROVIDE HOARDINGS, BRACING, SHORING, COVERS, ETC. AS REQUIRED. CONTACT ALL REQUISITE MUNICIPAL AND UTILITY AUTHORITIES FOR LOCATES, AND CARRY OUT WORK TO THEIR APPROVAL. MAKE GOOD ANY DAMAGE.
- DO NOT COVER-UP ANY WORK UNTIL PROPERLY INSPECTED BY THE PERTINENT CONSULTANT AND/OR AUTHORITY.
- OBTAIN APPROVAL FROM GEOTECHNICAL / STRUCTURAL ENGINEER FOR ALL BEARING SURFACES, BACKFILL MATERIALS, AND COMPACTION.
- SEE SURVEYOR'S PLAN FOR PROPERTY LINE DIMENSIONS AND DIRECTION.
- SEE GEOTECHNICAL ENGINEER'S REPORT FOR SOIL / BEARING CONDITIONS.



VERSION FOR RE-ZONING PURPOSES

PreThamesy Associates Ltd. Surveying (reg. 116)
400-250-5555 | 1-250-897-5668
FILE: 2211-0010-08





Rear Elevation- Not to scale



Right Elevation - Not to scale



Left Elevation - Not to scale



Front Elevation - Not to scale

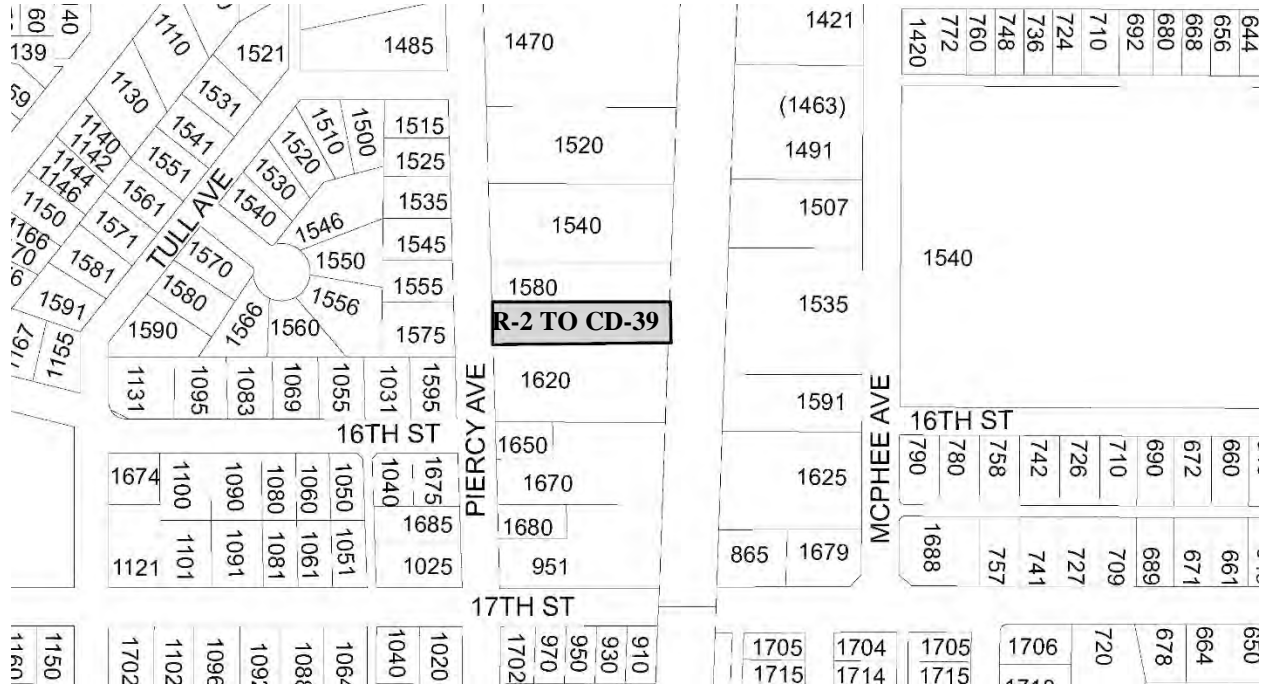
Materials Legend

Wood Tone Fibre Cement Panel	For Timber Framing and Ornamental Wooden Sides
Dark Siding	Glassing
Medium Siding	Accent Panels
Light Gray Fibre Cement Panel	Touch on Roofing Membrane

 MARRIOTT ARCHITECTURE INC. 7000 EAST ASHLEY AVENUE, SUITE 100 DENVER, CO 80231 TEL: 303.733.3300 WWW.MARRIOTTARCHITECTURE.COM	7 11/18/2018
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VERSION FOR RE-ZONING PURPOSES

ATTACHMENT B

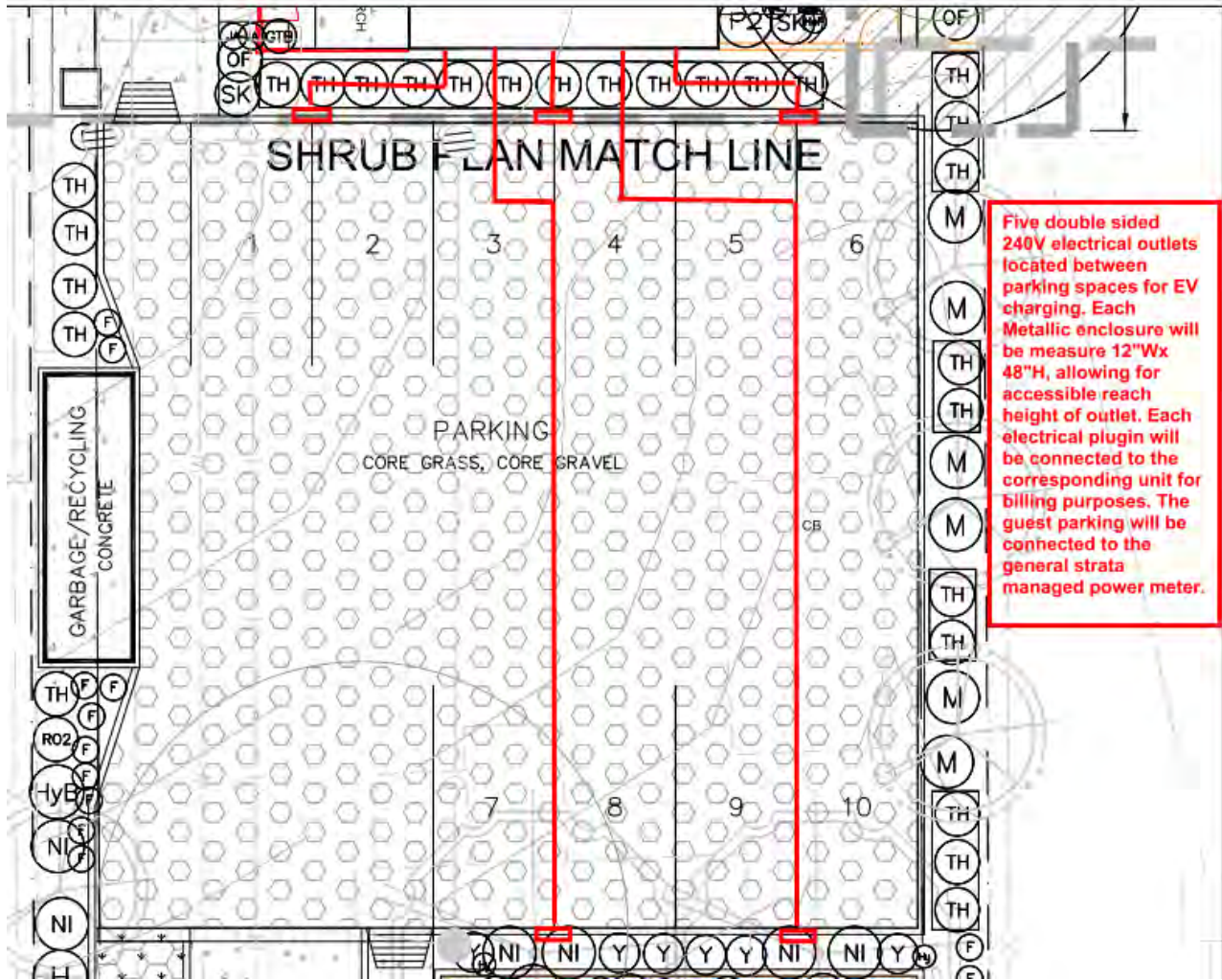


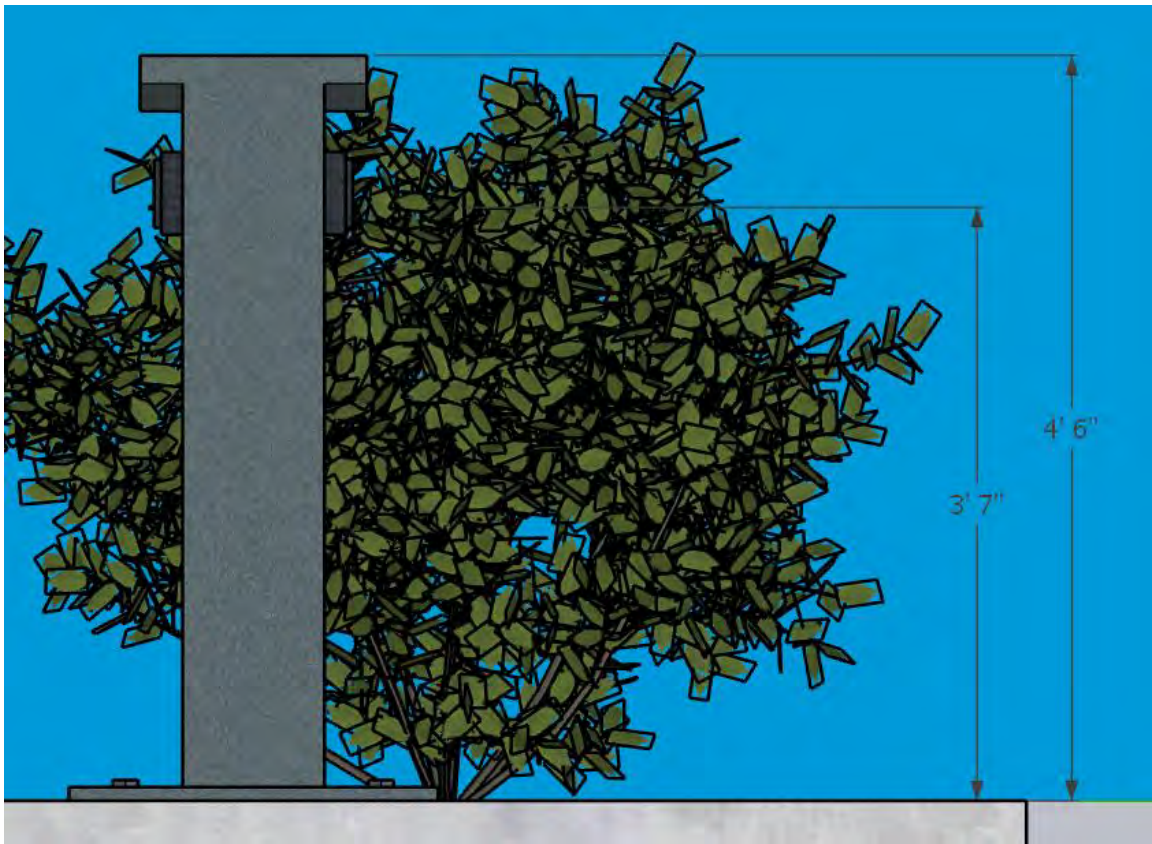
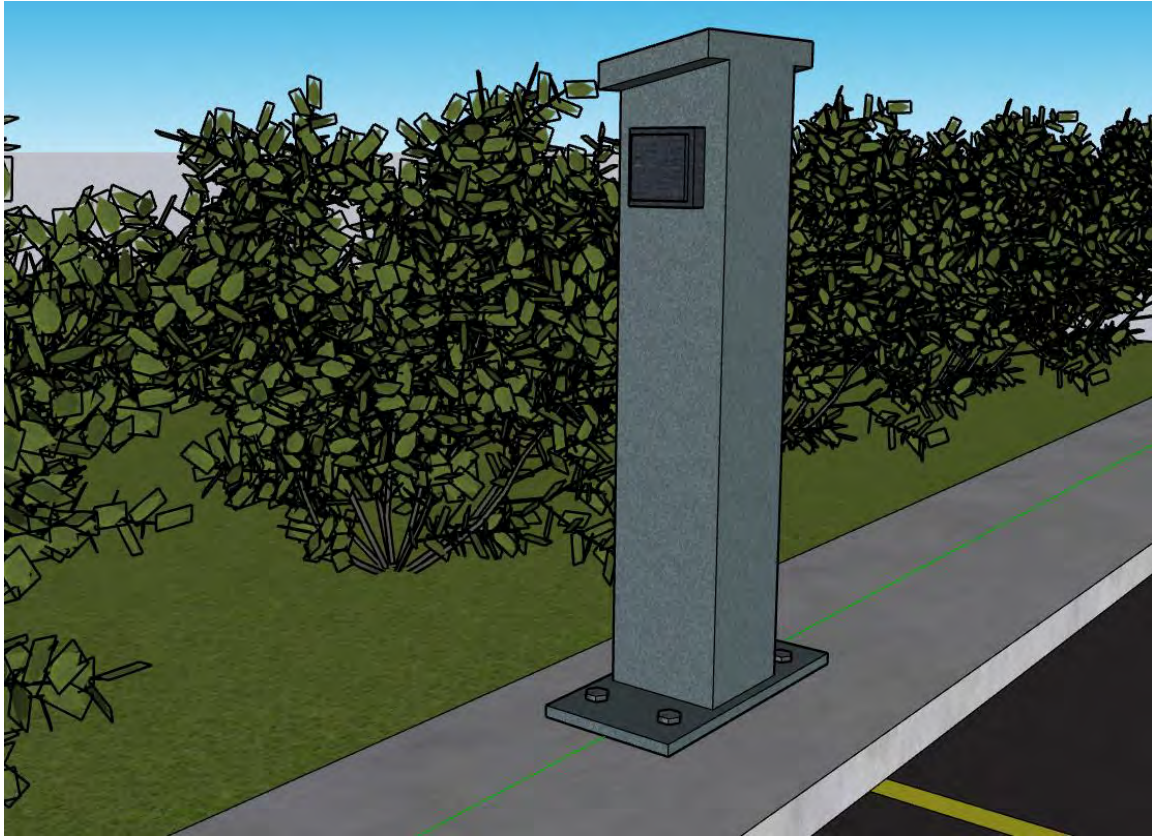
THE CITY OF COURTENAY
Attachment "B"
 Part of Bylaw No. 3037, 2023
 Amendment to the
 Zoning Bylaw No. 2500, 2007

PROPOSED EV Charging updates 1590 Piercy Ave

EV Charging will be available for all the 9 independently owned strata units. Each of the 5 charging Towers will be located between two parking stalls. Each tower will have two 240 volt plug in units, allowing independent charging connected to the corresponding housing unit.

Charging cables will be supplied by the car owner to ensure the correct charging cable is available. An additional charging station will be available for the guest parking stall. This unit will be connected to the strata managed power meter.





From: [REDACTED]
To: [PlanningAlias](#)
Subject: RZ000058 - 1590 Piercy Avenue - Comment for Public Hearing
Date: Friday, July 7, 2023 8:59:48 AM

Hello Planning Department,

I would like to convey my support for the rezoning application RZ000058 for 1590 Piercy Avenue.

The proposal appears to be consistent with the Neighbourhood Centre land use designation of the OCP and seems like an all-around good fit with the existing multi-family neighbourhood, close access to transit, bike lanes, parks, and downtown.

I appreciate the parking area being located behind the duplex building, the landscaping and open space, the permeable paving that is proposed, and the design of the ground-oriented buildings. You can see much thought went into this proposal.

Regards,

[REDACTED]
1944 Riverside Ln, Courtenay

THE CORPORATION OF THE CITY OF COURTENAY

BYLAW NO. 3037

A bylaw to amend Zoning Bylaw No. 2500, 2007

The Council of the Corporation of the City of Courtenay in open meeting assembled enacts as follows:

1. This bylaw may be cited for all purposes as “**Zoning Amendment Bylaw No. 3037, 2023**”.

2. That “Zoning Bylaw No. 2500, 2007” be hereby amended as follows:

(a) Amending Division 8 – Classification of Zones through the addition of:

Part 66 – Comprehensive Development 39 Zone (CD-39) 1590 Piercy Avenue as attached in **Attachment A**.

(b) By rezoning That part of Lot 3, Section 41, Comox District, Plan 4764, Shown Outlined in Red on Plan 1402-R (1590 Piercy Avenue) as shown in bold outlined on **Attachment B** which is attached hereto and forms part of this bylaw, from Residential 2 to Comprehensive Development Zone 39 (CD-39).

(c) That Schedule No. 8. Zoning Map be amended accordingly.

3. This bylaw shall come into effect upon final adoption hereof.

Read a first time this 14th day of June, 2023

Read a second time this 14th day of June, 2023

Considered at a Public Hearing this 19th day of July, 2023

Read a third time this 26th day of July, 2023

Finally passed and adopted this 12th day of June, 2024

Mayor Bob Wells

Lisa Bourgeois, Deputy Corporate Officer

Approved under S.52 (3) (a) of the *Transportation Act*



Tallina McRae, Development Services Officer
Ministry of Transportation and Infrastructure
Vancouver Island District

ATTACHMENT A

Part 66 – Comprehensive Development 39 Zone (CD-39) (1590 Piercy Avenue)

8.66.1 Intent

The CD-39 Zone is intended to accommodate a strata development of three buildings with 9 units on the property legally described as Lot 3, Section 41, Comox District, Plan 4764, Shown Outlined in Red on Plan 1402-R. The property shall be developed substantially in accordance with Schedules A and B which form part of this zone.

8.66.2 Permitted Uses

The following uses are permitted and all other uses are prohibited except as otherwise noted in this bylaw:

- (1) *Dwelling, duplex*
- (2) *Dwelling, townhouse*
- (3) *Accessory buildings and structures*
- (4) *Home occupation*

8.66.3 Minimum Lot Size

A lot shall have an area of not less than 2,030 m².

8.66.4 Floor Area Ratio

Providing all other applicable size, shape and siting conditions are met, the maximum *floor area ratio* shall not exceed 0.40.

8.66.5 Setbacks

Except where otherwise specified in this bylaw the following minimum *building setbacks* shall apply for the principle buildings:

- (1) *Front Yard* 7.5 m
- (2) *Rear Yard* 7.3 m
- (3) *Side Yard* - shall total 4.0 m, with a minimum *side yard setback* on one side of 1.5 m.

8.66.6 Height of Building

Maximum *building height* shall be 7.0 m.

8.66.7 Useable Open Space

Useable open space must be provided on a lot in the amount of 30.0 m² for each one-bedroom *dwelling unit* and 50.0 m² for each *dwelling unit* with two or more bedrooms.

8.66.8 Accessory Buildings and Structures

Shall not be permitted except for a waste and recycling enclosure and exterior bicycle storage areas:

- (1) The side yard setback for the waste and recycling enclosure shall be 0.0 m.

8.66.9 Off-Street Parking and Loading

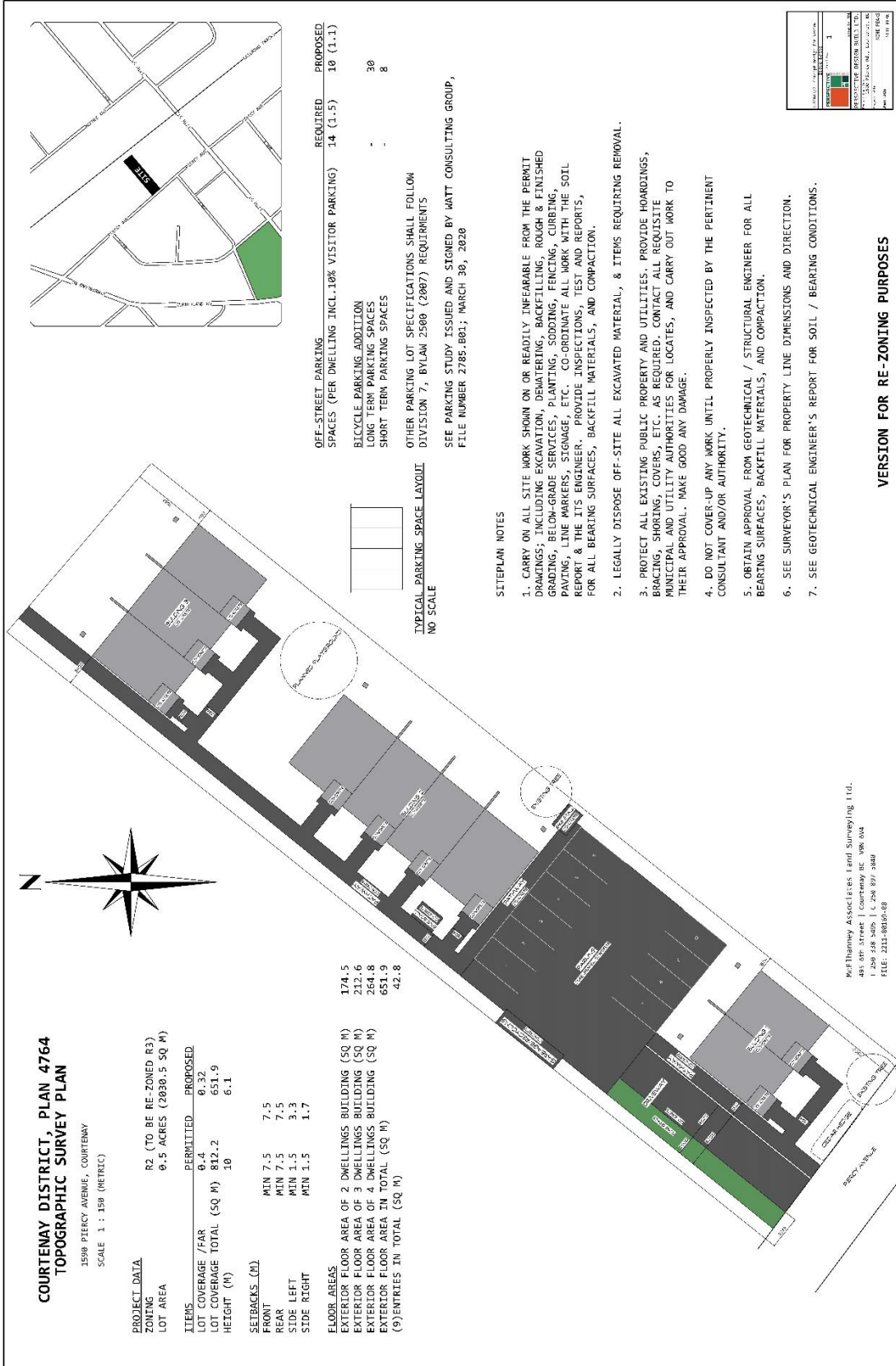
- (1) Off-street parking shall be provided and maintained in accordance with the requirements of Division 7 of this bylaw except in this zone, parking shall be provided at a rate of 1.11 parking spaces per dwelling unit inclusive of visitor parking;
- (2) Bicycle parking shall be provided and maintained in accordance with the requirements of Division 7, Part 3 of this bylaw.

8.66.10 Landscaping and Screening

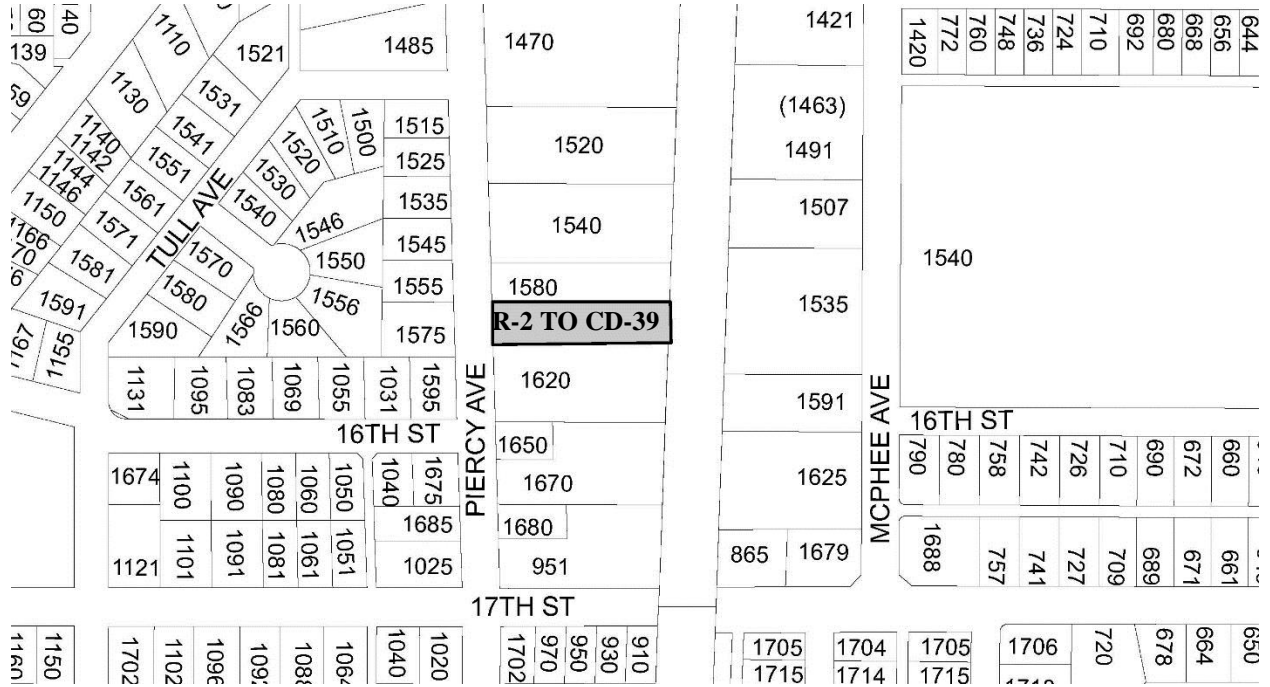
In addition to the Landscape Requirements in Part 14 of this bylaw, the following landscape requirements shall be met:

- (1) Landscaping shall be used to ensure privacy and to complement the overall development.
- (2) A 4.5 m landscape buffer in the rear and front yard, except for the driveway.
- (3) A 1.5 m landscape buffer for both side yards, except for the driveway and garbage enclosure.

SCHEDULE A



ATTACHMENT B



THE CITY OF COURTENAY
Attachment "B"
 Part of Bylaw No. 3037, 2023
 Amendment to the
 Zoning Bylaw No. 2500, 2007



The Corporation of the City of Courtenay

Staff Report

To: Council

File No.: 3360-20-2402/RZ000081

From: Development Services

Date: June 12, 2024

Subject: Adoption of Zoning Amendment Bylaw No. 3135, 2024 (small-scale, multi-unit housing)

PURPOSE:

For Council to consider adoption of Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing).

BACKGROUND:

At the Regular Council meeting on May 22, 2024 Council received a staff report (attachment 2) that provided modifications to Zoning Amendment Bylaw (small scale multi-housing No. 3135, 2024 (small-scale multi-unit housing), and second reading and third reading of Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing) as modified. Council passed the resolutions and gave second and third readings to Zoning Amendment (small scale multi-unit housing) Bylaw No. 3135 as modified.

DISCUSSION:

The City of Courtenay Zoning Bylaw No. 2500, 2007 contains sixteen (16) residential zones that meet the provincial definition of “Restricted Zone, being: R-1, R-1B, R-1C, R-1D, R-1E, R-1S, R-2, R-2A, R-2B, RR-1, RR-2, RR-2S, RR-3, RR-4, RR-5, and R-RU zones. A “Restricted Zone” is a residential zone that limits the permitted uses to single residential dwellings and/or duplexes, with or without a second accessory dwelling unit.

The City must comply with Bill 44 and subsequent changes to the *Local Government Act*, which requires that properties that qualify as “Restricted Zones” and have a lot size of under 4050m² be rezoned to permit three (3) dwelling units on lots under 280m² and four (4) dwelling units for lots over 280m² by June 30, 2024. Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing) is attached to this report as Attachment 1, and addresses the provincial regulations for Bill 44. Bill 47 Transit Oriented Development Areas (TOD) does not apply to the City of Courtenay and as such the proposed Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing) does not permit six (6) dwelling units per lot.

There are 5649 properties that have been identified to be rezoned to R-SSMUH. Figure 1 below identifies the distribution of parcel size ranges that will be rezoned to R-SSMUH by number of properties. This represents approximately 60% of our properties and 22% of the City area. Many of the larger lots could potentially be subdivided into two or even three lots with the 300 m² minimum lot size being proposed in the new R-SSMUH zone.

POLICY ANALYSIS:

The proposed Bylaw Amendments respond to Bill 44 provincial legislation and timelines for adoption, thereby aligning Courtenay bylaws with the new requirements.

In order to fully support the Small-Scale Multi-Unit Housing initiative, other City Bylaws may need to be amended, and staff will bring those amendments forward at the earliest opportunity for the consideration of Council.

FINANCIAL IMPLICATIONS:

The Province has provided funding to each local government to support planning and capacity to meet the new provincial housing legislative requirements. The City of Courtenay received \$286,000 from this fund earlier this year.

ADMINISTRATIVE IMPLICATIONS:

The mandatory timelines imposed by the province are challenging and will require priority to be assigned to this project at the expense of other regular duties. Consultant services will also be required.

STRATEGIC PRIORITIES REFERENCE:

This initiative addresses the following strategic priorities:

- Buildings and Landscape - Update Zoning Bylaw - review maximum building heights
- Buildings and Landscape - Review and update land use regulations and bylaws for consistency with OCP

PUBLIC ENGAGEMENT:

Bill 44 amendments to the *Local Government Act* prohibits a Council from holding a public hearing for zoning bylaw amendments that are for the purpose of implementing the Small-Scale Multi-Unit Housing legislation.

Staff held an information session with the Development Industry on June 5, 2024 with EKISTICS. The purpose of this meeting was to demonstrate the application of the bylaw and have discussions around interpretation. There will be continued discussion with the Industry as the bylaw is implemented and the Development Permit Areas are amended to have a R-SSMUH Development Permit Area (DPA).

OPTIONS:

1. THAT Council adopt Zoning Amendment Bylaw No. 3135, 2024 (small-scale, multi-unit housing).
2. THAT Council provide alternative direction to staff through resolution.
3. THAT Council not proceed.

ATTACHMENTS:

1. Zoning Amendment Bylaw No. 3135, 2024 (small-scale, multi-unit housing) Final Reading
2. Staff Report May 22 - Zoning Amendment Bylaw No. 3135, 2024 (small-scale, multi-unit housing)

Prepared by: Nancy Gothard, RPP, MCIP, Manager of Community and Sustainability Planning
Reviewed by: Marianne Wade, RPP, MCIP, Director of Development Services
Concurrence: Geoff Garbutt, M.Pl., MCIP, RPP, City Manager (CAO)



Bylaw No. 3135

A bylaw to amend Zoning Bylaw No. 2500, 2007

The Council of the Corporation of the City of Courtenay in open meeting assembles enacts as follows:

1. This bylaw may be cited for all purposes as **“Zoning Amendment Bylaw No. 3135, 2024 (small-scale, multi-unit housing).”**

2. That “Zoning Bylaw No. 2500, 2007” be amended by:

(a) **AMENDING DIVISION 3 INTERPRETATION Part 1 Definitions** by:

(i) **DELETING**

~~“**carriage house**” means an accessory dwelling unit located on the second storey of an accessory building accessory to a single residential use which contains no interior access to any part of the accessory building below the carriage house and is located on a lot which is a single real estate entity;~~

And

REPLACING the above noted definition with:

“accessory dwelling unit” means a self-contained dwelling unit, located on the same parcel of land as another dwelling unit that is separate, subordinate in size and accessory to the primary residence, and does not include a mobile home, recreational vehicle, or a storage container.

(ii) **DELETING**

~~“**dwelling, duplex**” means a building consisting of two dwelling units structurally adjoined back-to-front, side-by-side, or one above the other. A secondary suite is not permitted in a duplex dwelling.~~

And

REPLACING the above noted definition with:

“dwelling, duplex” means a building consisting of two dwelling units structurally adjoined back-to-front, side-by-side, or one above the other, and for certainty does not include a building that only contains a principal dwelling unit that contains a secondary suite.

(iii) **DELETING**

~~“**dwelling, townhouse**” means a building consisting of three or more dwelling units, where each unit has separate direct access to the outside at grade and wherein no dwelling unit is located wholly or partly above another dwelling;~~

Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing)

And

REPLACING the above noted definition with:

“dwelling, townhouse” means a building consisting of three or more dwelling units, where each unit has at least two separate direct accesses to the outside.

(iv) ADDING A NEW DEFINITION

“impermeable surface” means a surface which either prevents or impedes the entry of water into the soil mantle as under natural conditions prior to development, and/or a hard surface area which causes water to run off the surface in greater quantities or at an increased rate of flow from the flow present under natural conditions prior to development.

(v) DELETING

~~***“secondary residence” means an additional dwelling unit which:***~~

- ~~–(a) is accessory to a single residential dwelling,~~
- ~~–(b) having a total floor area of not more than 90m² in area,~~
- ~~–(c) located on a property of residential occupancy containing only one other dwelling unit,~~
- ~~–(d) located on a property which is a single real estate entity,~~
- ~~–(e) located on a property over 1,250 m² in lot area.~~

~~***“secondary suite” means a dwelling unit which is accessory to the principle use being made of the lot upon which the secondary suite is located:***~~

- ~~–(a) having a floor space less than 40% of the habitable floor space of the building,~~
- ~~–(b) located within a building of residential occupancy containing only one other dwelling unit,~~
- ~~–(c) located in and part of a building which is a single real estate entity;~~

And

REPLACING the above noted definitions with:

“secondary suite” means a dwelling unit which is:

- (a) accessory to the principal use being made of the lot upon which the secondary suite is located, and***
- (b) located within a building of residential occupancy.***

(vi) DELETING

~~***“yard” means an open space that lies between the principal or accessory building or buildings measured from the building wall and the nearest lot line which is unoccupied and unobstructed from the ground upward including, as illustrated in Figure 4 and detailed below:***~~

- ~~(a) ***“front yard” means that portion of the lot extending from one side of the lot to the other at the narrowest point of the lot with road frontage. The depth of the front yard shall mean the minimum required distance from the front lot line to a line drawn parallel to it.***~~

Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing)

~~(b) "rear yard" means that portion of the lot opposite to the front yard extending from one side of the lot to the other. The depth of the rear yard shall mean the distance from the rear lot line to a line drawn parallel to it. Where the rear portion of the lot is bounded by intersecting side lot lines, the depth of the rear yard shall mean the distance from the point of intersection to a line drawn parallel to the front lot line.~~

~~(c) "side yard" means that portion of the lot extending from the front yard to the rear yard. The width of the side yard shall mean the distance from the side lot line to a line drawn parallel to it.~~

And

REPLACING the above noted definition with:

"yard" means an open space that lies between the principal or accessory building or buildings measured from the building wall and the nearest lot line which is unoccupied and unobstructed from the ground upward including, as illustrated in Figure 4 and detailed below:

(a) "front yard" means that portion of the lot extending from one side of the lot to the other at the narrowest point of the lot with road frontage. The depth of the front yard shall mean the minimum required distance from the front lot line to a line drawn parallel to it.

(b) "rear yard" means that portion of the lot opposite to the front yard extending from one side of the lot to the other. The depth of the rear yard shall mean the distance from the rear lot line to a line drawn parallel to it. Where the rear portion of the lot is bounded by intersecting side lot lines, the depth of the rear yard shall mean the distance from the point of intersection to a line drawn parallel to the front lot line.

(c) "side yard" means that portion of the lot extending from the front yard to the rear yard and may be an interior side yard which abuts the side lot line of an adjacent lot, or an exterior side yard which abuts a street. The width of the side yard shall mean the distance from the side lot line to a line drawn parallel to it.

(b) AMENDING DIVISION 6 GENERAL REGULATIONS by:

i) DELETING Part 6 Siting Exemptions Section 6.6.1.

~~6.6.1. Where chimneys, cornices, leaders, gutters, pilasters, sills, decks, landings or ornamental features project beyond the face of a building, the minimum distance to an abutting lot line as permitted elsewhere in this Bylaw may be reduced by not more than 0.6 m providing that such reduction shall apply only to the projecting feature and except for a zero lot line in which case no feature shall project over the lot line.~~

And

REPLACING the above noted Section with a new Section 6.6.1.

6.6.1. Where chimneys, cornices, leaders, gutters, pilasters, sills, decks, posts, landings or similar features project beyond the face of a building, the minimum distance to an abutting lot line as permitted elsewhere in this Bylaw may be reduced by not more than 0.6 m

Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing)

providing that such reduction shall apply only to the projecting feature and except for a zero-lot line in which case no feature shall project over the lot line.

ii) ADDING A NEW SECTION 6.19.1.

6.19.1. The following conditions shall apply to Secondary Suites and Accessory Dwelling Units:

(i) Not more than one (1) Secondary Suite and one (1) Accessory Dwelling Unit shall be permitted as accessory to a dwelling on a lot where such principal uses are permitted, subject to all other regulations in this Bylaw, and as may be governed by the development regulations of a particular zone.

(ii) No subdivision of a Secondary Suite or Accessory Dwelling Unit shall be permitted.

(iii) The minimum floor area of a Secondary Suite or Accessory Dwelling Unit shall not be less than:

(a) 30 m² for a studio unit;

(b) 45 m² for a one bedroom unit;

(c) 65 m² for a two bedroom unit;

(d) 85 m² for a three bedroom unit.

(iv) An Accessory Dwelling Unit shall not exceed the lesser of 6.5 m in height or the height of the principal residence, unless otherwise specified in the development regulations of a particular zone. Where the width of a dormer or dormers exceeds 50% of the width of the roof on which they are located, the height of the building will be measured to the top of the dormer or to the top of the main roof, whichever is greater.

(v) Parking shall be in conformance with the regulations of this Bylaw.

(vi) Where a Secondary Suite or Accessory Dwelling Unit is permitted, a minimum area of 20 m² of private amenity space shall be provided per dwelling unit. The private amenity space shall have a direct connection to the habitable space and be defined and screened through the use of landscaping such as: plantings, architectural elements such as a trellis, low fences, planters or changes in elevation.

(vii) A lighted pathway is required from the on-site Secondary Suite or Accessory Dwelling Unit parking stall(s) to the entrance door of the Secondary Suite or Accessory Dwelling Unit, provided that the lighting does not shine directly on adjoining properties or interfere with the effectiveness of any traffic control device.

(c) AMENDING DIVISION 7 OFF STREET PARKING AND LOADING Part 7 General Requirements by:

i) ADDING A NEW SECTION 7.1.13. as follows:

7.13.1. Parking Reductions within the Walkable Areas

(1) Properties zoned Residential Small-Scale Multi-Unit (R-SSMUH) within the estimated walking distance shown on Schedule 7F may have the total amount of parking required on a property reduced by one space.

And

Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing)

ADDING A NEW SCHEDULE 7F Walkable Areas to the list of Schedules in DIVISION 7 after Schedule 7E as shown in Schedule C of this Bylaw.

ii) AMENDING Schedule 7A Required Number of Off-Street Parking Spaces

A. Residential Uses

(1) DELETING

~~*Bed and Breakfast – 2 spaces per dwelling unit and 1 space for each sleeping use used for Bed and Breakfast.*~~

And

REPLACING the above noted Subsection with

Bed and Breakfast – 1 space per dwelling unit and 1 space for each sleeping use used for Bed and Breakfast.

And

(2) DELETING

~~*Granny Flat or Carriage House – 1 space per dwelling unit*~~

And

REPLACING the above noted Subsection with

Accessory Dwelling Unit – 1 space per dwelling unit.

(3) ADDING

R-SSMUH - 1 space per dwelling unit

(d) AMENDING DIVISION 8 CLASSIFICATION OF ZONES by:

i) DELETING the following zone classifications from the table:

Zone Classification	Short Title	Lot Size	Description
Residential One	R-1	650 m ²	Single Residential
Residential One-B	R-1B	850 m ²	Single Residential

Zone Classification	Short Title	Lot Size	Description
Residential One-C	R-1C	2500 m ²	Single Residential
Residential One-D	R-1D	500 m ²	Single Residential
Residential One-E	R-1E	300 m ²	Single Residential, Carriage House, Secondary Residential
Residential One-S	R-1S	650 m ²	Single Residential

Residential Two	R-2	750 m ² 900 m ²	Duplex, Carriage House, Secondary Residential
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Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing)

Residential Two A-B	R-2A	730 m ² 900 m ²	Duplex, Carriage House, Secondary Residential
Residential Two B	R-2B	365 m ² 420 m ² 550 m ²	Duplex, Carriage House, Secondary Residential

Rural Residential One	RR-1	1250 m ²	Rural Single Residential
Rural Residential Two	RR-2B	1850 m ²	Rural Single Residential
Rural Residential Two S	RR-2S	1850 m ²	Rural Single Residential
Rural Residential Three	RR-3	2000 m ²	Rural Single Residential
Rural Residential Four	RR-4	2000 m ²	Rural Single Residential
Rural Residential Five	RR-5	4000 m ²	Rural Single Residential

And

- ii) **REPLACING** the above noted Classification of Zones with the following:

Zone Classification	Short Title	Lot Size	Description
Residential Small-Scale Multi-Unit Housing	R-SSMUH	300 m²	Single Residential, Duplex, Townhouse

- (e) **AMENDING DIVISION 8 Part 1 Residential Zones** by:

- i) **DELETING** the following Subsections in their entirety:

Residential One	R-1
Residential One B	R-1B
Residential One C	R-1C
Residential One D	R-1D
Residential One E	R-1E
Residential One S	R-1S
Residential Two	R-2
Residential Two A-B	R-2A
Residential Two B	R-2B
Rural Residential One	RR-1
Rural Residential Two	RR-2B
Rural Residential Two S	RR-2S
Rural Residential Three	RR-3
Rural Residential Four	RR-4
Rural Residential Five	RR-5

And

- i) **REPLACING** the above noted Subsections with:

Residential Small-Scale Multi-Unit Housing	R-SSMUH
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Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing)

As shown in Schedule A attached to and forming part of this Bylaw

2. That Schedule No. 8, (Zoning Map) to Zoning Bylaw No. 2500, 2007 Appendix A be amended in accordance with Schedules B1 through B18, each schedule containing a list of addresses and legal descriptions of properties to be rezoned to the Residential – Small-Scale Multi-Unit (R-SSMUH) Zone, Agriculture One (A-1), Public Use and Assembly Two (PA-2) accompanied by a graphic representation of the properties to be rezoned outlined in bold, all Schedules attached to and forming part of this Bylaw, and listed below as follows:

- Schedule B1** Properties to be rezoned from **Residential One (R-1) to Residential Small-Scale Multi-Unit Housing (R-SSMUH)** (2119 properties)
- Schedule B2** Properties to be rezoned from **Residential One B (R-1B) to Residential Small-Scale Multi-Unit Housing (R-SSMUH)** (304 properties)
- Schedule B3** Properties to be rezoned from **Residential One C (R-1C) to Residential Small-Scale Multi-Unit Housing (R-SSMUH)** (41 properties)
- Schedule B4** Properties to be rezoned from **Residential One D (R-1D) to Residential Small-Scale Multi-Unit Housing (R-SSMUH)** (88 properties)
- Schedule B5** Properties to be rezoned from **Residential One E (R-1E) to Residential Small-Scale Multi-Unit Housing (R-SSMUH)** (2 properties)
- Schedule B6** Properties to be rezoned from **Residential One S (R-1S) to Residential Small-Scale Multi-Unit Housing (R-SSMUH)** (93 properties)
- Schedule B7** Properties to be rezoned from **Residential Two (R-2) to Residential Small-Scale Multi-Unit Housing (R-SSMUH)** (2422 properties)
- Schedule B8** Properties to be rezoned from **Residential Two A (R-2A) to Residential Small-Scale Multi-Unit Housing (R-SSMUH)** (5 properties)
- Schedule B9** Properties to be rezoned from **Residential Two B (R-2B) to Residential Small-Scale Multi-Unit Housing (R-SSMUH)** (147 properties)
- Schedule B10** Properties to be rezoned from **Rural Residential One (RR-1) to Residential Small-Scale Multi-Unit Housing (R-SSMUH)** (69 properties)
- Schedule B11** Properties to be rezoned from **Rural Residential Two (RR-2) to Residential Small-Scale Multi-Unit Housing (R-SSMUH)** (119 properties)
- Schedule B12** Properties to be rezoned from **Residential Two S (RR-2S) to Residential Small-Scale Multi-Unit Housing (R-SSMUH)** (3 properties)
- Schedule B13** Properties to be rezoned from **Rural Residential Three (RR-3) to Residential Small-Scale Multi-Unit Housing (R-SSMUH)** (13 properties)
- Schedule B14** Properties to be rezoned from **Rural Residential Four (RR-4) to Residential Small-Scale Multi-Unit Housing (R-SSMUH)** (73 properties)
- Schedule B15** Properties to be rezoned from **Rural Residential Five (RR-5) to Residential Small-Scale Multi-Unit Housing (R-SSMUH)** (36 properties)
- Schedule B16** Properties to be rezoned from **Residential Rural (R-RU) pursuant to the “Rural Comox Valley Zoning Bylaw No. 520, 2019” (consolidated to October 17, 2023) to Residential Small-Scale Multi-Unit Housing (R-SSMUH)** (114 properties)

Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing)

Schedule B17 Properties to be rezoned from **Rural Residential 5 (RR-5)** to **Agricultural One (A-1)** (1 property)

Schedule B18 Properties to be rezoned from **Residential One (R-1), Residential One B (R-1B), Residential One C (R-1C), Residential One D (R-1D), Residential Two (R-2), Rural Residential Five (RR-5), and Rural Residential (R-RU)** to **Public Use and Assembly Two (PA-2)** (14 properties)

3. That Zoning Bylaw No. 2500, 2007 is hereby ratified and confirmed in every other respect.

4. And that this Bylaw shall come into effect upon final adoption.

READ A FIRST TIME this 22th day of May, 2024.

READ A SECOND TIME this 22th day of May, 2024.

PUBLIC HEARING for the sole purposes of complying with section 481.3 [*zoning bylaws and small-scale multi-family housing*] are prohibited by the Local Government Act section 464 (4).

READ A THIRD TIME this 22th day of May, 2024.

FINALLY PASSED AND ADOPTED this day of , 2024.

Mayor

Corporate Officer

Certified a True Copy

Corporate Officer

Schedule A to Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing)

Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing)

Part 1 – Residential Small Scale Multi Unit Housing (R-SSMUH) Zone

8.1.1. Intent

The R-SSMUH zone is intended to provide appropriate zoning for residential uses including small scale multiple unit housing and infill development on serviced lots.

8.1.2. Permitted Uses

In the R-SSMUH Zone, the following uses are permitted, and all other uses are prohibited except as otherwise noted in this bylaw:

Principal Uses

- (1) *Single residential dwelling*
- (2) *Duplex*
- (3) *Townhouse dwelling*

Secondary Uses

- (4) *Secondary suite*
- (5) *Accessory buildings and structures*
- (6) *Home Occupation*
- (7) *Accessory Dwelling Unit*

8.1.3. Density

A maximum of four dwelling units per lot is permitted subject to the development regulations in this zone, and the provisions of Section 6.19.1. General Regulations for Secondary Suites and Accessory Dwelling Units.

8.1.4. Minimum Lot Dimensions

(1) Lot Size

- i. A lot shall have an area of not less than 300 m² for a single residential dwelling or duplex.
- ii. Where a lot is developed with a multiple unit form and being subdivided along a party wall, the minimum lot area for a fee simple party wall subdivision is 270 m². Where a fourplex is being created by party wall subdivision, no additional secondary dwelling units are permitted.

(2) Lot Frontage

- i. A lot shall have a frontage of not less than 10 m except for a corner lot, where the frontage shall be not less than 13 m. In the case of a lot fronting the bulb of a cul-de-sac, the frontage shall be measured at the front yard setback line.
- ii. Where lots are being subdivided along a party wall, the minimum lot frontage shall be 9 m.

(3) Lot Depth

- i. A lot shall have a depth of not less than 30 m, measured from the front lot line to the rear lot line.

Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing)

8.1.5. Lot Coverage

The maximum lot coverage for buildings, structures and impermeable surfaces is 60%.

8.1.6. Setbacks

(1) Except where otherwise specified in this Bylaw, the following minimum building setbacks shall apply:

- i. Front yard: 4.0 m except that the minimum front yard setback for garages or carports shall be 6.0 m.
- ii. Rear Yard: 5.0 m
- iii. Side Yard: 1.5 m except that
 - (a) Where a side yard flanks a street, excluding a lane, the minimum side yard shall be 3.0 m, and
 - (b) Where a secondary residence or accessory dwelling unit located behind the principal building lacks side or rear lane access, the minimum side yard setback distance for the principal building on one side shall be 3.0 m.
 - (c) Where units are separated by a party wall subdivision, the side yard setback along the party wall shall be 0 m.

8.1.7. Height of Buildings

- (1) The height of a principal building shall not exceed 11.0 m.
- (2) The height of a secondary residence or accessory dwelling unit shall not exceed 6.5 m.

8.1.8. Accessory Buildings and Accessory Structures

- (1) Shall not exceed 4.5 m in height
- (2) Shall be permitted in the side and rear yard provided they shall conform to all relevant siting regulations of this bylaw
- (3) Shall be permitted at the front of a principal residence provided they shall conform to all relevant siting regulations of this bylaw
- (4) Shall not be located within 1.5 m from the side and rear lot line except where the side or rear yard flanks a street, excluding a lane, in which case the minimum yard distance shall be 3.0 m.

8.1.9. Off-Street Parking

- (1) Except as provided in this section, off-street parking shall be provided and maintained in accordance with the requirements of Division 7 of this bylaw.
- (2) Where more than one access point to a property is available, such as a front street and laneway, required parking stalls must be distributed in such a manner as to prevent a continuous row of parking stalls.

8.1.10. Landscaping and Screening

- (1) In addition to the Landscape Requirements identified in Division 6, Part 14 of this bylaw, a vegetated buffer or screen fence not more than 1.5 m high shall be provided along rear or side property lines adjoining other properties.

8.1.11. Conditions of Use

Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing)

- (1) Despite the permitted use regulations in this zone, no more than one (1) Secondary Suite or one (1) Accessory Dwelling Unit shall be permitted as accessory to a single family or duplex dwelling on a lot that is not connected to community water or sewer services.

Schedule B to Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing)

Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing)

Schedule C to Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing)



The Corporation of the City of Courtenay

Staff Report

To: Council

File No.: 3360-20-2402

From: Director of Development Services

Date: May 22, 2024

Subject: Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing)

PURPOSE:

For Council to consider first reading of Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing) (Attachment 1 May 8, 2024), modifications to Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing), and second reading and third reading of Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing) as modified. Attachment 3 is a track changes version of Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing) for consideration, and Attachment 4 is the same bylaw with the track changes modifications incorporated.

BACKGROUND:

Council, at its Regular Meeting of May 8, 2024, considered a report entitled Small-Scale Multi-Unit Housing Regulations from the Director of Development Services, which provided a summary of recent amendments to the *Local Government Act* intended to accommodate small-scale infill housing and the proposed R-SSMUH Zone regulations. Staff was seeking some feedback from Council on parking requirements and the general regulations. As a result of this discussion Council passed the following resolutions:

THAT Council receive for information the report from the Director of Development Services entitled "Small-Scale Multi-Unit Housing Regulations" and dated May 8, 2024; and THAT the parking requirements that apply to properties that are subject to the Small-Scale Multi-Unit Housing be as follows:

- properties within 15-minute walking radius of the downtown core, or town centres, or urban corridor, be reduced by one stall per property; and,*
- when more than one access point to a property is available, such as a front street and laneway, parking stalls must be distributed in a manner as to prevent a continuous row of parking stalls.*

THAT Council direct staff to bring forward amendments to "Zoning Bylaw No. 2500, 2007" in substantially the form set out in the reported dated May 8, 2024 and Attachment 1, if applicable, other affected bylaws that will bring municipal regulations into conformity with the Local Government Act.

THAT Council direct staff report back to Council on the feasibility of providing residential on street parking to meet the parking requirements of the R-SSMUH Zone, to identify and consider any required amendment to Division 7 of the "City of Courtenay Zoning Bylaw No. 2500, 2007", in 2025.

Proposed Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing) was posted on social media and webpage after Council's consideration of the report. Given this notice formed part of the information available to the Public, modifications will need to be considered to Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing) by Council prior to consideration of second reading of the bylaw.

DISCUSSION:

The City must comply with Bill 44 and subsequent changes to the *Local Government Act* that mandates properties that qualify as “Restricted Zones” and have a lot size of under 4050m² must be rezoned to permit three (3) dwelling units on lots under 280m² and four (4) dwelling units for lots over 280m² by June 30, 2024. Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing), is attached to this report as Attachment 1, it addresses the provincial regulations for Bill 44. Bill 47 Transit Oriented Development Areas (TOD) does not apply to the City of Courtenay and as such the proposed Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing) does not permit six (6) dwelling units per lot.

Eligible Properties

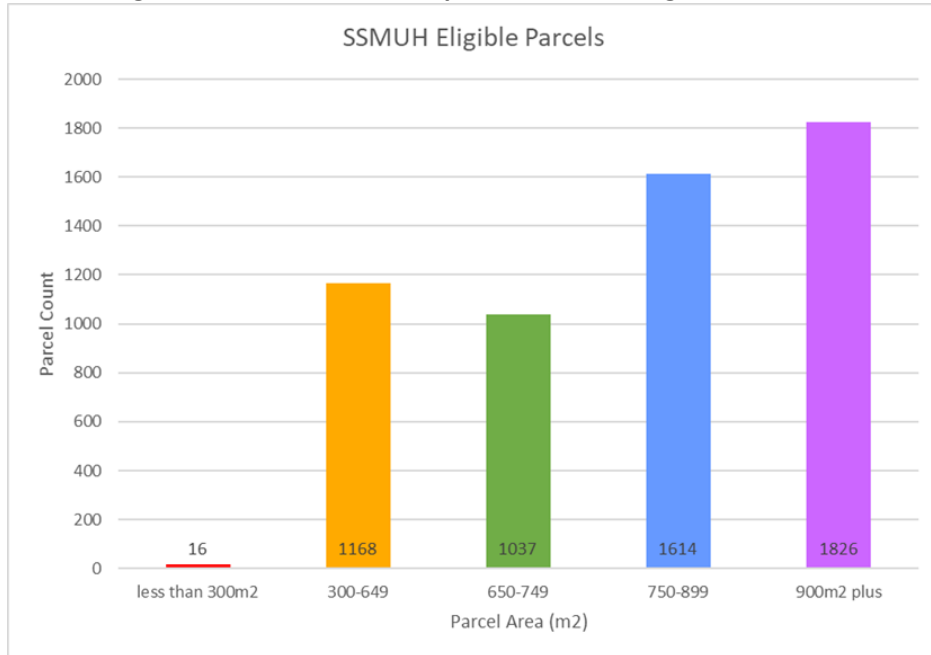
Staff note that there is a handful of lots under 300m² for the properties identified as “restricted Zone” which would be eligible for three (3) dwelling units. As noted in Figure 1 the majority of the eligible properties that are considered “Restricted Zone” are 300 m² or greater and would qualify for four (4) dwelling units under Bill 44. Future modifications to the R-SSMUH zone can be undertaken after this date and staff will be monitoring the impact on the R-SSMUH zone after its adoption and will update Council.

The City of Courtenay Zoning Bylaw No. 2500, 2007 contains sixteen (16) residential zones that meet the provincial definition of “Restricted Zone, being: R-1, R-1B, R-1C, R-1D, R-1E, R-1S, R-2, R-2A, R-2B, RR-1, RR-2, RR-2S, RR-3, RR-4, RR-5, and R-RU zones. A “Restricted Zone” is a residential zone that limits the permitted uses to single residential dwellings and/or duplexes, with or without a second accessory dwelling unit.

Staff evaluate that the most appropriate approach for the City of Courtenay is to rezone all properties in the “Restricted Zones” to the new Residential Small-Scale Multi-Unit Housing (R-SSMUH) zone shown as Schedule A in Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing) in Attachment 1 to this report. As many of the identified “Restricted Zones” are similar in terms of permitted principal and secondary uses, and development regulations such as minimum lot size, lot depth, and setbacks in existing zones are equal to or greater than those proposed in the new R-SSMUH zone, the risk of creating non-conforming properties is minimized.

There are 5649 properties that have been identified to be rezoned to R-SSMUH. Figure 1 below identifies the distribution of parcel size ranges that will be rezoned to R-SSMUH by number of properties. This represents approximately 60% of our properties and 22% of the City area. Many of the larger lots could potentially be subdivided into two or even three lots with the 300 m² minimum lot size being proposed in the new R-SSMUH zone.

Figure 1: Size of Parcels Proposed for Rezoning To R-SSMUH



R-SSMUH Zone

The proposed R-SSMUH zone follows provincially recommended reductions in setbacks and parking, as well as recommendation for increased height and lot coverage, which will allow for additional units on the average size lot. Details such as lot size and shape, lane access, corner or interior lot location, size of proposed units, Development Permit Area (DPA) Regulations for Form and Character, and the impacts of any environmental/hazardous features will determine the potential for additional units on any specific lot within the proposed R-SSMUH zone.

Proposed Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing) Attachment 1 contains a series of text and map amendments to:

DIVISION 3 INTERPRETATION

- Part 1 Definitions that will streamline and facilitate the administration of the proposed R-SSMUH zone. This includes deletions, revisions, and additions to support the SSMUH regulations noted in Zoning Bylaw Amendment No. 3135, 2024.

DIVISION 6 GENERAL REGULATIONS

- A new Section 6.19.1 has been added that will allow either a Secondary Suite and/or Accessory Dwelling unit on properties that are not currently being considered for R-SSMUH zoning.

DIVISION 7 OFF STREET PARKING AND LOADING

- Proposed parking requirements for the R-SSMUH Use in Schedule A Required Number of Off-street Parking spaces in Zoning Bylaw No. 2500, 2007 is one per dwelling unit regardless of dwelling type.
- In addition, pursuant to Council’s direction at the Regular Meeting of May 8, 2024, a reduction of one parking space within the Walkable Areas r based on a 15 minute walk.
- A new Section 7.1.13. Parking Reductions within the Walkable Areas has been added and are included as a proposed text amendment in Attachment 1 to this report. A map has been

generated based upon a 15-minute walk and illustrated in “Schedule 7F “Walkable Areas” has been attached to proposed Bylaw No. 3135, 2024 as Schedule C.

DIVISION 8 CLASSIFICATION OF ZONES

- Schedule A to proposed Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing) outlines the regulations for the Residential Small-Scale Multi-Unit Housing (R-SSMUH) Zone which includes Permitted Principal and Secondary Uses, Density, Minimum Lot Size, Building Height and Setbacks, that are consistent with the intent of the provincial requirements and guidelines.

SCHEDULE 8 ZONING MAP

Map and Table of Eligible Properties

- Schedules B1 through B16 identify the current “Restricted Zone” properties that are proposed to be rezoned to R-SSMUH. This are zoning map amendments to Zoning Bylaw No. 2500, 2007 that are identified in Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing). These proposed mapping amendments are to Schedule 8 (Zoning Map) to Zoning Bylaw No. 2500, 2007, and list the legal descriptions, addresses and Property Identification (PID) numbers of those properties proposed to be rezoned from their existing “Restricted Zone” to the proposed Residential Small-Scale Multi-Unit Housing (R-SSMUH) zone.

Map and Table of Non-Eligible Properties for R-SSMUH

- Schedules B17 and B18 identify 15 non-eligible properties for R-SSMUH that were discovered during staff’s examination of the total number of lots eligible for SSMUH zoning. Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing) identifies these mapping amendments which will be applied to Schedule 8 (Zoning Map) to Zoning Bylaw No. 2500, 2007, and list the legal descriptions, addresses and Property Identification (PID) numbers. Of the 15 properties in “Restricted Zones” with existing residential zoning the following amendments are proposed:
 - One (1) property is a large lot within the Floodplain designated for Agriculture in the Official Community Plan (Schedule B17 to proposed Bylaw 3135, 2024). This property is proposed to be rezoned from Rural Residential 5 (RR-5) to the Agricultural One (A-1) zone as it is not suitable for SSMUH zoning.
 - The remaining 14 properties identified as not eligible for R-SSMUH are City property, provincially owned, or owned by a land trust, with Official Community Plan land use designations as Parks and Recreation. These 14 lots are proposed to be rezoned to Public Assembly 2 (PA-2) (Schedule B18 to proposed Bylaw 3135, 2024).

Details of those properties identified as non-eligible are presented in Table 1 Properties Not Eligible for SSMUH zoning in Attachment 2 to this report. Provincial regulations require that a list of properties in “Restricted Zones” not being rezoned to a residential scale-scale multi-unit housing zone be provided to the Province along with appropriate justification for this decision. Table 1 Properties Not Eligible for R-SSMUH will form part of that submission to the Province.

Official Community Plan (OCP)

An Official Community Plan (OCP) land use designation conflict was identified that requires an amendment to the OCP for 2576 Hebrides Crescent from Parks and Recreation to Urban Residential as the zoning is R-1B and will be rezoned to R-SSMUH. This is a privately owned property. Bill 44 permits this conflict to exist in

order to facilitate SSMUH legislative goals. This OCP designation will be officially corrected in the next OCP update cycle which must occur by December 31, 2025.

Address	Current Zone	Proposed Zone	Land Use Designation	Legal description
2576 Hebrides Cr	R-1B	R-SSMUH	Parks and Recreation	LOT 22 DISTRICT LOT 158 COMOX DISTRICT PLAN VIP79704

Bill 44 and subsequent amendments to the Local Government Act

Section 481.3 (7) of the *Local Government Act* requires that:

“In developing or adopting a zoning bylaw to permit the use and density of use required under this section to be permitted, a local government must consider applicable guidelines, if any under Section 582.1 (provincial policy guidelines related to small-scale multi-unit housing)”.

Table 1 below shows a comparison of the proposed R-SSMUH zoning regulations with guidelines contained in Part 4 of the Provincial Policy Manual and Site Standards. The proposed R-SSMUH zoning regulations follow the recommended provincial guidelines fairly closely with minor variations for local conditions.

Table 1: Comparison of Provincial Recommended Guidelines and Proposed Residential – Small-Scale Multi-Unit Housing (R-SSMUH) Zone Regulations

Zoning Bylaw Parameter	Recommended Benchmark	Provincial Considerations	Proposed R-SSMUH Zone Regulations
Front Yard Setback	Minimum 2 m	A front lot line setback of 4-6 m may be warranted if there are no sidewalks or public boulevards for trees, or to accommodate stormwater infrastructure or future road right of way dedications.	Minimum: 4 m with provision for garage/carport setback of 6 m.
Rear Yard Setback	Minimum 6 m for principal buildings Minimum 1 m for ADUs	Actual rear lot line setbacks will approximate 5 m if parking in rear is required due to parking requirements and lot configuration	Minimum: 5 m for principal buildings 1.5 m for ADUs
Side Yard Setbacks	Minimum 1.2 m	Actual side yard setbacks will approximate 3 m if parking in rear is required due to parking requirements and lot configuration.	Minimum: 1.5 m for interior side yards 3.0 m for exterior side yards
Height	Maximum 11 m to midpoint of sloped roof or flat roof on principal buildings	A universal height limit that permits three storeys regardless of the method of measurement, site gradient, or roof style is recommended to help improve the viability and diversity of housing forms	Maximum: 11 m to midpoint of sloping roof or flat roof on principal buildings 6.5 m to midpoint of sloped roof or flat roof for ADUs

Maximum No. of Storeys	3 storeys for principal building 2 storeys for ADUs		No regulations for storeys
Maximum lot coverage	50%	Onsite parking requirements will contribute significantly to impervious surface coverages on lots. Impervious coverages exceeding 60% may require on-site stormwater retention and/or treatment.	60% for impermeable surfaces.
Off-Street Parking	1 space per dwelling unit	Other factors that could be used to set parking requirements include proximity to services (i.e. designated village or town centres), walk scores, and the availability of on-street or other parking alternatives. Higher maximum parking requirements (e.g. 1.5 spaces per unit may be appropriate in smaller communities with no or limited public transportation, or for example, where on-street parking is impractical due to snow removal requirements.	1 space per dwelling unit
Adapted from Table 6 Recommended zoning regulations for lots requiring a minimum of 3 or 4 units that are less than 1,215 m ² in size. (Provincial Policy Manual and Site Standards Part 4 p.70)			

Secondary Suites and Accessory Dwelling units in other Zones

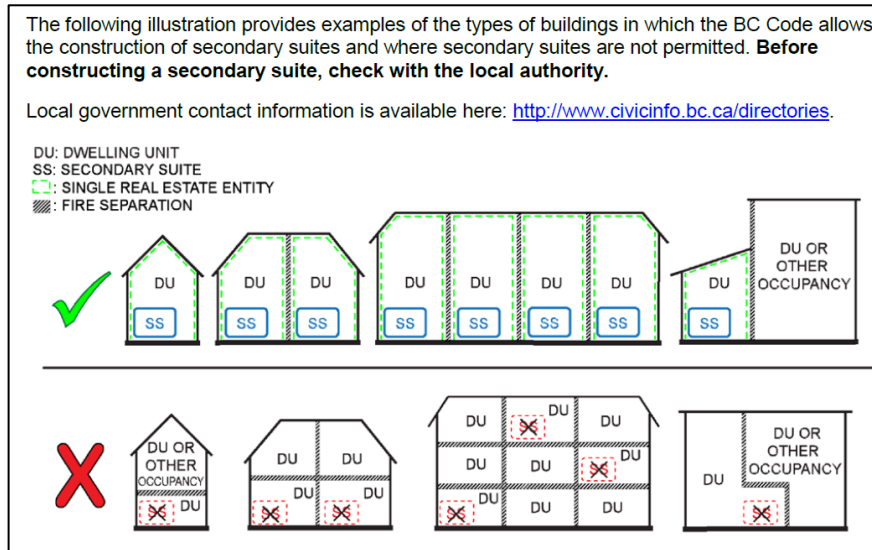
In addition to zoning approximately 5649 lots within the City of Courtenay to the proposed R-SSMUH zone, a proposed new Section 6.19.1. in DIVISION 6 GENERAL REGULATIONS will allow either a Secondary Suite and an Accessory Dwelling Unit as accessory to a single residential or duplex dwelling in any zone where such principal uses are permitted, including Agricultural, Rural Residential or CD Zones subject to development regulations in the applicable zone or any other restrictions such as Restrictive Covenants or strata bylaws.

In this manner, the City of Courtenay expects to achieve its goal of significantly adding to the number, size, diversity and tenure (ownership or rental) of housing options available within the City. This will align with Bill 44 expectations of the “Restricted Use” be permitted a secondary suite and an accessory dwelling unit on properties that permit the use to increase housing supply.

BC Building Code

Figure 2 below, is an excerpt from BC Building Code Information Bulletin No. B19-05, and illustrates potential housing forms and tenure options that may be possible within the Building Code regulations for secondary suites in a single dwelling unit or a duplex or a townhouse, considering health, safety and fire protection. Building Code standards in addition to zoning parameters will determine the opportunity for infill development of secondary suites within a dwelling unit.

**Figure 2: Excerpt from BC Building Code Information Bulletin No. B19-05. December 12, 2019
Secondary Suites, Changes to Design and Construction Requirements BCBC 2018 Revision 2**



Testing of the R-SSMUH Zone

At the May 8, 2024 regular Council meeting, EKISTICS presented a series of graphics on the various forms and unit type mix may be possible to test the R-SSMUH zone regulations to demonstrate what may be built. Some of these graphics were in the May 8, 2024 report to Council. (Please see Attachment 5)

POLICY ANALYSIS:

The proposed Bylaw Amendments respond to Bill 44 provincial legislation and timelines for adoption, thereby aligning Courtenay bylaws with the new requirements.

In order to fully support the Small-Scale Multi-Unit Housing initiative, other City Bylaws may need to be amended, and staff will bring those amendments forward at the earliest opportunity for the consideration of Council.

FINANCIAL IMPLICATIONS:

The Province has provided funding to each local government to support planning and capacity to meet the new provincial housing legislative requirements. The City of Courtenay received \$286,000 from this fund earlier this year.

ADMINISTRATIVE IMPLICATIONS:

The mandatory timelines imposed by the province are challenging and will require priority to be assigned to this project at the expense of other regular duties in addition to consultant services.

STRATEGIC PRIORITIES REFERENCE:

This initiative addresses the following strategic priorities:

- Buildings and Landscape - Update Zoning Bylaw - review maximum building heights
- Buildings and Landscape - Review and update land use regulations and bylaws for consistency with OCP

PUBLIC ENGAGEMENT:

Bill 44 amendments to the *Local Government Act* prohibits a Council from holding a public hearing for zoning bylaw amendments that are for the purpose of implementing the Small-Scale Multi-Unit Housing legislation.

The City is still required to provide notice as per Section 467 of the *Local Government Act*, and Development Procedures Bylaw 3106, 2023. Notice was placed in the newspaper, on social media and posted on the City Webpage. As there are greater than 10 properties affected, approximately 5649 properties, notice is given through social media, webpage and advertisement in the paper. No mailouts are required under the *Local Government Act*.

For the 15 properties that are not eligible for R-SMMUH zone and being rezoned to PA-2 or A-1, a mailout was undertaken to occupants and owners within a 100-metre radius of that property which resulted in a total of 623 notices that were mailed.

As of the date of this report, one written comment has been received and is attached to this report. Staff have received some questions seeking clarifications. Any further written comments received will be provided to Council. (Attachment 6)

OPTIONS:

1. **NOTE:** Each motion to be read separately.

THAT Council give first reading to Zoning Amendment Bylaw No. 3135, 2024 (small-scale, multi-unit housing) dated May 8, 2024.

That Council modify Zoning Bylaw Amendment Bylaw No. 3135, 2024 (small-scale, multi-unit housing) as follows:

- a. In Section 6.19.1. (i) delete “or” after (1) secondary Suite add “and”;
- b. In Section 6.19.1 (ii) delete “or strata titling”;
- c. In Section 7.1.13 delete “the Downtown Core Area” and replace with “Walkable Areas”;
- d. In Section 7.1.13 (1) delete “for additional units, secondary to the principal residential use” and replace with “on a property”;
- e. In Section 7.1.13 add after Schedule 7F “Walkable Areas” and after Schedule 7E add “shown in Schedule C of this Bylaw”;
- f. In Schedule 7A-Required Number of Off-Street Parking Spaces add the R-SSMUH use under A. Residential and add under the Required Parking Space 1 per dwelling unit to correspond with the SSMUH use;
- g. In Schedule A to Zoning Amendment Bylaw No.3135, 2024 (small-scale, multi-unit housing), renumbering the duplicate occurrence of 8.1.6 to become 8.1.7, and renumbering the remainder of section 8.1 accordingly;
- h. In Schedule A to Zoning Amendment Bylaw No.3135, 2024 (small-scale, multi-unit housing) Section 8.1.9 Off Street Parking (2) delete “No more than 50% of the front yard area shall be used for off street parking” and replace with “Where more than one access point to a property is available, such as a front street and laneway, required parking stalls must be distributed in such a manner as to prevent a continuous row of parking stalls”;

- i. In (e) AMENDING DIVISION 8 Part 1 Residential Zones 2. Change “B17 to B18” and add after Small-Scale Multi-Unit (R-SSMUH) Zone, “Agriculture One (A-1), Public Use and Assembly Two (PA-2)”;
- j. In DIVISION 3 Part 1 add to the definition of duplex after “other” “and for certainty does not include a building that only contains a principal dwelling unit that contains a secondary suite”;
- k. In DIVISION 3 Part 1 for the definition dwelling townhouse:
 - i. after “has” add the words “at least two”;
 - ii. delete the word “a”; and
 - iii. add the letters “es” to access;
- l. In DIVISION 3 Part 1 for the definition of secondary suite:
 - i. add the word “and” after “located”; and
 - ii. delete the words “contained within a building which is a single real estate entity”.

THAT Council give second reading as modified to Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing).

THAT Council give third reading as modified to Zoning Amendment Bylaw No. 3135, 2024 (small-scale-multi-unit housing).

THAT given Ministerial Order No. M114, dated May 8, 2024, and regrading Approval Exemption (Controlled Access Highway) Regulation, Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing) does not require referral to the Minister of Transportation and Infrastructure for approval.

THAT Council confirms that it has given consideration to the Provincial Policy Manual and Site Standards document prepared by the Province of British Columbia, specifically, the standards recommended in Part 4 of the document.

THAT pursuant to Section 484.4 (2) of the *Local Government Act*, Staff prepare and submit the required letter of notification to the Province as soon as practicable after adoption of Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing), identifying the “Restricted Zone” properties that have been rezoned to the Residential Small-Scale Multi-Housing zone and the additional properties within “Restricted Zones” that are exempt from Sections 481.3 (4) and (5) of the *Local Government Act*.

THAT staff, following final adoption and before the end of 2024, bring a report back to Council on the effects of Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing) on the development of infill housing within Courtenay.

THAT, given the size and scope of Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing), staff may need to bring forward additional amendments to correct the zoning bylaw, which will be done at no cost to the property owner.

2. THAT Council provide alternative direction to staff.

ATTACHMENTS:

1. Zoning Amendment Bylaw No. 3135, 2024 (small-scale multi-unit housing) May 8, 2024
2. Table of properties not eligible for R-SSMUH zone
3. May 22 modified Zoning Amendment Bylaw No. 3135 (small-scale multi-unit housing) with Track Changes
4. May 22 modified Zoning Amendment Bylaw No. 3135 (small-scale multi-unit housing)
5. 2024 Staff report dated May 8, 2024
6. Written Comment received in response to public notification

Prepared by: Nancy Gothard, RPP, MCIP, Manager of Community and Sustainability Planning

Reviewed by: Marianne Wade, RPP, MCIP, Director of Development Services

Concurrence: Geoff Garbutt, M.Pl., MCIP, RPP, City Manager (CAO)



The Corporation of the City of Courtenay

Bylaw No. 3138

**A bylaw to amend the License of Occupation fees within the Schedule of Fees and Charges
SECTION II APPENDIX I DEVELOPMENT FEES of City of Courtenay Fees and Charges Bylaw No.
1673, 1992**

The Council of the City of Courtenay, in open meeting assembled, enacts as follows:

1. This Bylaw shall be cited as “Fees and Charges Amendment Bylaw No. 3138, 2024 – Development Application Fees”.
2. That “City of Courtenay Fees and Charges Bylaw No. 1673, 1992” be amended by:
(a) AMENDING Licence of Occupation fee category by:

- i. ~~DELETING
Sidewalk Cafés~~

~~\$5.00 per m² per month for sidewalk or SRW occupied~~

~~Plus \$100.00 per parking space occupied per month~~

And

REPLACING the above noted fee category with:

Sidewalk Cafés

- a) For occupation of an on-street parking stall, annual fee \$100 per stall
- b) For occupation of sidewalk or statutory right of way, annual fee of \$5.00 per m²

- ii. ~~DELETING
Seasonal License Fee~~

~~————— \$25.00 per month or \$125.00 for May 1 to October 31~~

And

REPLACING the above noted fee category with:

Seasonal Application Fee

\$25.00 per month or \$125.00 for April 1 – October 31

3. And that this Bylaw shall come into effect upon final adoption.

Read a first time this 22nd day of May, 2024
Read a second time this 22nd day of May, 2024
Read a third time this 22nd day of May, 2024
Adopted this [day] day of [month], 2024



THE CORPORATION OF THE CITY OF COURTENAY

COUNCIL MEMBER REPORT

To: **COUNCIL**

File No.: 0540

From: Councillor Hillian

Date (MMM-YYYY): May-2024

Subject: **REPORT OF ACTIVITIES AND EVENTS**

	DATE (MMM-DD)	EVENT/LOCATION	COMMENTS
1.	May-03	Sewer Conveyance Project Groundbreaking Ceremony	
2.	May-06	Meeting with City Manager and Mayor; meeting with citizen group re delegation to Council	
3.	May-07	Sewage Commission; Water Committee; Recreation Commission; Regional District Board meeting	
4.	May-08	Meeting with KFN Councillor; McHappy Day fundraiser for Foundry;	
5.	May-09	K'omoks/CVRD leadership meeting re Sewer project traffic management; Chamber of Commerce office opening	
6.	May-13	Kus-kus-sum partner group meeting; Council Quarterly Check-In session	
7.	May-15	Council and RCMP meeting re Community Policing Priorities	
8.	May-16	Emterra cheque presentation to Hospital Foundation; Moosehide event re intimate partner violence	

	DATE <i>(MMM-DD)</i>	EVENT/LOCATION	COMMENTS
9.	May-22	Community Justice Centre committee meeting	
10.	May-23	Community Justice Centre Board meeting; community presentation by Joe Roberts, the "Skid Row CEO"	
11.	May-24	Island Health Update for community leaders; Comox Fire Department truck dedication to John Marinus	
12.	May-27	Providence Living at the Views (dementia village) information session	
13.	May-28	Sewage Commission; CVRD Board; Mile of Flowers Plant-In	
14.	May-30	Regional District Strategic Planning Session	
15.	May-31	City Engagement Strategy Interview	
16.			
17.			
18.			



THE CORPORATION OF THE CITY OF COURTENAY

COUNCIL MEMBER REPORT

To: **COUNCIL**

File No.: 0540

From: Councillor McCollum

Date (MMM-YYYY): May-2024

Subject: **REPORT OF ACTIVITIES AND EVENTS**

	DATE (MMM-DD)	EVENT/LOCATION	COMMENTS
1.	Apr-02	Lunch & Learn - fees and charges 1-1 with Mayor re: acting schedule	
2.	Apr-03	Council agenda review	
3.	Apr-04	Parks and Recreation Advisory Commission meeting	
4.	Apr-05	Meet the mayor session - BC Housing development	
5.	Apr-08	CVRD agenda review with CAO & Council	
6.	Apr-09	Sewage Commission CVRD Board	
7.	Apr-12	AVICC conference	
8.	Apr-16	Elected Officials forum - Tourism Strategy	

	DATE <i>(MMM-DD)</i>	EVENT/LOCATION	COMMENTS
9.	Apr-17	Meet the Mayor session - BC housing project Meet the Mayor session- owners of 951 Fitzgerald	
10.	Apr-19	Lunch and Learn - Intergrated Rainwater Management Plan (IRMP)	
11.	Apr-22	CVRD Agenda review	
12.	Apr-23	Meet w/ the Mayor Water Committee CVRD Board	
13.	Apr-26	Meeting with CAO Lunch and Learn - Traffic Calming Policy	
14.	Apr-29	Meet the Mayor session - pickle ball facility request	
15.	Apr-30	CVRD Rec commission agenda review with CVRD staff E-Bikes for Climate and Equality Webinar	
16.	May-03	Vancouver Island Coastal Community - Committee for Solid Waste and the Circular Economy	
17.	May-06	Community Energy Association - Lunch and Learn CVRD Agenda Review	
18.	May-07	MFA Board of Trustees Meeting Sewage Commission CV Recreation Commission CVRD Board Meeting	

	DATE <i>(MMM-DD)</i>	EVENT/LOCATION	COMMENTS
19.	May-08	MFA Board of Trustees Meeting Sewage Commission CV Rec Commission CVRD Board	
20.	May-10	CEA Strategy Lunch & Learn - AI implementation CVRD Agenda review with Council	
21.	May-11	Sewage Commssion CVRD Board meeting	
22.	May-13	Quarterly Council check-in	
23.	May-15	Chair/Vice CAO liasion Special Council Meeting - RCMP discussion	
24.	May-16	CSWM - Board meeting	
25.	May-21	Finance and Audit Committee for Community Energy Association	
26.	May-22	Community Energy Association - Q2 Board	
27.	May-23	CEA Strategy Lunch and Learn - UBCM perspectives	
28.	May-28	Sewage Commission Regional Parks and Trail Committee CVRD Board Meeting	